

4

KEY PRINCIPLES OF THE MASTERPLAN AND ESTABLISHING THE PARAMETER PLANS

4.1 OUR RESPONSE: KEY PRINCIPLES OF THE MASTERPLAN AND ESTABLISHING THE PARAMETER PLANS

A masterplan is not a graphic pattern; it is a description of vital relationships. The long-term implementation of a Masterplan of this scale and complexity depends on maintaining the flexibility implied in these relationships. Therefore, there is flexibility within the layout, where roads, paths, passages and building boundaries are not fixed but can be subject to levels of deviation. These deviations are carefully considered to ensure the Masterplan can continue to ensure to secure the deep-rooted design principles which underpin it.

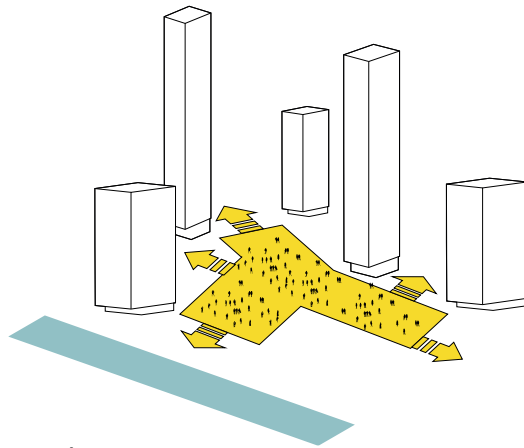
These principles have evolved from an understanding of the context to promote the creation of specific urban character while preserving the open-ended and dynamic possibilities required to sustain the future health of this urban quarter.

Our analysis of North Quay and the contextual framework related to the development of the Site has culminated in a number of guiding principles - principles which will remain valid and robust even as inevitable changes in the financial climate, building regulations, construction practice and technology occur during its implementation. The Masterplan also places importance on environmental considerations including health, well-being, emissions reduction and climate change adaptation.

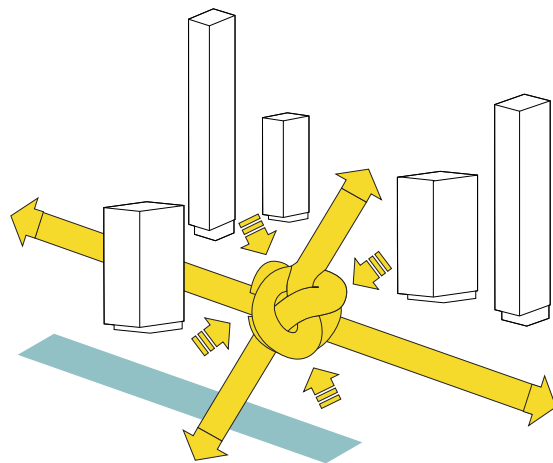
The principles are crucial to the development of the Site. If the guiding design principles are respected, the details of The Masterplan can evolve according to future needs without detriment to the character of the place or the wider aspirations of the vision for North Quay.

The following ten principles have been developed for this Site and represent a culmination the Applicant brief, extensive analysis of the existing Site and surrounding context, considerable statutory and community consultation, positive attributes retained from previous designs, and Allies and Morrisons philosophy as masterplanners.

They have informed the design process of the Indicative Scheme at every stage and will be critical to the future development of North Quay as a lively and thriving place.



Principle one



Principle two

PRINCIPLE ONE

Public Realm

Public realm leads the design through a legible network of high quality streets and safe, accessible spaces, which distribute value and a sense of place across the Masterplan.

Many masterplans primarily focus on the creation of building plots and buildings; the public realm for these plans is often designed in semi-isolation, making good of the spaces which result from the buildings the masterplan describes.

In contrast, the Masterplan should place the creation of high-quality public space at the head of the design process. The principle achievement of the Masterplan – the ‘glue’ which holds the neighbourhood together – will always be its well-considered network of routes, open spaces, and the waterside.

Previous schemes have proposed large elevated podium structures, lifting the public above the waterside level. To make this Masterplan more like a piece of functioning city, there should be a philosophy of spreading the spaces out at dockside level and placing them close to buildings, thus providing amenity on their doorstep.

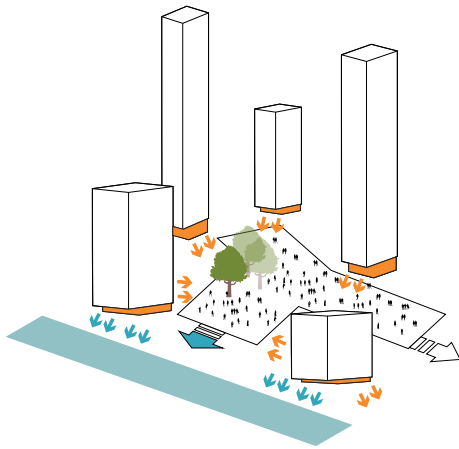
PRINCIPLE TWO

Making Connections

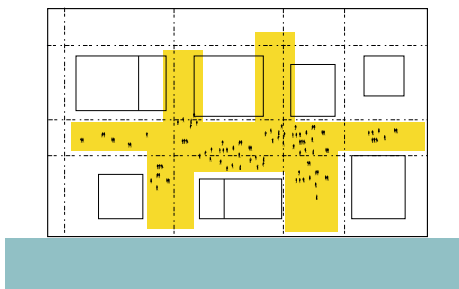
New and existing connections to and through the Masterplan provide resilient, convenient, inclusive and permeable routes for the surrounding communities, emphasising connections for pedestrians and cyclists from South Poplar to public transport modes while allowing on-street drop-off, to be accessible to all. Anticipate future connections to and through the Billingsgate site.

The making of connections is essential to establish continuity between existing and new parts of the city. The proposed interconnected street network for North Quay creates a balanced range of convenient, human-scaled, safe routes for pedestrians and vehicles within the Site.

The ambition for the Masterplan is to bring vitality to its location and act as catalyst for long-term growth, investment and opportunity beyond its own red line boundary. To do this, North Quay must create synergies with its neighbours – South Poplar, West India Quay, Canary Wharf and, in the near future, the Billingsgate site – by inviting in and opening out to people and uses in adjacent places.



Principle three



Principle four

PRINCIPLE THREE

Active Spaces

A variety of public realm and building uses, bringing together live/stay, work, retail and food/beverage, will activate frontages and encourage animation of public spaces, streets and the water edge.

This masterplan-level strategy is intended to encourage a wider mix of uses in North Quay and a complementary mix of uses within buildings (vertically) and across the Masterplan (horizontally).

The existing Canary Wharf estate is characterised by its extensive below-grade shopping and leisure facilities. In contrast, as with that emerging in Wood Wharf, North Quay should bring these activities to street and quayside level. North Quay takes London's typical mixed-used streetscapes as its precedent - encouraging civility and social interaction at ground level.

Our experience as city-dwellers and designers teaches us that the most attractive urban neighbourhoods are the ones that support a range of useful amenities close to everyone's doorstep. When such useful amenities and public transport are within easy walking and cycling distance, people spend less time in cars.

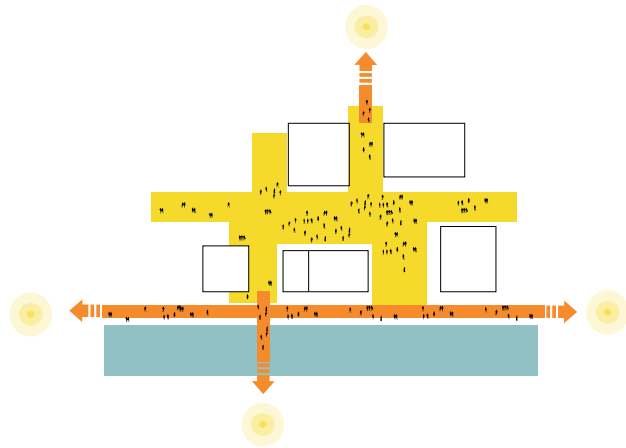
PRINCIPLE FOUR

A Flexible Framework

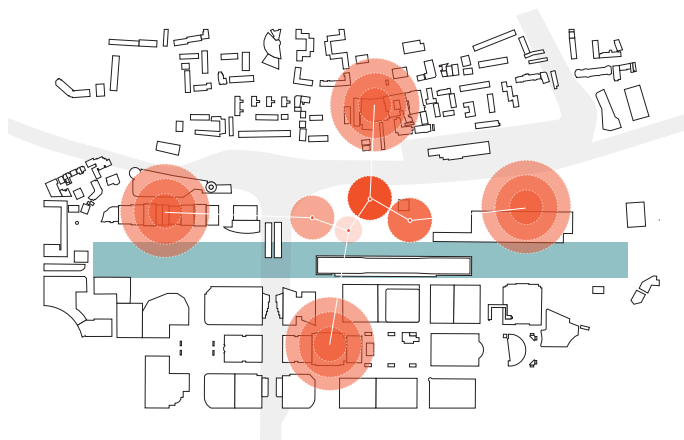
A rational plan arrangement offers greater flexibility in terms of building typology, use and future change, thereby providing inherent robustness that will secure the long-term viability of the Masterplan.

Geometrically complex Development Plots which rely on overly prescriptive building forms risk becoming outmoded when the property market, building technologies or tastes change. Likewise, buildings which are oddly shaped often prove less adaptable to other uses, undermining their flexibility and longevity.

Therefore, the Masterplan should define Development Zones that are flexible and based on relatively simple geometric forms and floorplate dimensions capable of supporting a range of building types and uses.



Principle five



Principle six

PRINCIPLE FIVE

Site Responsive

Allow the unique character and history of the Site to inform the design response, maximising the potential of the accessible and active waterside and the close relationship to Canary Wharf and South Poplar.

The Site has a unique history which should be retained and highlighted wherever possible. This embedded heritage helps to establish a sense of place on the Site. The dockside environment is an integral part of the current character and historic identity of North Quay and, as such, an opportunity.

The Masterplan should create an accessible water edge route and adjacent spaces which provide public amenity and the opportunity for engagement with the existing water space of the dock.

Visual, as well as physical, connection to the water should be prioritised from important spaces within the plan.

Improving connectivity with South Poplar is key to encouraging people into North Quay and the occupiers of the Site out to the wider community. This will improve access through to public transport, the Major Town Centre and onward to existing open spaces for play and recreation.

PRINCIPLE SIX

Character

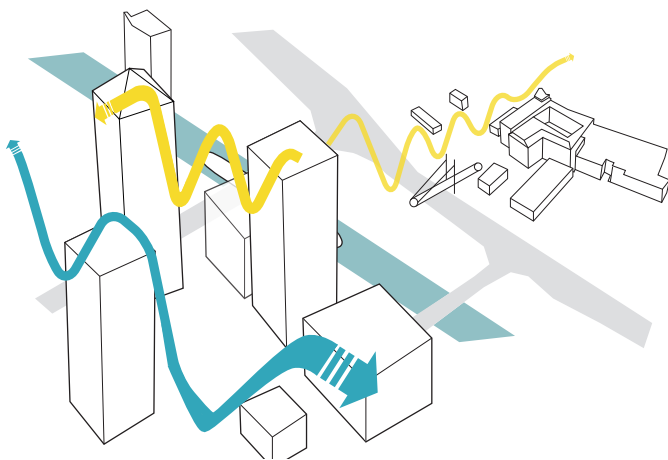
Create a varied and rich character to the different spaces within the Masterplan to enhance the whole Development and engage with the surrounding context.

Cities are not made of homogeneous environments. The character of a city is defined both by the quality of its various urban quarters and by the coherent impression they form as a whole.

The character of North Quay should take advantage of the Site's pre-existing physical qualities and topography, while complementing the established Canary Wharf. The opportunity exists to create a variety of urban environments within the Masterplan, including waterfront and garden spaces, a vibrant civic square, a new stepped plaza, as well as areas for business and recreation. In turn, these pieces should fit together – each according to its place – as a complex but legible urban ensemble.



Principle seven



Principle eight

PRINCIPLE SEVEN

Creating Places

Extend the Canary Wharf business, retail and leisure hub into The Masterplan, with new public space surrounded by shops and restaurants, state of the art offices and live/stay buildings, as an attractive destination for local residents, workers and visitors from further afield.

The provision of new live stay buildings and modern workspace within the Masterplan will bring a critical mass of people to this area. The Masterplan should create a vibrant heart which will provide a public focal point for the critical mass to congregate, generating an identity for North Quay.

Public consultation showed a need to provide open space and co-working opportunities within the Masterplan. Bringing many uses together in one central melting pot will be inviting to businesses from all industries.

PRINCIPLE EIGHT

Townscape

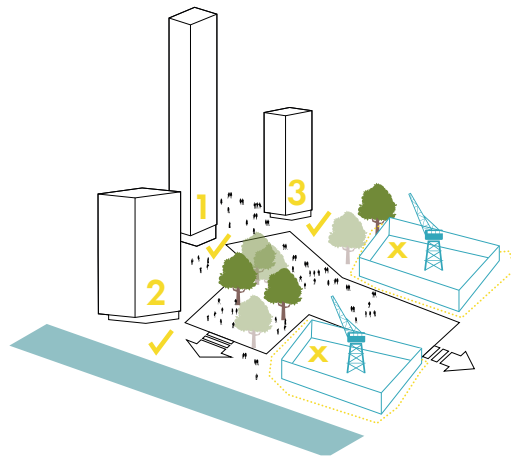
Building heights and massing should be varied to provide an articulated skyline that responds to the Canary Wharf context but also provides a positive urban relationship to Aspen Way and South Poplar, with urban grain and proportions considered in relation to both the local and wider context.

Building heights in the Masterplan are determined by a number of factors including the City Airport flight path, planning policy, strategic and local townscape views, overshadowing and wind microclimate, adjacent tall buildings and the West India Dock Conservation Area. The buildings should not only be considered in relation to their immediate neighbours but also to their place in wider panoramic views from other parts of the city.

There should be a variety of scales and building heights across the Site, generated by the desire to create successful spaces within a legible urban realm and a varied skyline that is complementary to the existing context.



Principle nine



Principle ten

PRINCIPLE NINE

Sustainable Design

A sensible and holistic approach incorporating biodiversity measures and sustainable strategies, to form an intelligent Masterplan response to environmental issues relating to operational and embodied energy, circular economy principles, water use, pollution, ecology and habitat, and the promotion of health and wellbeing.

A sustainable place is one which provides conditions that promote and maintain happy bodies and minds while at the same time imposing a light footprint on the ground. the Masterplan embraces a best practice approach to green infrastructure, energy use, walking, cycling, public transport, recreation and culture.

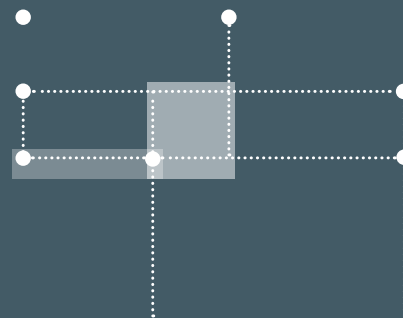
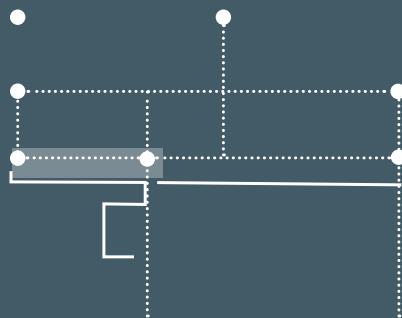
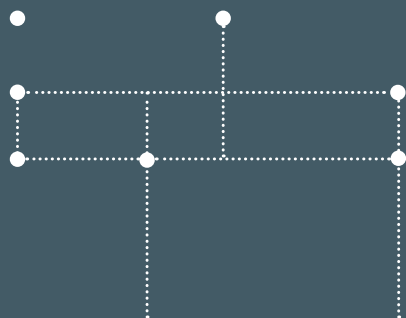
The Site will be assessed under BREEAM Communities and new commercial buildings will target BREEAM Excellent. Residential buildings will be assessed under Home Quality Mark (HQM) methodology, targeting an equivalent to Code for Sustainable Homes Level 4.

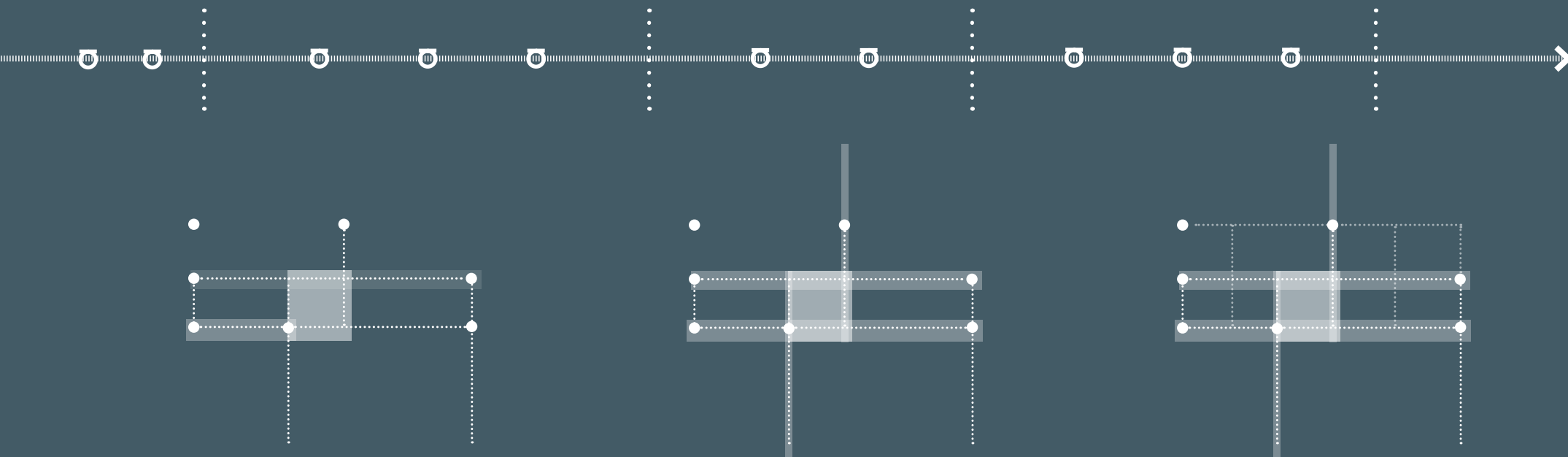
PRINCIPLE TEN

Phased Delivery

Enable a viable phasing strategy which is adaptable and amenable to future change, allows temporary alternative uses, and where each phase can successfully stand on its own.

It is important that at all stages of execution the Masterplan should define a safe, legible and positive urban environment. It is our goal that, over time, future positive opportunities for the creation of North Quay as a place can be incorporated without significant adaptation to the strategic rules set within this OPA.



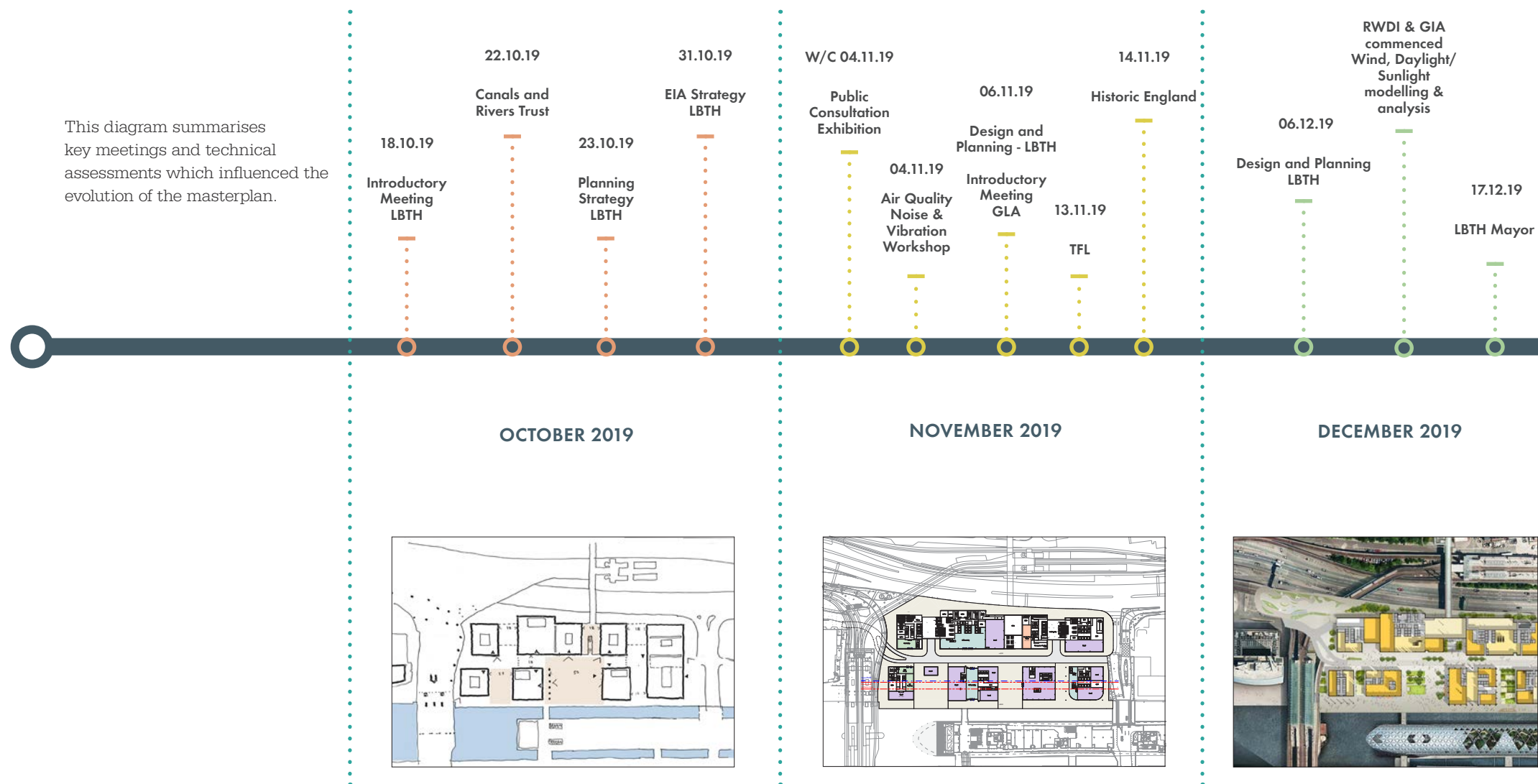


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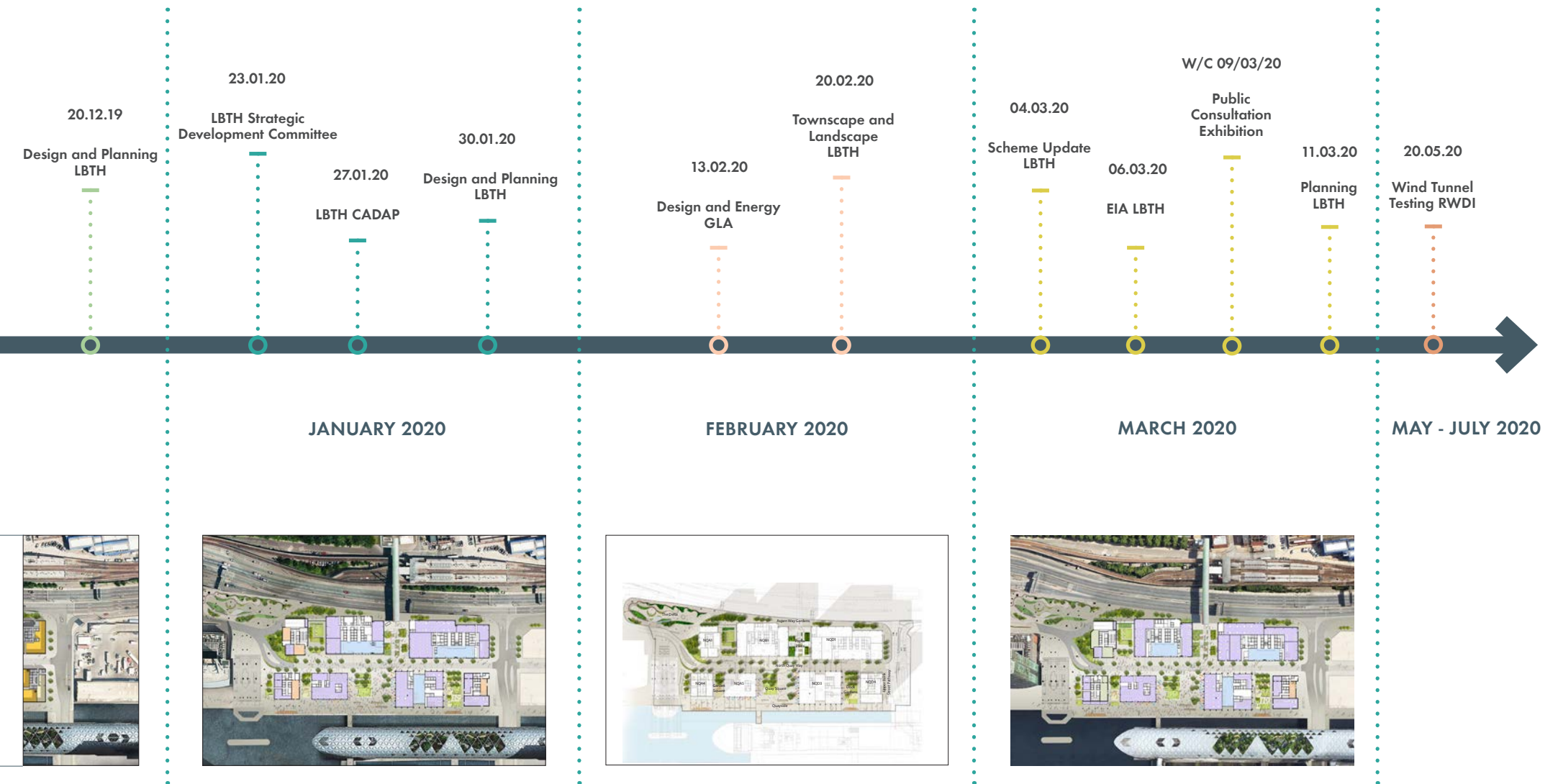
DESIGN EVOLUTION AND
CONSULTATION

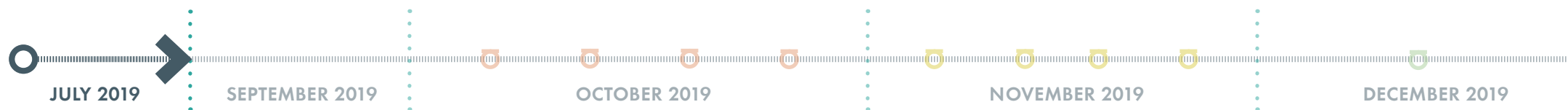
5.1 ALLIES AND MORRISON DESIGN EVOLUTION

This diagram summarises key meetings and technical assessments which influenced the evolution of the masterplan.



A timeline to illustrate dates in key consultation, technical assessment milestones and evolution of the masterplan





5.1.1 APPOINTMENT

Allies and Morrison were appointed by the Applicant in July 2019 to develop a masterplan for the redevelopment of the Site and to work towards applications for OPP and supporting LBC. This was to be developed in the context of the previous schemes from 2007 and 2017 and the new Applicant brief as outlined in section 2.1 of this document.

5.1.2 CONSULTATION AND ENGAGEMENT

The key stages of design evolution have been informed by feedback received from pre-application consultations with the local authority, other statutory consultees and engagement with local residents and businesses. This chapter sets out this process.

From October 2019 through to March 2020, a structured series of pre-application meetings were held with representatives from:

- LBTH Planning
- LBTH Design
- LBTH Highways
- LBTH Mayor
- LBTH SDC

- LBTH CADAP
- GLA Planning, Design and Energy
- TFL / DLR
- Historic England
- CRT

A series of public consultations including open exhibitions and meetings with local resident groups and stakeholders have been held through the pre-application stages. Public exhibitions were advertised and held firstly in November 2019 and again in March 2020. These were supported by several smaller engagement sessions with local community groups and businesses. The feedback from this consultation process was instrumental in informing the design development of the Masterplan. This process is described in the accompanying Statement of Community Involvement (SCI) (NQ.PA.14).

5.1.3 TECHNICAL ASSESSMENTS

During the evolution of the Masterplan, a number of technical assessments were undertaken. Namely wind, air quality, noise, sunlight, daylight and overshadowing. These also influenced the emerging masterplan.

July-September 2019: Initial design proposals and design evolution

A contextual analysis was undertaken looking at the Local Plan Site Allocation, surrounding context, emerging adjacent development sites and the constraints and opportunities of the Site. This has been summarised in Chapters 1 and 2.

A number of walking routes were undertaken from the various nearby transport nodes and Poplar High Street through to the Site to understand key arrival positions and the current journey experience for pedestrians.

This provided an understanding of existing urban relationships, views and desire lines and the importance of improving pedestrian and cyclist connectivity from within the wider surrounding context through to the various public transport nodes and the facilities of Canary Wharf.

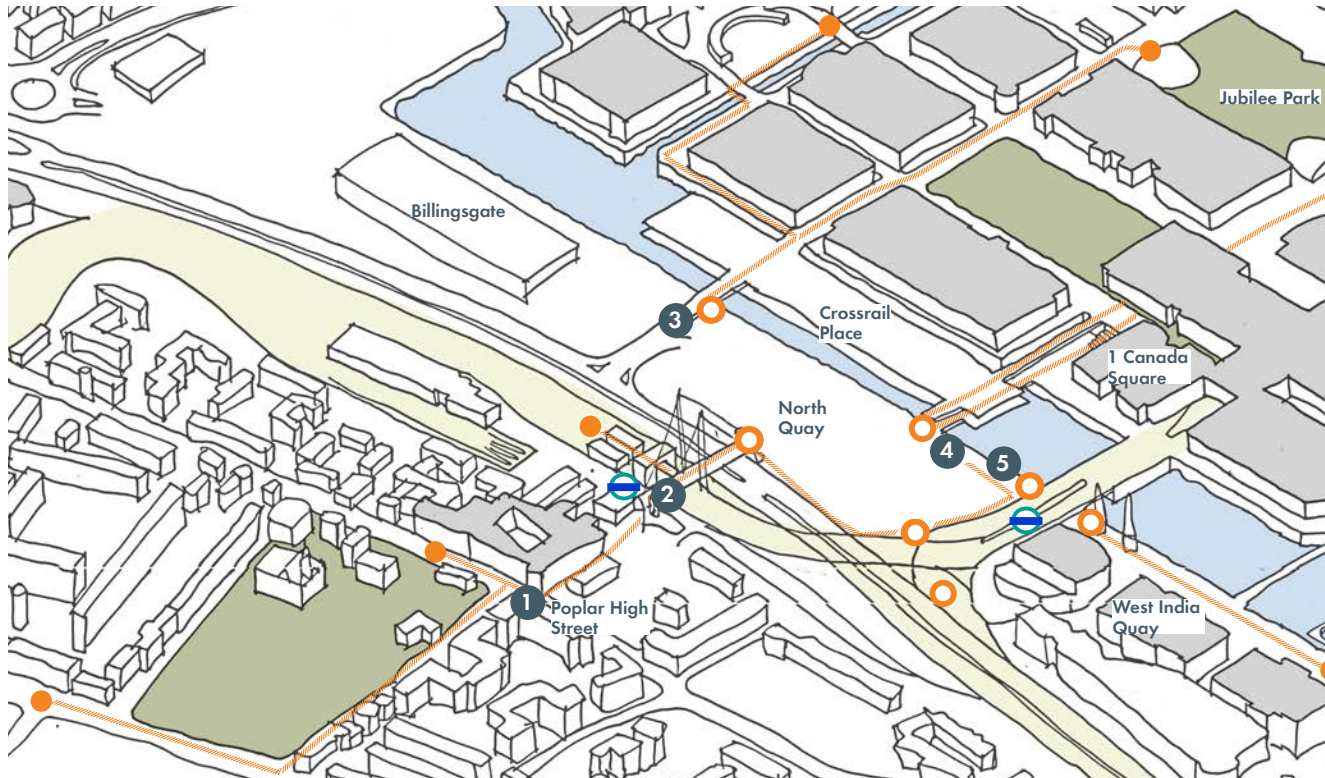
This highlighted the key opportunities of the North Quay Site in improving north-south connections from South Poplar through to Canary Wharf, the Elizabeth Line and the Jubilee Line. It also highlighted the importance of the Quayside to connect West India Quay through to Billingsgate as a future development site.

JANUARY 2020

FEBRUARY 2020

MARCH 2020

JULY 2020



3D map of walking routes

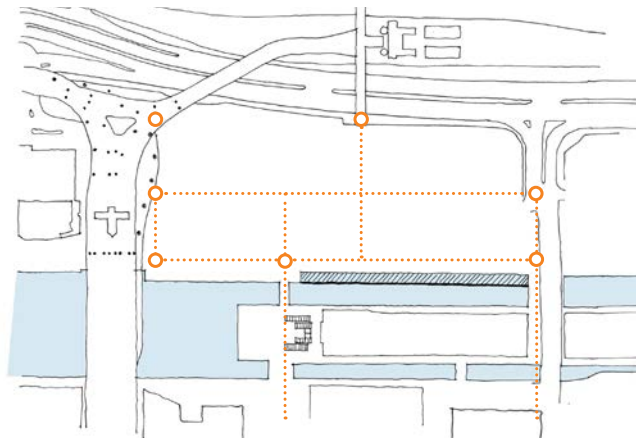


Photos of walking routes

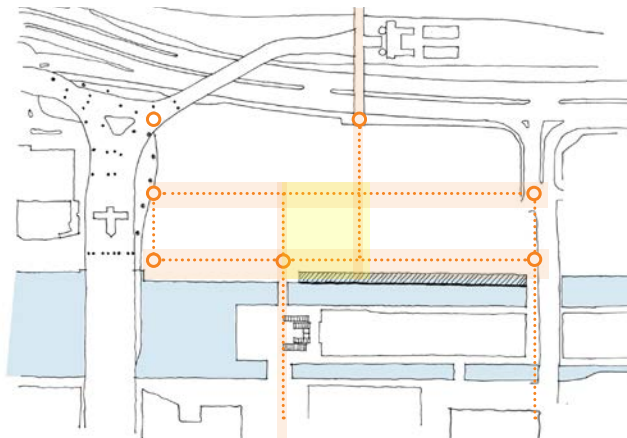
Learning from the two previous applications and particularly the withdrawn 2017 scheme for the Site, a new approach was developed and discussed for the Masterplan which included the following key objectives:

- Reduce infrastructure, bringing the public realm down to ground level and creating a human scale
- Incorporate flexibility to enable the scheme to react to an unpredictable future
- Allow for a range of land uses
- Learn from the success of the Wood Wharf masterplan

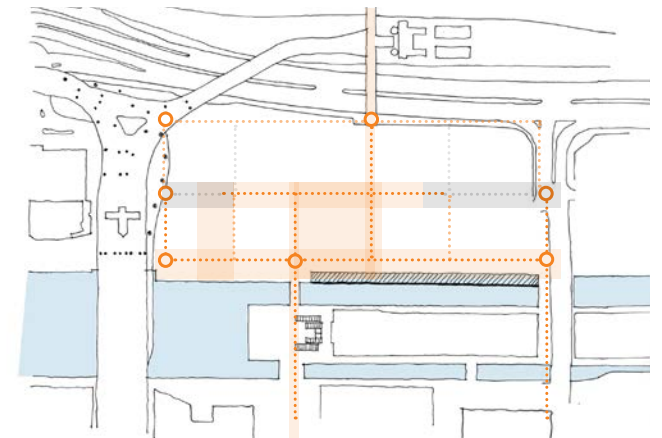
Early massing sketches also started to look at the principle of creating an articulated skyline to respond to the wider site context. Some early placement of this massing in a couple of local townscape views helped the design team to assess potential wider setting impacts.



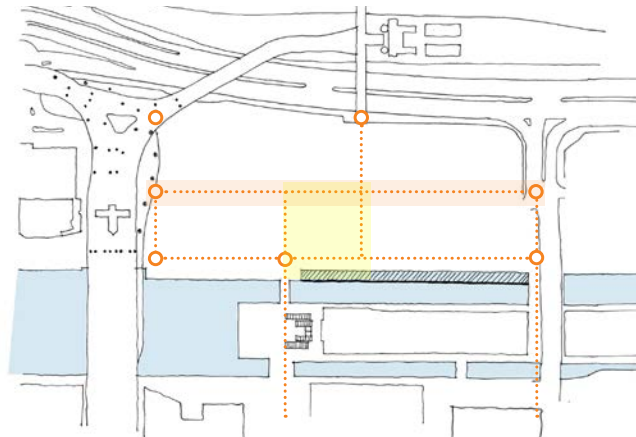
1 - key connections



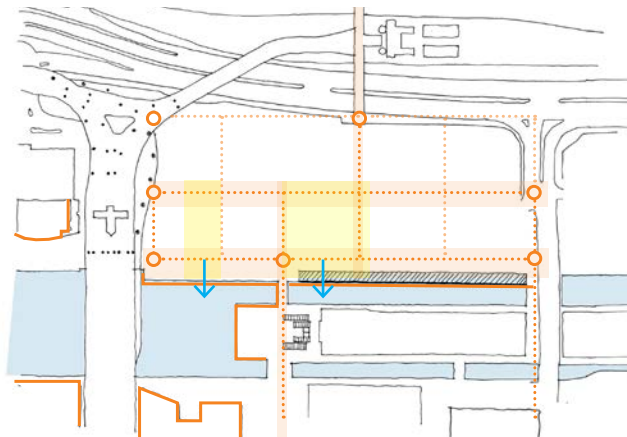
3 - public realm through key connections



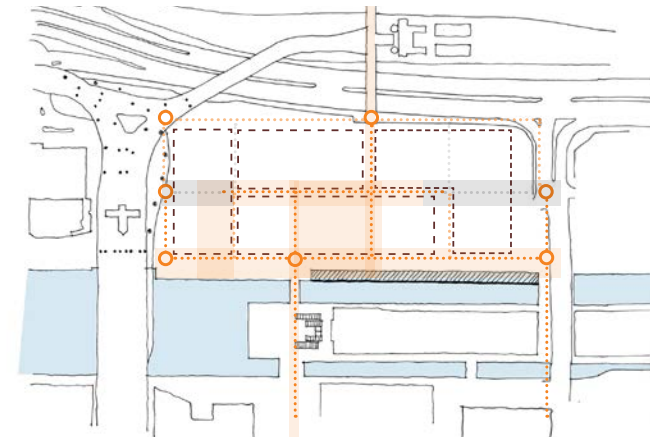
5 - first thoughts on servicing



2 - a key central space and street



4 - key connections to public spaces and to the water



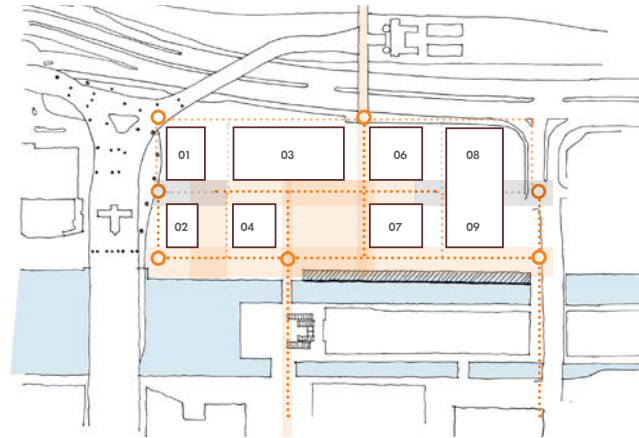
6 - development zones

JANUARY 2020

FEBRUARY 2020

MARCH 2020

JULY 2020



7 - development plots

October 2019: Introductory Pre-Application meeting

On 18th October 2019, the first introductory pre-application meeting was held with LBTH planning and design officers to introduce to them the early site and context analysis and emerging thoughts on the framework and massing for the Masterplan.

This meeting introduced the new design team working on the project and the context of the new planning approach of working towards an OPA. Allies and Morrison outlined the key objectives and how, by using a similar

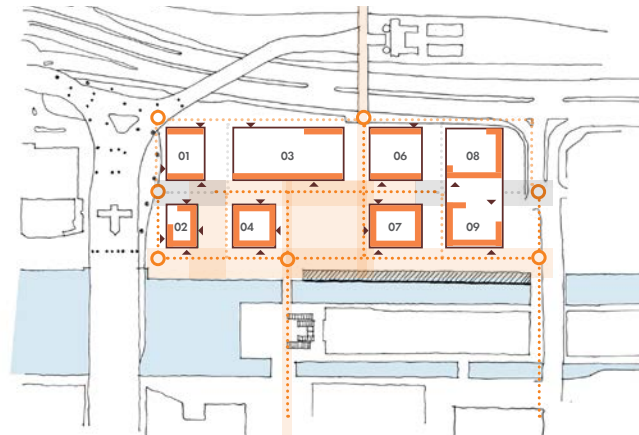
approach to the consented Wood Wharf permission and, learning from the previous detailed applications for the North Quay Site, a flexible future proof OPA framework could be developed through consultation with LBTH.

A series of design principles were discussed, looking at an outward looking public realm led scheme with a mix of uses that would be sufficiently flexible to respond to changing needs and attract a broad range of tenants and occupiers.

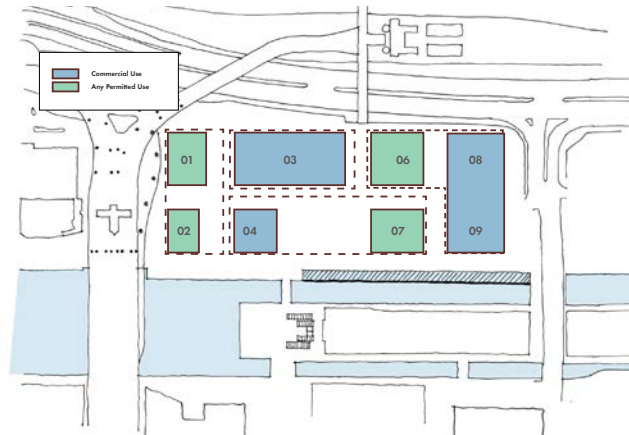
Site context, constraints and opportunities were discussed including the walking routes and opportunities for wider connectivity that had been undertaken.

Diagrams presented showed the design team's first thoughts on potential upper floor uses, locations for building entrances, activating ground floor frontages with retail and leisure uses and an early illustrative site plan layout.

This culminated in initial first thoughts for the Site – looking to connect the arrival points together with a series of routes and spaces within the Site, forming a 'chassis' with a new central street and principal open space addressing its waterside setting. This framework of routes and spaces then created a series of flexible development zones with additional secondary spaces between.



8 - activating ground floors with entrances, food and beverage and retail



9 - development zones and flexible upper use classes



MASTERPLAN 1



First thoughts - Masterplan 1

JANUARY 2020

A series of hand drawn sketches and two preliminary townscape outline views suggested an approach to building heights and 3d massing that would complement the existing Canary Wharf skyline with a campus of buildings with a varied articulated skyline.

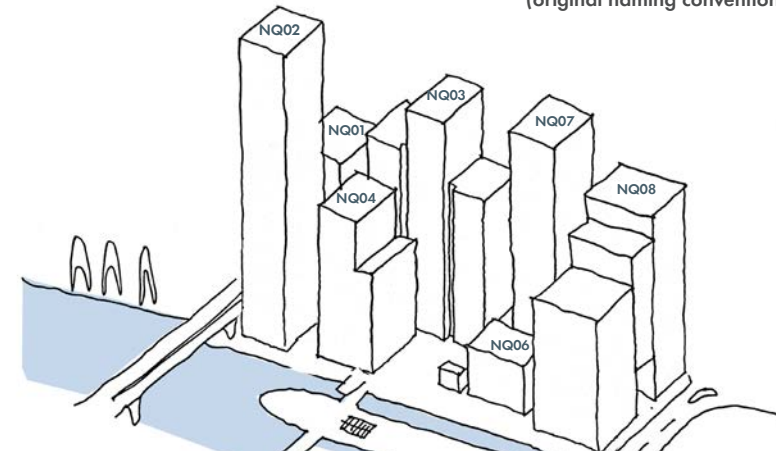
This was explained in the context of a possible maximum parameter envelope that would allow some flexibility for the massing of each building to be developed as each plot comes forward, controlled by the Parameter Plans, Development Specification and a set of Design Guidelines.

FEBRUARY 2020

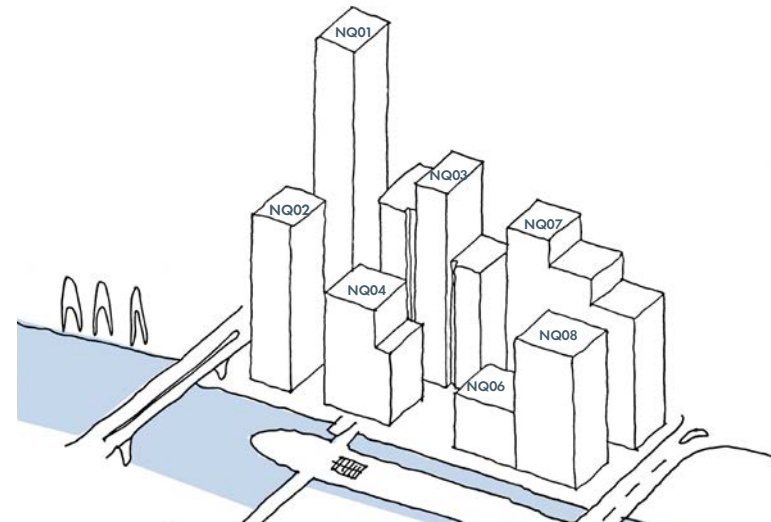
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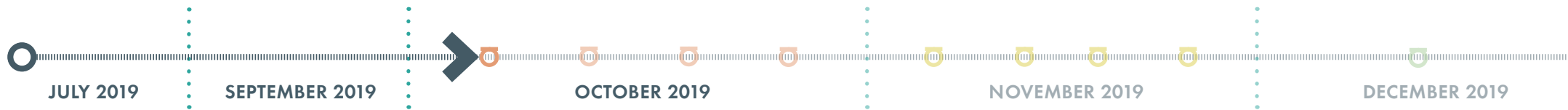
(original naming convention)



Tower on the Quayside and building over the central street (NQ08)

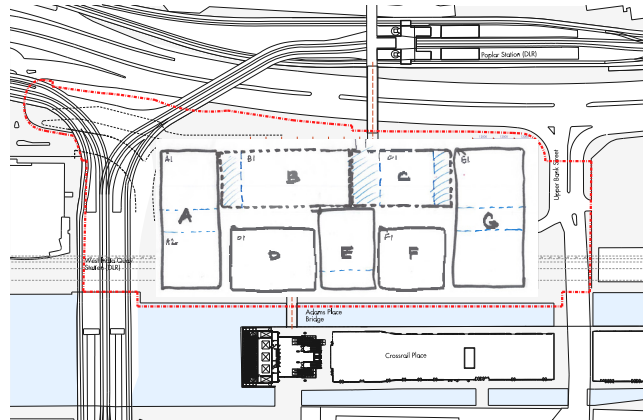


Tower on Aspen Way and rotated NQ07 away from the central street



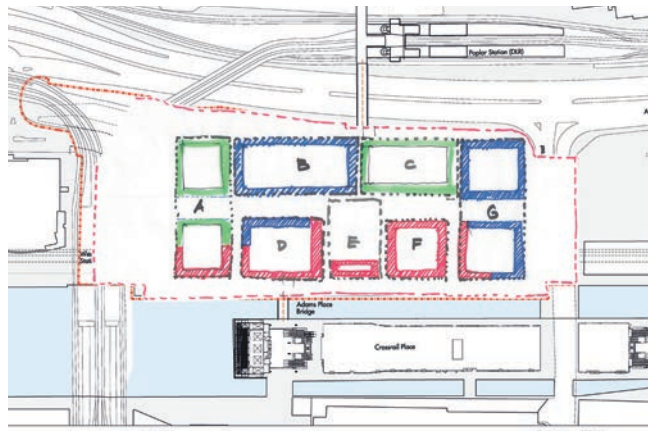
Early sketch of key parameter plan - Land Use

- Development Zone
- Predominantly A/D Uses
- Predominantly B1/C1 Uses
- Any Permitted Uses

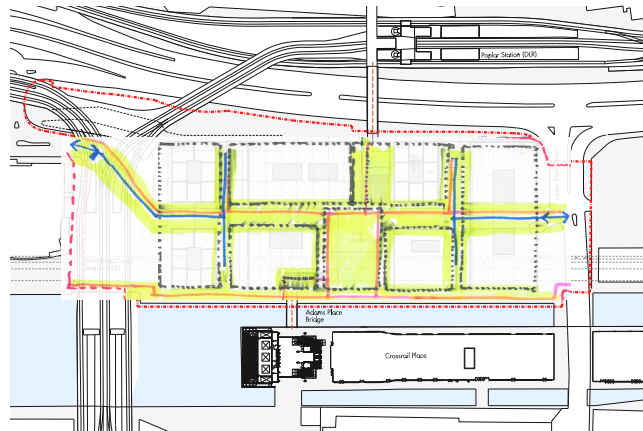


Early sketch of key parameter plan - Development Zones

- Development Zone
- Limit of Deviation
- Vehicular Route
- Pedestrian and Emergency Vehicle Route



Early sketch of key parameter plan - Ground Floor Frontages



Early sketch of key parameter plan - Access and Circulation

There was initial broad support from the local authority for the new approach to the Site and the OPA route. Further information was requested about the landscape and open space strategy alongside discussion on how the flexibility sought might be included within the Parameter Plans and other Control Documents.



Key summary of the first presentation to LBTH:

- Fine grain of Masterplan supported
- The proposed flexibility and use of Control Documents was supported
- The relationship between NQ and Billingsgate should be considered
- Consideration of how the scheme will relate to South Poplar was welcomed
- Important that the scheme will allow for a range of employment floor space
- Consideration of the Aspen Way footbridge is crucial
- The level changes must be fully considered
- Noise and air quality limitations must be understood.

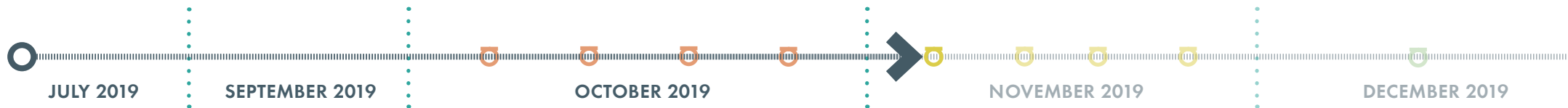
This version of the masterplan was presented to Canals and Rivers Trust (CRT) on 22nd October 2019 to provide them with a scheme overview and discuss the relationship of the development with the dock.

CRT were supportive of the emerging principles, in particular proposals for bringing the public realm down to grade and bringing people closer to the water, activating the dock edge by introducing retail and restaurants.

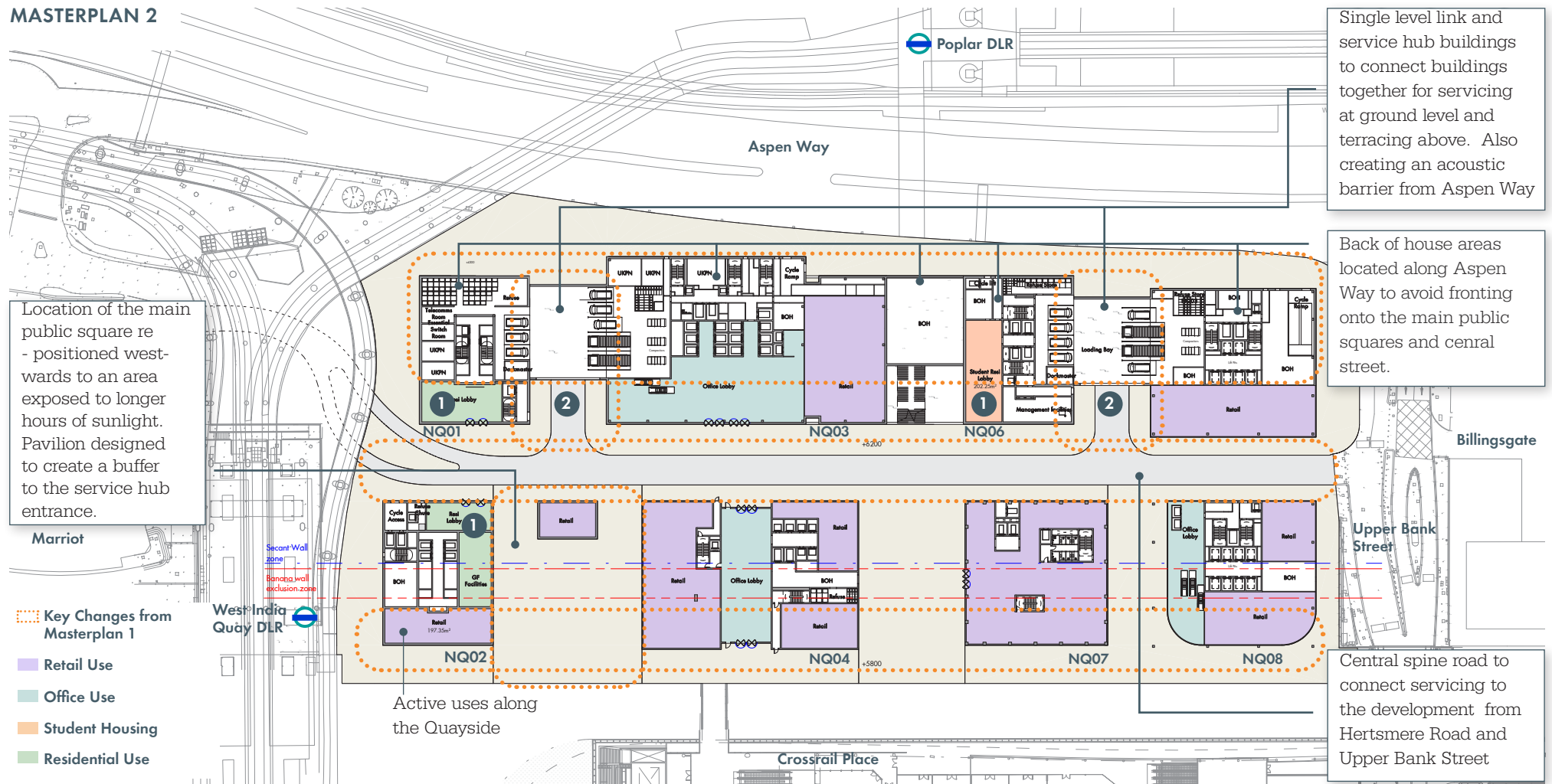
Next Steps

Following this initial and positive feedback, further site plan analysis was undertaken and 3d massing studies were developed and tested for an emerging Indicative Scheme. Alternative massing scenarios were studied looking at the potential for different mixes of live/stay buildings and workspace buildings, in line with the client brief, to consider how this impacted on the urban relationships between buildings and on the massing composition as a whole.

Eye level sketch perspective views showing the relative scales of routes and open spaces were developed and evaluated using a 3d design model. This helped to inform LDA Design who had been appointed to develop their initial landscape strategy for the Masterplan alongside Allies and Morrison.



MASTERPLAN 2



Ground floor plan illustrating how the masterplan has evolved since the first pre application meeting (original naming convention)

JANUARY 2020

November 2019: Further LBTH officer engagement

A further version of the masterplan was presented to LBTH and the GLA on 6th November 2019.

Allies and Morrison presented a design update. Reflecting and expanding on the discussions from the first Pre-Application meeting, the site diagrams had been developed towards an emerging Indicative Scheme that demonstrated three commercial buildings, two residential led tall buildings, one flexible tall building and two smaller buildings framing a central square.

1 Through the use of similar sized floor plates for the three tall buildings, the Architects showed how these plots might accommodate a range of interchangeable flexible uses including residential, serviced apartments and student housing.

2 An emerging ground floor plan was also presented showing two servicing hubs at ground floor level, located on the north side of the site and accessed from the central street.

FEBRUARY 2020

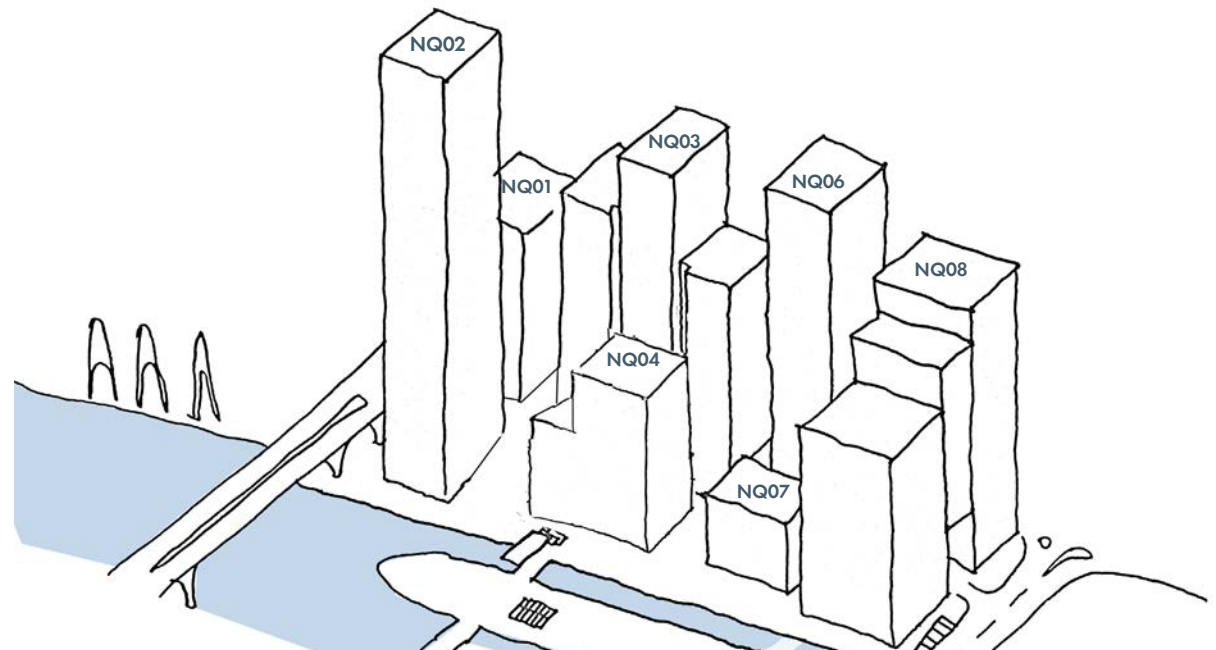
3d massing images were presented for the emerging Indicative Scheme demonstrating a range of building heights from 4 storeys to around 60 storeys across 7- 8 development plots. A series of alternative massing scenarios were also presented showing how flexibility, that could be built into the Parameter Plans and Development Specification, would allow some of these buildings to adjust in height, allowing flexibility for the future.

MARCH 2020

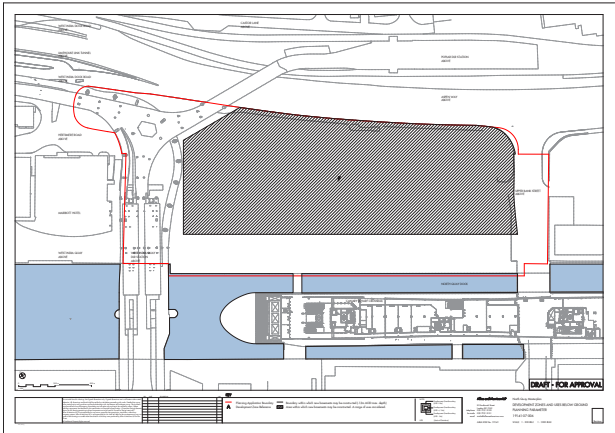
JULY 2020



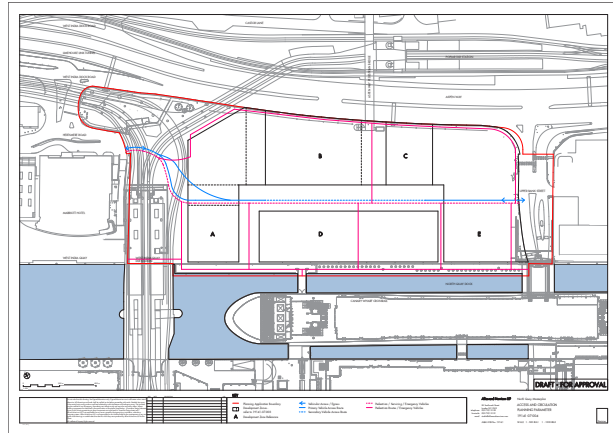
3d massing with flexibility built in for future use classes



3d massing with flexibility built in for future height variables (original naming convention)

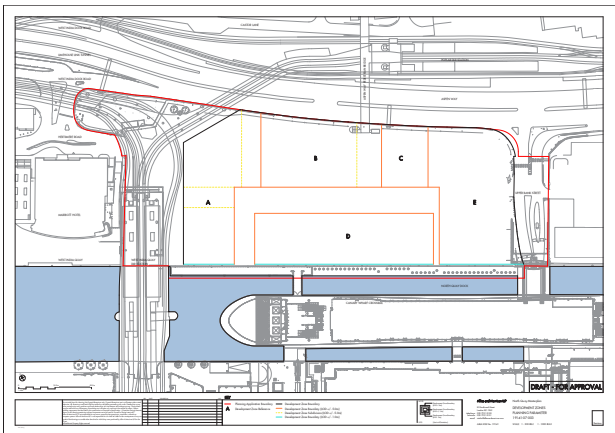


Early version of Parameter Plans - Development Zones and Uses Below Ground

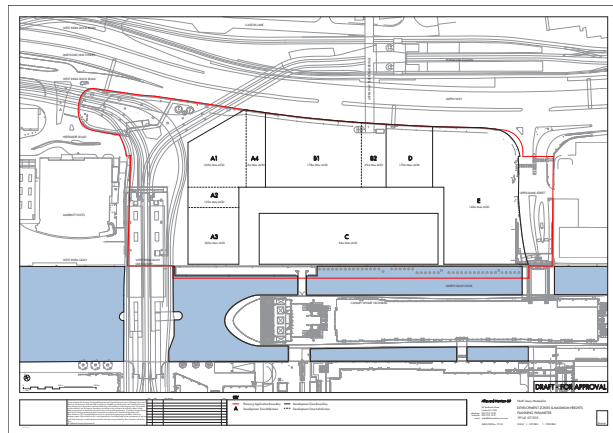


Early version of Parameter Plans - Access and Circulation

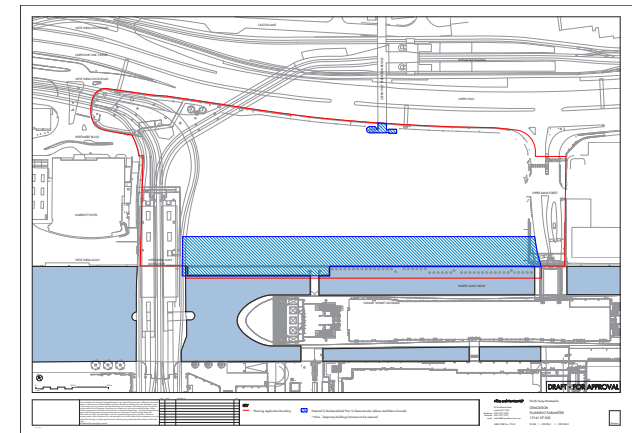
Early versions of the key Parameter Plans, which would act as a key control document in the OPA and in the design and development of future Reserved Matters Applications were also discussed.



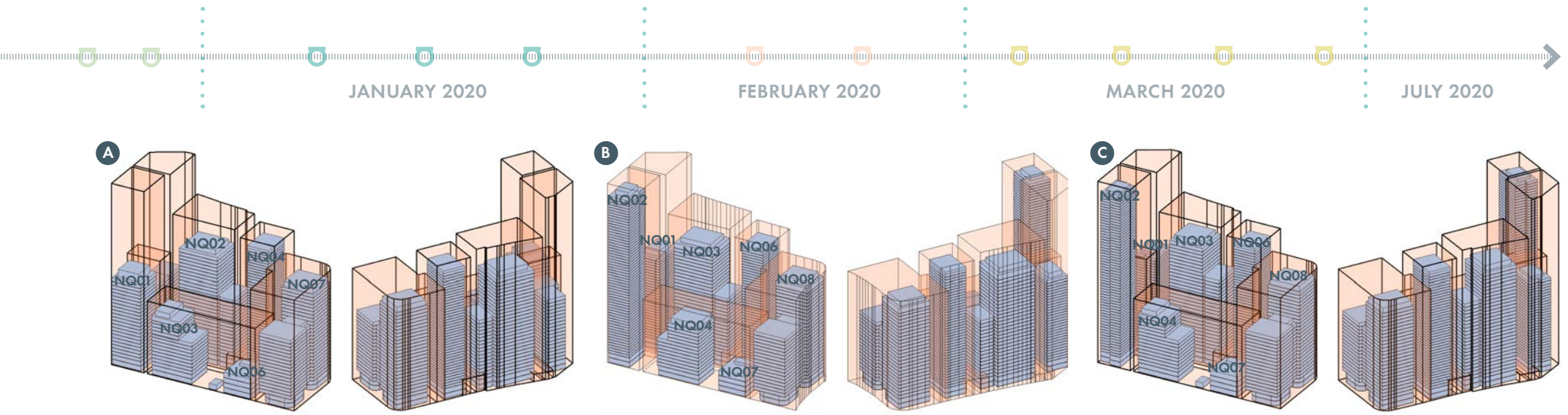
Early version of Parameter Plans - Development Zones



Early version of Parameter Plans - Development Zones Max Heights

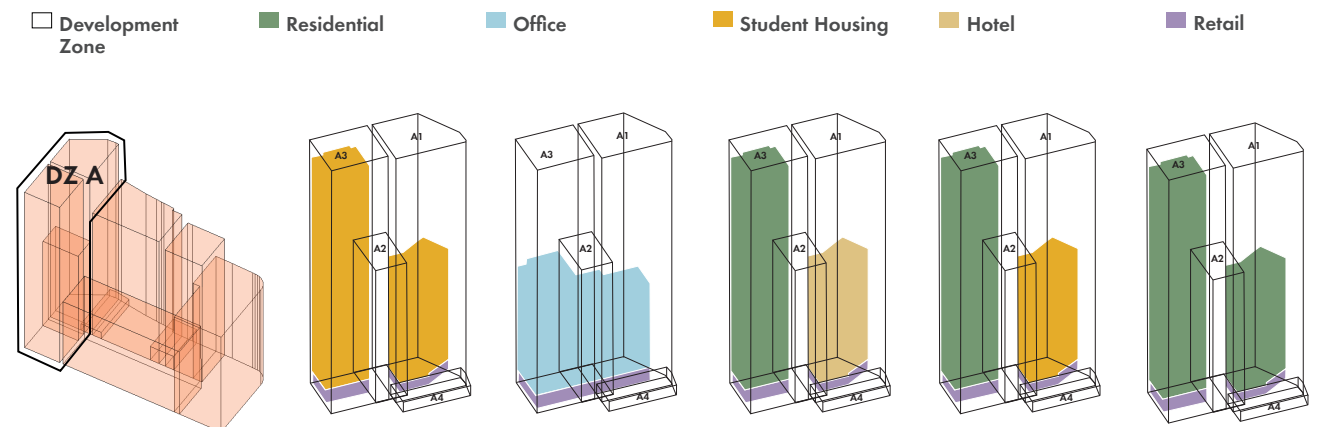


Early version of Parameter Plans - Demolition Plan

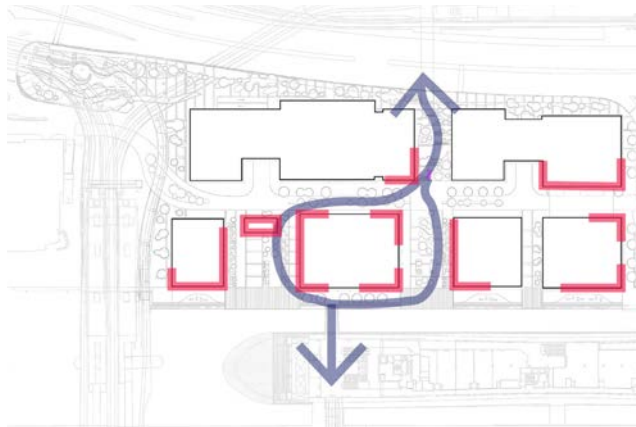


Three 'jelly mould' options (A, B and C) viewed from north and south, testing flexibility of varying Indicative Schemes for massing arrangements and heights adjustments

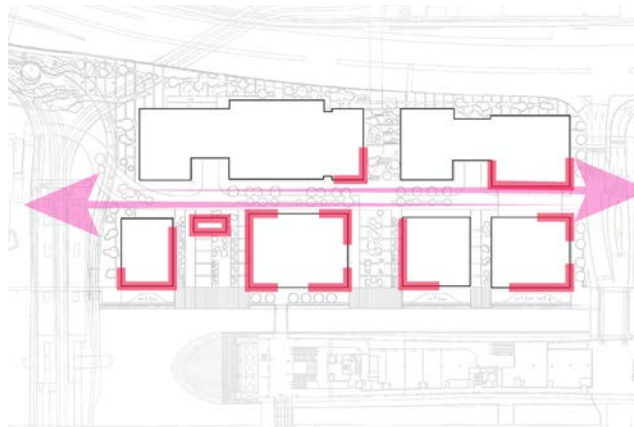
These diagrams demonstrate how a similar quantum of overall space can be accommodated in different ways within the 'jelly mould' maximum parameter envelope. The first two diagrams show a lower/denser mid rise proposal that has an increased office element to the scheme with an office building across NQ01 and NQ02. The remaining images show a more mixed use proposal that includes residential uses in taller buildings in NQ01 and NQ02, with NQ04 and NQ07 arranged in two different configurations.



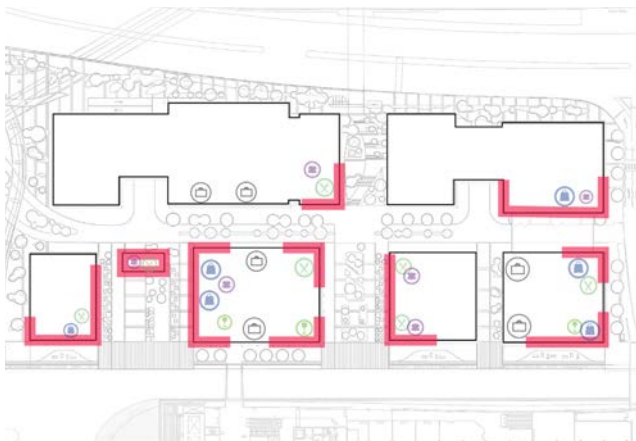
'Jelly Moulds' testing varying options and scenarios of massing heights and flexibility - an example shown for Development Zone A illustrated



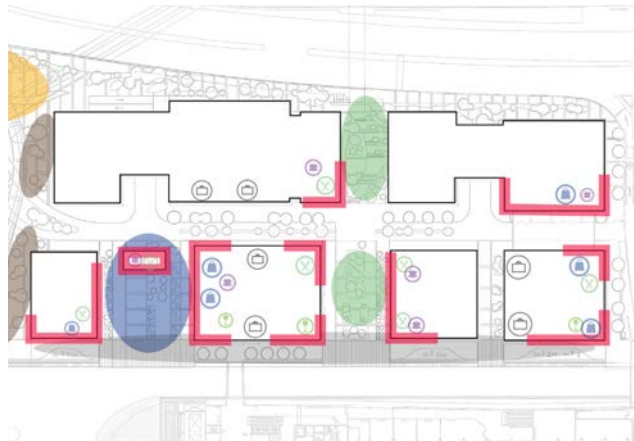
North South Connections



Shared spaces



Working with entrances and active frontages



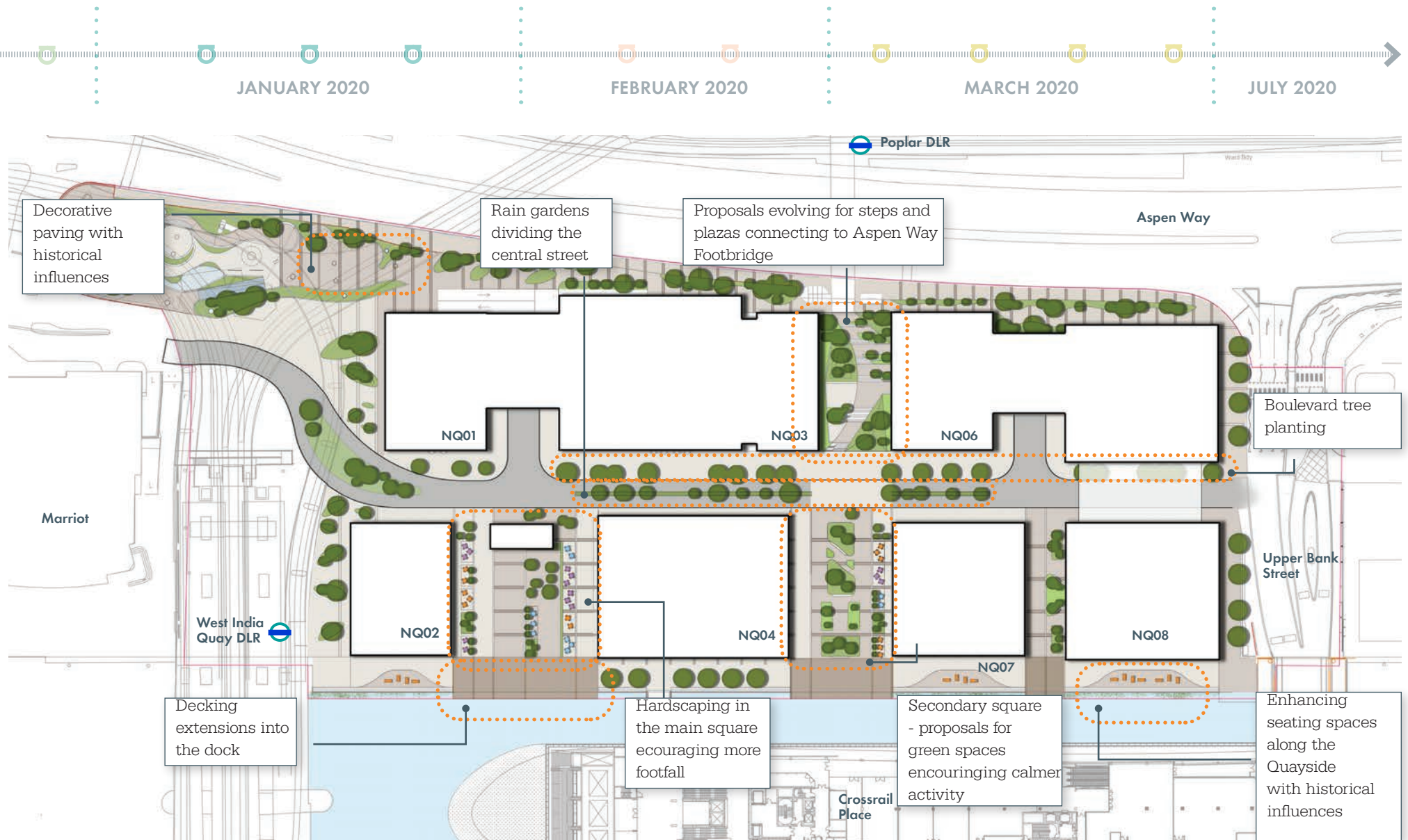
Creating public squares



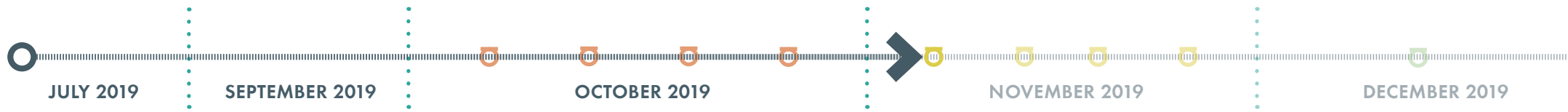
East west connectivity

LDA Design presented the initial landscape strategy for the Masterplan. This expanded on the needs for strengthening existing east-west and north-south connections that had been discussed at the previous pre-application meeting. It also looked at the opportunities of creating and enhancing spaces along the Quayside, along Aspen Way and under the DLR Delta Junction.

A series of principles to help inform and establish the emerging masterplan were presented. The key principles highlighted the importance of connecting Canary Wharf, North Quay and South Poplar through high quality public spaces. Also, the connection east - west along Aspen Way and the Quayside should be spaces to activate and celebrate.



Emerging landscape plan



View from Canary Wharf



View through the central street from Upper Bank Street



View along Aspen Way



View from the central street to the main square



View along the Quayside to West India Quay

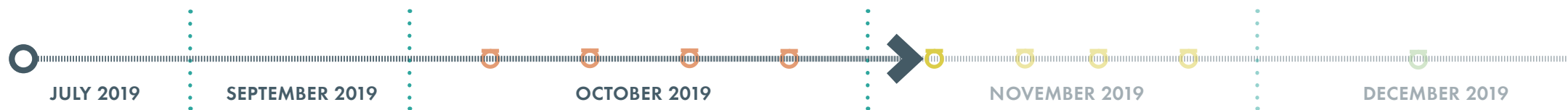
A selection of eye level 3d perspective views were also presented, generated from an emerging 3d model of the Indicative Scheme, showing the relative scales of routes and open spaces within the Site.



3D Aerial view of Masterplan 2 in the context of South Poplar, Canary Wharf, Wood Wharf and beyond

LBTH feedback on Masterplan 2:

- Should buildings on the eastern side of the site be higher given CAA height limitations and future development of Billingsgate?
- Further justification of the arrangement of open spaces is required. Aspen Way frontage is important.
- Consideration must be given to avoid any perceived 'wall of development' when viewed from Poplar.
- Concern raised about the legibility of the route in the SW corner near West India Quay DLR station and consideration should be given to a cut back.
- The Poplar route must be inviting.
- Is the dock edge route wide enough?
- What are the impacts of the back of house spaces and servicing entrances, potentially limiting active frontages and their effect on the character of public realm being created.
- It was agreed that opportunities of improving east-west cycle routes through the site would be explored further.



Greater London Authority feedback on Masterplan 2

Feedback from the GLA was broadly positive, with detailed comments received being consistent with points being raised by LBTH design and planning officers. The overall concept of a finer grain that connects Canary Wharf to Poplar was felt to have merit at this early stage.

Support was shown for the decision to prioritise activity at street level and allowing flexibility – these have most potential for creating a new vibrant piece of city. Support was given for the initial masterplan principles of plots and public realm as a single composition, and the need for buildings to work as a ‘legible ensemble’ – this will happen if a successful street network with practical plots is prioritised.

There was some concern about the current lack of clarity on the future character and status of Aspen Way, questioning will it always be solely a traffic corridor, or could it evolve to become more of a city street? The northern side of North Quay has an important role to play.

It was noted that the east - west route along Aspen Way and the bridge to South Poplar should be seen as key connections, along with existing/potential connections from other

bridges/stations/sites. It was made clear that improving connections across Aspen Way is a fundamental strategic objective – whilst public space at the pedestrian bridge landing from Poplar on North Quay has potential. It was also noted that the bridge has to be seen as a potential enhancement project in itself with Canary Wharf/New City College/TfL/LBTH all clear on shared objectives and responsibilities.

It was questioned whether there is further potential for public space to the west of the site that could complement and positively address North Dock and the DLR undercroft where this will receive afternoon sun.

It was noted that the public realm proportions seemed less than generous given the density of development - pedestrians should be absolute priority, and design code or guidelines for the street network should set out how this happens. It was further questioned whether the public spaces were in the right places and of the right proportions. They currently seem very regular, which risks some being used less than others. These should be reviewed in light of a clearer understanding of how people could/will use and move through each place. They may become different shapes and sizes as a consequence.

4th November 2019: Air Quality & Noise and Vibration Workshop

In November 2019 a design workshop was held with Allies and Morrison, LDA, CWG, Trium, AQC and Sandy Brown to discuss the air quality and noise implications across the Site and specifically at the Delta Junction, to ensure that the existing air quality and noise pollution levels were considered and that safe and useable spaces could be provided. Existing air quality monitoring data was used to provide a detailed information on existing pollution levels and was used to influence where playspace and outdoor space would be located in the Site.

Additional air quality and noise analysis was undertaken on the Delta Junction, with recommendations that this area should be targeted to older children where possible and mitigation measures suggested to reduce air quality and noise pollution including noise screening barriers, green walls and specific publicly accessible areas provided behind these structures (reducing adverse air quality and noise effects (with a potential reduction in noise levels of approx. 5 dB).

The initial air quality analysis also highlighted that residential units would most likely require mitigation if located at ground level and should ideally be located at level 1 and above.



13th November 2019: Consultation meeting with Transport for London

On 13th November 2019, an initial pre-application meeting was held with Transport for London (TfL). This included attendees from Docklands Light Railway (DLR).

Discussions were held around the draft London Plan and promoting sustainable transport modes, limiting car parking, enhancing cycle routes, and the quantum of cycle parking (short and long stay) to be provided in the Masterplan.

Highways and vehicular access from Upper Bank Street and Hertsmere Road were presented and discussed including emerging proposals to modify the junction of Upper Bank Street with Aspen Way. These were accepted in principle but it was noted that the Aspen Way interface would need to be consulted with RMS who operate Aspen Way for TfL.

Discussions were held between Steer and TfL officers on trip generation and strategic analysis/modelling methodologies to be used for the Transport Statement. Discussions were also held on requirements for DLR infrastructure protection.

14th November 2019: Consultation meeting with Historic England

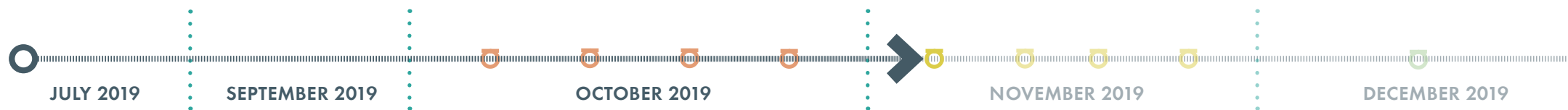
On 14th November 2019, an initial pre-application meeting was held with Historic England. A presentation was made explaining the new client brief and aspirations and also the existing site and its heritage, conservation areas and townscape context by Allies and Morrison and Peter Stewart (heritage and townscape advisor).

The emerging masterplan principles and massing proposals were shown, and discussions were had around the proposed flexibility sought through the OPA framework.

A townscape discussion was held around the site in the context of the London Views Management Framework (LVMF) and with 4 initial wireline local views looking at the emerging Indicative Scheme massing proposals alongside the previous 2017 application massing.

Discussions were also held around the proposals for the protection and stabilisation of the listed dock 'banana' wall and building structures spanning over this.

The meeting was positive and engaging and there were no concerns raised by Historic England.



November 2019: Public consultations

Three public exhibitions were held in early November 2019 in different locations to the north, south and west of the Site, as well as a dedicated session held at Poplar Shahjalal Mosque. Over 150 people attended the events in total. The majority of attendees were local residents.

74 people provided written feedback. 69 completed a form at an event with an additional 5 community stakeholders providing feedback via a survey online. The key themes that emerged from these consultations included:

Public spaces - The need for attractive public spaces was raised more than any other issue in the written feedback - safe spaces, green spaces and spaces next to the dock. There was a strong view that spaces should feel welcoming, inviting and inclusive.

Connections - Many people could see the benefit in improving the connections through North Quay to Poplar as well as improving the east-west integration of the Site. It was generally considered that the existing Poplar Link Bridge needed to be improved to make the link more appealing.

Family friendly activities - Activities or places for children and families to visit was high up the agenda. Family friendly public spaces were also mentioned.

Workspace - Where workspace was referenced it was regarding the need for more variety, with smaller offices, flexible set ups and co-working spaces all suggested so that smaller businesses can benefit from the proximity to Canary Wharf.

Housing - There were mixed views on the need for more homes.



November 2019 Public Consultation boards



Photo from public consultation on 5th Nov 2019 - Marriot Hotel



Photo from public consultation on 7th Nov 2019 - Ideas Store

JANUARY 2020

Leisure - 39% of respondents also considered leisure to be a priority for the area.

Retail - The need for retail received mixed views, although the majority (55%) felt that it would be of benefit to the area.

Pedestrian and cycle infrastructure - Existing pedestrian and cycle infrastructure around the Site is currently considered to be poor. People are keen to see improvements here in terms of accessibility and safety.

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Character - Feedback on character related to the opportunity for North Quay to step down in scale to respect the context of Poplar, the need for the development to feel inclusive, and to make more of the history of the docks in the design approach.

Community spaces / venues - Similarly, there were a number of suggestions for facilities that would serve the community, such as affordable spaces to hire and cultural facilities. Poplar Union was mentioned as a reference.

MARCH 2020

Events and attractions - Suggestions for live events and attractions received a high percentage of comments, with the view that they could improve social links in the area and provide a reason to visit North Quay.

Training opportunities - Access to training courses relevant to local people was raised by both the public. There was a suggestion for links with New City College.

Consultation notes to also be referenced alongside the SCI, report number NQ.PA.14.

JULY 2020



Photo from public consultation on 7th Nov 2019 - Ideas Store



Photo from public consultation on 7th Nov 2019 - Ideas Store



Photo from public consultation on 9th Nov 2019 - St Matthias Community Centre



MASTERPLAN 3



Ground floor plan presented to LBTH in early December - key changes since Masterplan 2

JANUARY 2020

December 2019: Design evolution following feedback

On 6th and 20th December 2019, two further pre-application meetings were held with LBTH design and planning officers and a presentation was made to the LBTH Executive Mayor on 17th December 2019. Following feedback from the consultation meetings and public exhibitions held through November 2019, further design work had been undertaken and was discussed.

On 6th December 2019, the design team presented a revised Indicative Scheme with further study on the pedestrian route to and from Poplar, a design study of the Quayside area and further study on the range of open spaces provided within the Masterplan.

Significant design development had been made since the previous pre-application meetings, taking on board comments and concerns raised by LBTH, GLA and TfL. Strategic changes had been made to adjust the configuration of the proposed building plots, to adjust open space areas and to revise the servicing strategy to respond to various points made during November 2019. Key changes were made to the proportions of the Quayside and Poplar Plaza and their interconnectivity through the central main open space.

FEBRUARY 2020

1 Revised Servicing strategy

Following comments about the quantum of back of house space at ground level, a revised servicing strategy was developed and presented that was based on using basement level loading bays in lieu of ground level loading bays. Accessible from a ramp at the west of the Site from Hertsmere Road, this significantly reduced the amount of back of house space at ground level and was broadly supported as a preferred strategy.

2 A legible network of connections and spaces

The relocation of the two lower buildings either side of the central square (NQ04 and NQ07 at that time) together with the flipping of the office and live/stay buildings (NQ06 and NQ08) create a much stronger position and scale for the central urban square relative to the key route from Poplar pedestrian bridge through to Crossrail Place bridge.

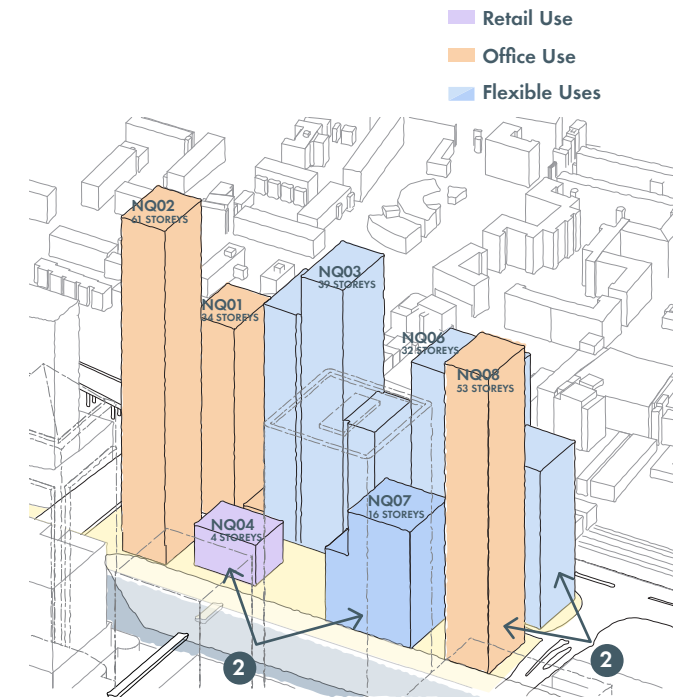
3 Together with the widening of the Quayside, each of the routes and spaces become much better proportioned relative to the nature of their place and use within the masterplan. The more generous central urban square becomes a key connector between Poplar Plaza, the Quayside and Crossrail Place. A larger and smaller garden square created either side

MARCH 2020

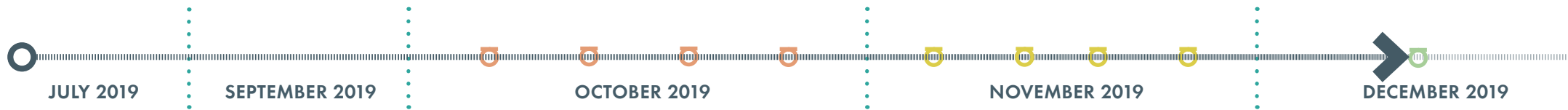
JULY 2020

become more intimate secondary spaces. The Quayside widens where it is sunniest, overlooking the dockside and also most busy in terms of feeding people into West India Quay DLR and the Elizabeth Line at peak times.

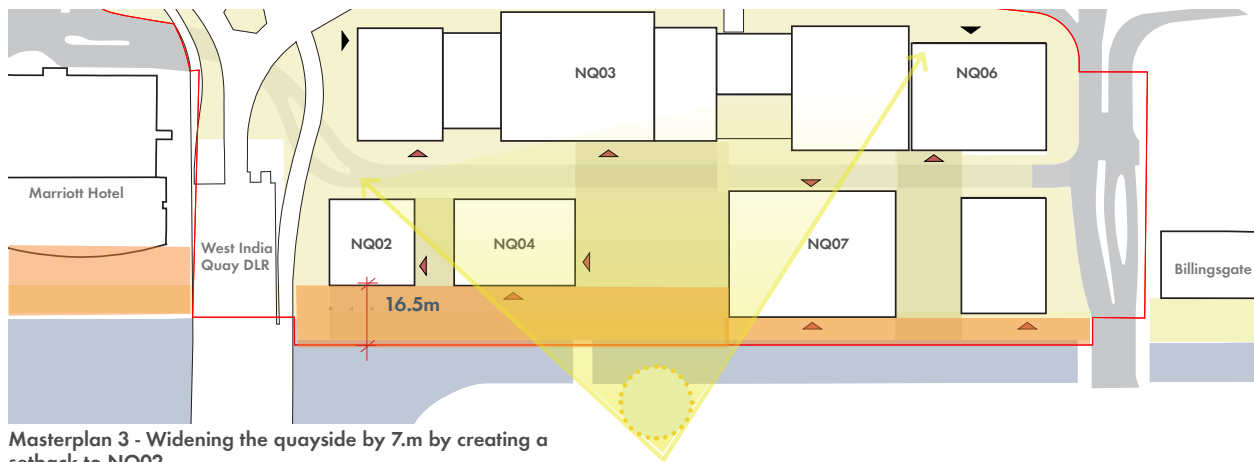
4 Together with the revised servicing strategy, a much enhanced and activated public realm offer is created as a result.



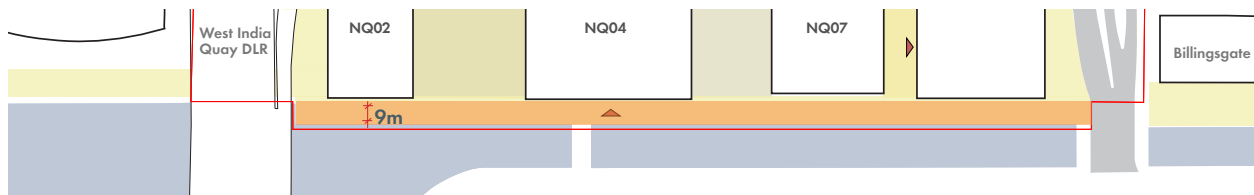
Relocation of buildings - Masterplan 3



Masterplan 3



Masterplan 3 - Widening the quayside by 7m by creating a setback to NQ02



Masterplan 2 - Quayside layout

5 Widening the quayside

Following a study of the Quayside area, the tower closest to West India Quay DLR was set back at ground/first floor level to open up long views through to West India Quay beyond and the alignment of some of the other buildings was adjusted to provide a more generous public open space adjacent to the water. This was also informed by overshadowing and sunlight input from GIA.

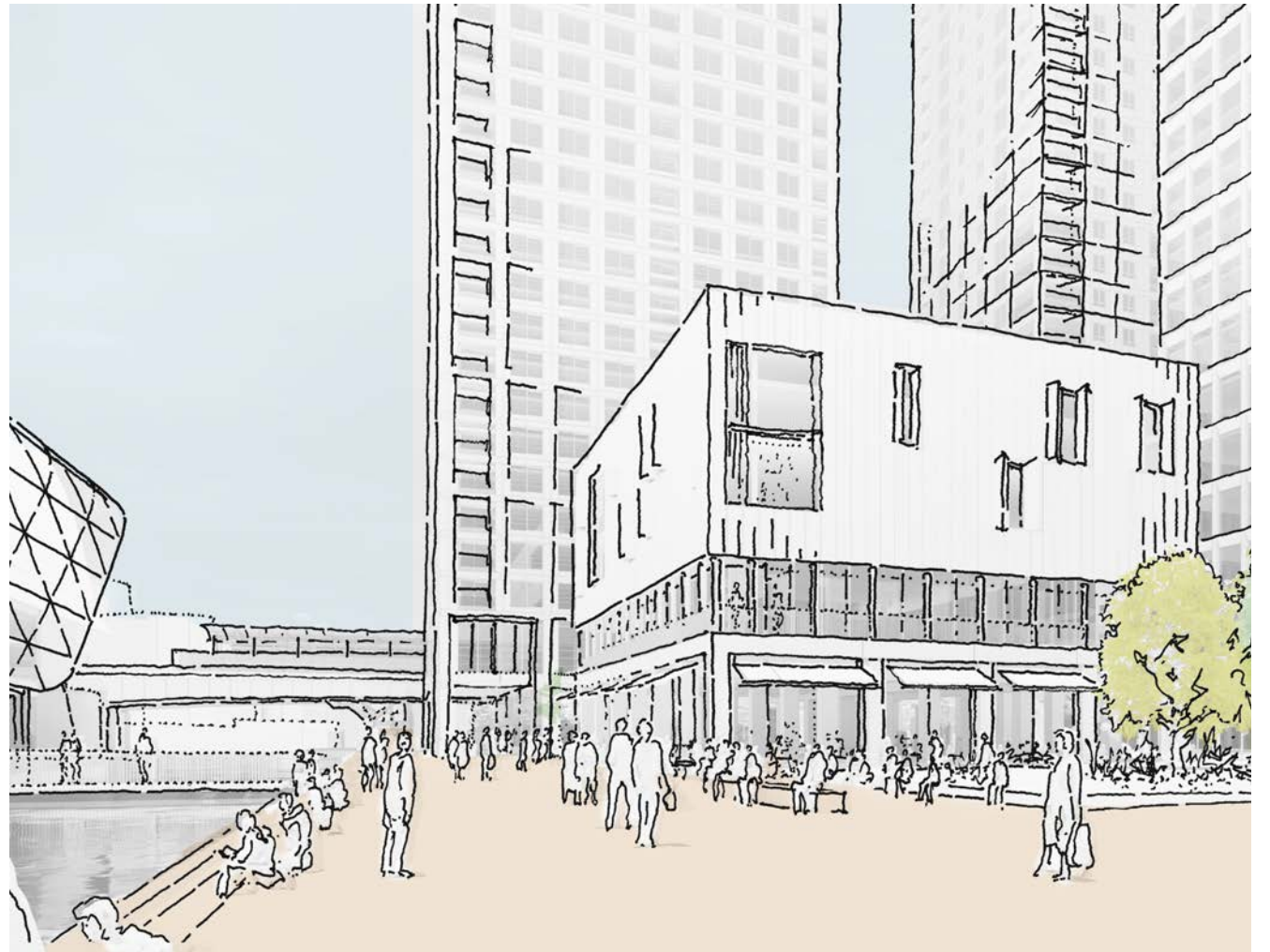
Further eye level views along the Quayside and within North Quay Way demonstrated improved legibility and generosity of the key open spaces in the Masterplan.



View from Crossrail Place



Widening the quayside - opening up the view from West India Quay



Widening the quayside - View to West India Quay



Key summary of the third design presentation to LBTH:

- Queried why the North Quay Square had moved back to centre from the west.
- Concerns were raised regarding the arrival to North Quay from the Aspen Way Footbridge and the positioning of NQ03.
- It was agreed that Poplar Plaza should be a place to dwell rather than just pass through.
- The route from Poplar to Canary Wharf must be better understood.
- Confirmation of the appropriateness of Delta Junction for the uses proposed was sought.
- A general agreement on massing, although control to avoid a “wall of development” on Aspen Way would be required.
- Further clarity on the extent of flexibility in terms of massing and land use was sought.

Pre-Application LBTH Mayor Presentation 17th December 2019

On 17th December 2019, an initial pre-application meeting was held with the LBTH Mayor.

Feedback from the Mayor was broadly positive, with detailed comments received in regards to the quality of the spaces near to Aspen Way.

Key summary of the presentation to LBTH Mayor:

- Queried what was the impact of increased footfall from Poplar DLR - is the footbridge suitable and are there any plans to improve this connection?
- Concerns were raised over the traffic along Aspen Way and the impact on air quality around the Delta Junction.
- What measures will be put in place to protect the Delta from an accident coming off Aspen Way.
- Could this OPA be a trigger for LBTH to improve the air quality and reduce emissions from vehicles on Aspen Way.

Air Quality & Noise and Vibration Facade Workshop December 2019

A façade workshop was undertaken in December 2019 with Sandy Brown, Trium and CWG following initial noise modelling of the Site and emerging Indicative Scheme which provided details on the likely noise reduction levels required by proposed façade materials and in particular in relation to windows and balconies.

This analysis was taken on board during design evolution to ensure suitable mitigation could be provided and thereby reducing adverse noise effects from this scheme evolution.



Wind Microclimate December 2019

RWDI undertook Computational Fluid Dynamics (CFD) modelling of a number of iterations of the Indicative Scheme to inform and aid technical wind microclimate feedback into the design.

This led to the inclusion of the following measures in the Indicative Scheme to create acceptable wind conditions throughout the Site; canopies above certain entrances predominately facing south such as the southern façade of NQ01, NQ02 and NQ04; 1.5m baffles to the southern facade of NQ07; and additional landscaping including trees to mitigate down washing and corner wind acceleration particularly to the west of the Proposed Development including to the west of NQ01 and NQ02.

Daylight, Sunlight and Overshadowing Analysis December 2019

GIA ran analysis on a number of iterations of maximum height parameters and Indicative Schemes to provide input on potential effects to sensitive surrounding receptors as well as internal daylight and sunlight levels.

This analysis fed into the most suitable locations of building massing, public realm and open spaces.

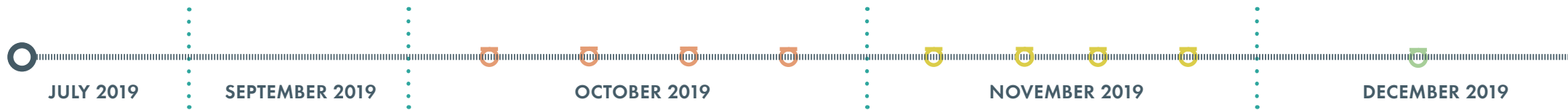
The main square in the middle and south of the Site was of particular focus with NQ04 being reduced in height to improve sunlight levels to this area. In addition, cruciform building structures were assessed as part of the evolving Indicative Scheme for NQ.01 to improve internal daylight and sunlight levels for residential dwellings.

Townscape and Heritage December 2019

Peter Stewart Consultancy provided input into the evolving Design Guidelines and Parameter Plans to ensure relevant codes were included to control the maximum building heights and articulation throughout the development plots.

This input included Design Guides on establishing minimum differences in height between live / stay buildings, minimum horizontal separation between all buildings, a code committing to high quality architecture and setting general locations and minimum widths of key routes throughout the Site.

These Design Guidelines or 'controls' form part of the Control Documents.



MASTERPLAN 4



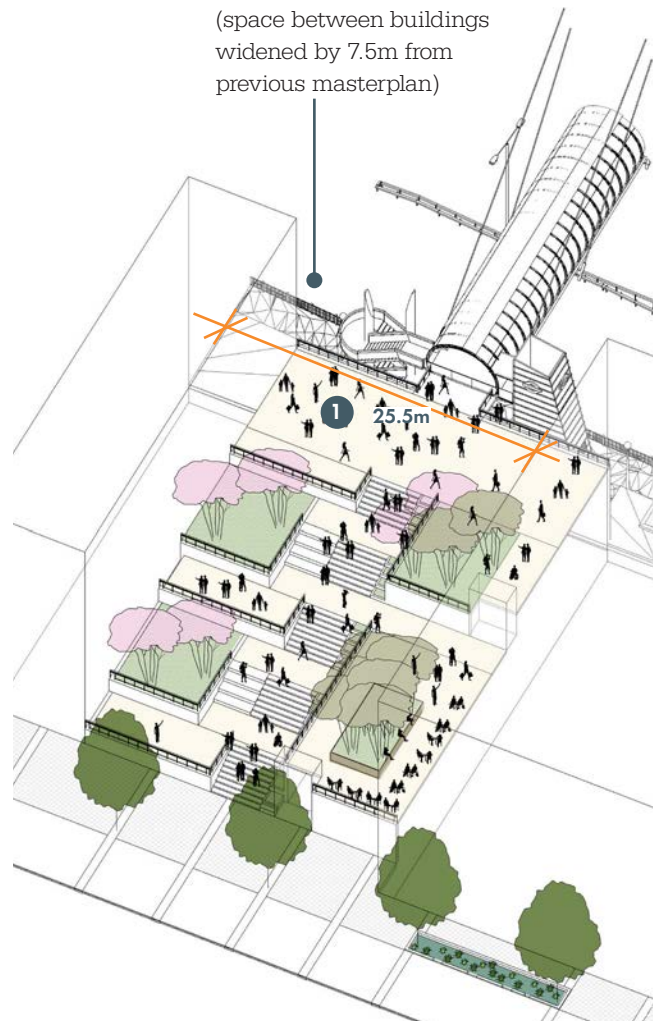
Masterplan 4 - Roof plan presented to LBTH in late December

Pre-Application Meeting 20th December 2019

At the pre-application meeting on 20th December 2019, Allies and Morrison presented a further update on Poplar Plaza and its alignment relative to the footbridge and the view to this from Poplar High Street, were the City College extension to be removed as part of their redevelopment proposals (outside of this OPA).

- 1 The Indicative Scheme was further widened and the emerging open space on arrival from the Aspen Way Footbridge repositioned, termed 'Poplar Plaza', to land the footbridge more centrally and in a more generous space that encouraged people to dwell as well as move through.

Discussions were held around the options of securing potential improvements to the existing Aspen Way Footbridge.



3d model showing the plaza, lifts and steps transitioning from the footbridge

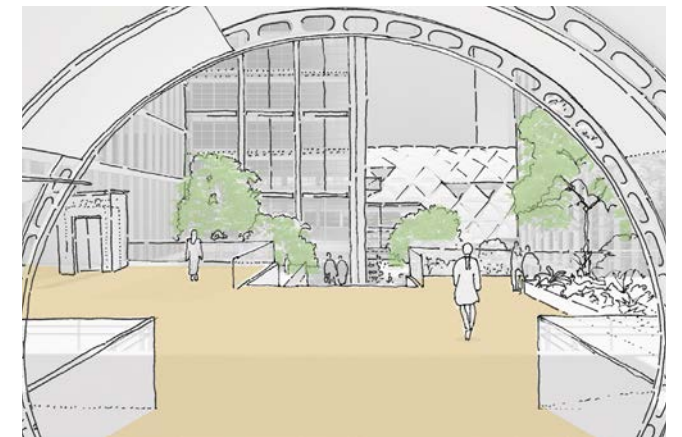


Sketch view from the exit of the bridge to the Crossrail station access

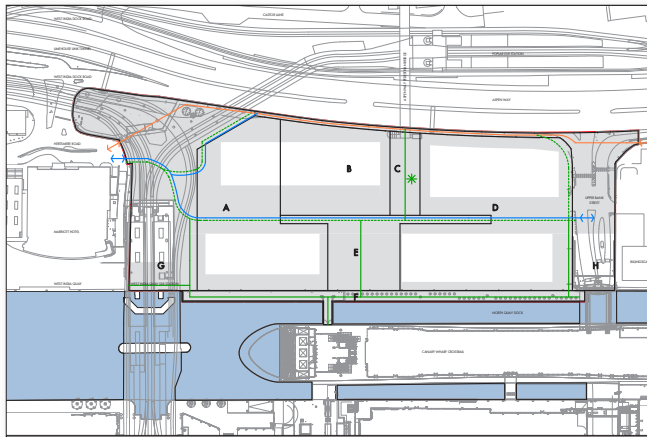
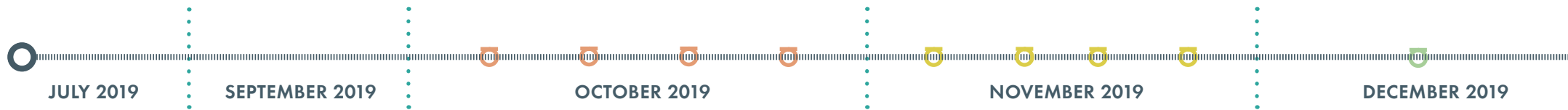
A series of eye level 3d views were produced demonstrating the improved legibility of the pedestrian route including one view from Poplar High Street showing the potential positive impact were the New City College extension to be removed and a wider route be created, allowing the creation of a long ramped route to the Aspen Way footbridge from Poplar High Street.



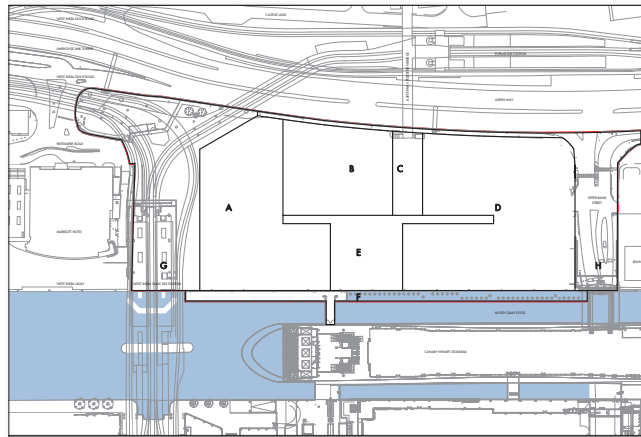
Impact of New City College extension removal from Poplar High Street



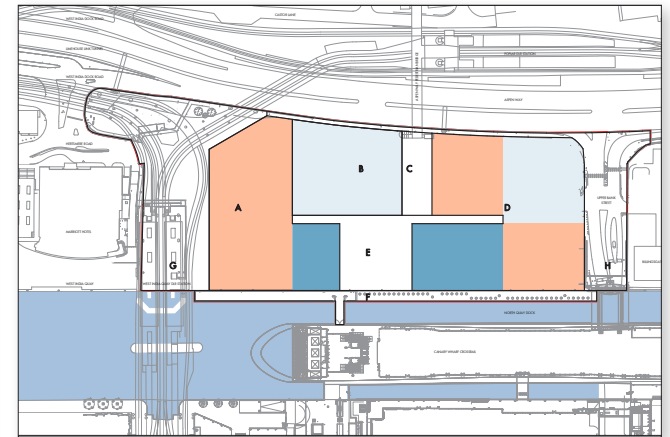
Sketch of improvements to relationship of Poplar Plaza & the footbridge



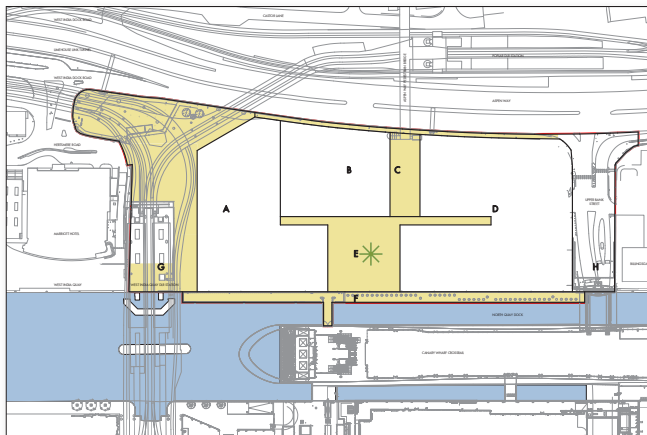
December development of key parameter plans - Access and Circulation



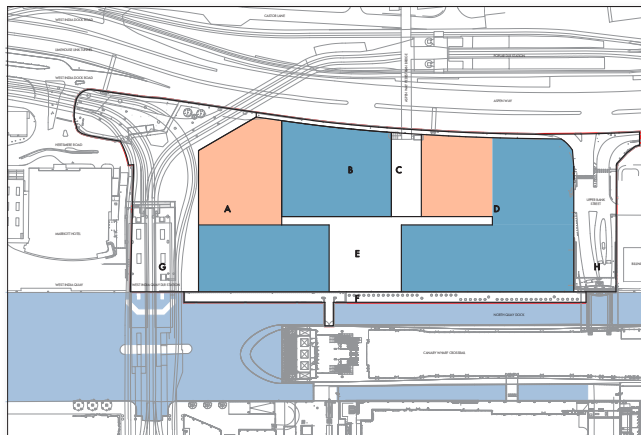
December development of key parameter plans - Ground Level Uses



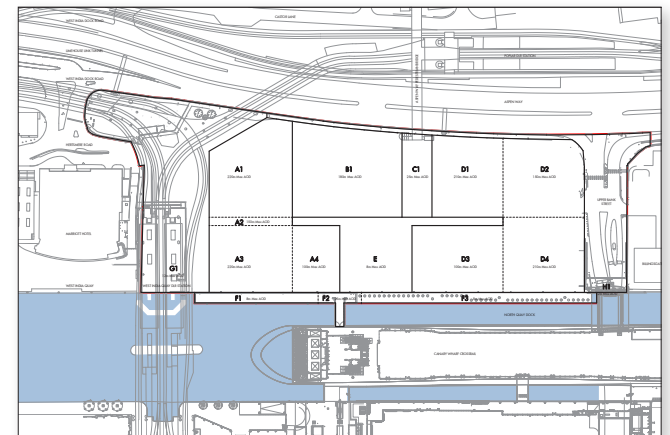
December development of key parameter plans - Upper Level Land Uses



December development of key parameter plans - Public Realm



December development of key parameter plans - Ground Level Uses



December development of key parameter plans - Maximum Heights



Further development was also presented and discussed on the draft Parameter Plans allowing discussions on ways in which uses, open space, routes/connections, extents of basements and building heights could be controlled whilst allowing appropriate flexibility for the future in line with the Design Principles.

Having presented the plans a number of times, the comments by this stage were mostly on the details rather than broad principles however LBTH comments on the draft Parameter Plans did include:

Access and Circulation Plan:

An option be included on the Parameter Plan in the event that residential/student development came forward to maintain a route between NQ02 & NQ04; and NQ07 & NQ08.

Public Realm Plan:

To consider how 0.4ha open space target is captured on Parameter Plan, especially along the dockside.

Development Zones Plan:

Though accepting the potential for build over of the central street in Zone A, there was still some concern about how to control build over in Zone D.

Upper Level Uses plan:

LBTH questioned the extent of “Any Permitted Uses” areas in Zone D.

Ground Level Uses plan:

Plan to be reviewed to determine how to incorporate active frontages through proposed uses.

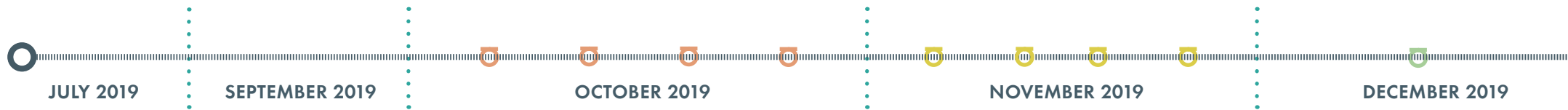
Maximum Heights plan:

LBTH questioned why Zone A4 was set at 100m AOD given that it was proposed as a 4 storey building. They stated that 180m AOD was acceptable across Zone B1, however the Design Guidelines would need to strongly control how the shoulder of the building would come forward.

On the whole, this was a very positive meeting. Minor comments were made on the potential for alternative options for the lifts and stair transition within Poplar Plaza and ways in which the Design Guidelines would capture some of the key details and principles discussed through recent pre-application meetings.

Key summary of the fourth design presentation to LBTH:

- The realignment of the footbridge to land centrally in Poplar Plaza was positive.
- A minimum width should be determined for Poplar Plaza in the Design Guidelines
- Preference for a single lift alternative option to be demonstrated to transfer from the bridge level to ground level. Information on queuing and capacity should be provided.
- North Quay should create a condition for Aspen Way which TfL would need to take into account when considering future proposals for the road
- Preference for the security hut in the proximity of Delta Junction to be positioned as far as possible from NQ01/NQ02 in order that its impact on pedestrian movement was limited.
- Indicative proposals for improvements to the Aspen Way Footbridge should be suggested. This could include looking at how the entrances to the bridge could be altered.
- Positive response to buildings NQ02 and NQ04 moving further away from the dockside. Double storey height underneath the NQ02 colonnade which would rise above the DLR tracks was acceptable



MASTERPLAN 5

NQ01 change to building footprint after daylight/sunlight recommendations



Masterplan 5 - Ground floor plan presented to LBTH in January (original naming convention)

JANUARY 2020

January 2020: Further design development of the Masterplan

Further design development during this period was mostly focused on the Indicative Scheme landscape and public realm design and environment. Detailed studies were undertaken and presented on each of the open spaces and routes in and through the Masterplan. This led to the development of two 3d movies showing kinetic movement and the changing experience whilst walking through the Site from Aspen Way Footbridge to Crossrail Place and along Aspen Way towards The Delta. This proved a very useful tool for the consultations with SDC and CADAP as well as to assist further design development.

Informed by a review of the 3d kinetic videos, a number of detailed refinements were also made to the Indicative landscape masterplan. These included the landscaping zones along Aspen Way Gardens and the design of The Delta, located under the elevated DLR tracks. Input from technical consultants on transport, acoustics and air quality helped to inform the evolving illustrative landscape proposals and the configuration and locations of areas for child play. A study was also undertaken on the pedestrian capacity of the existing Aspen Way footbridge and potential enhancements that could be made.

FEBRUARY 2020

MARCH 2020

JULY 2020



Aspen Way Footbridge



North Quay Way



Aspen Way



Poplar Plaza



Quayside



The Delta



Animation captures moving through the Site enabled a great presentation tool and opportunities to make detailed refinements to the masterplan



Presentations to SDC and CADAP

Presentations were focused on this version of the masterplan to LBTH Strategic Development Committee on 23rd January and the Conservation and Design Advisory Panel (CADAP) on 27th January 2020.

Feedback from the CADAP members was positive and they welcomed the opportunity to review the emerging proposals.

The Panel wanted to ensure that the development would successfully integrate with neighbouring developments, including emerging and possible future developments. They agreed that the redevelopment of North Quay presents an excellent opportunity to create a better link between the residential communities in Poplar to the north of the site, with the commercial development at Canary Wharf to the south.

The importance of successful landscape design was raised. The Panel stressed the importance of designing the public spaces so that they can be enjoyed by both visitors and residents including sufficient space for children, young people and students. The Panel suggested that the landscape design might also include references to the former docks and recommended that a public art contribution is included, perhaps through some form of historic interpretation of the docks and

celebrate the significance of the site heritage. Precedent landscape images presented to the panel can be seen on the following pages provoking much feedback.

The Panel were encouraged by initial proposals for the use of the space under Delta Junction. In particular, they were pleased to see ways to redevelop this space for the benefit of residents. They also stressed that the space would need to be well managed and could potentially include kiosks.

The Panel noted that different land uses would give rise to different public open space requirements and that the open spaces would need to be flexible as well, to respond to different development scenarios. They suggested that the applicant may wish to commit to a fixed proportion of different uses in the outline permission, to give greater clarity about the type of place that would be created. The Panel also thought it might be appropriate to include more than one Indicative Scheme as part of the OPA.

Feedback from SDC was also very positive and they welcomed the opportunity to review the emerging proposals, especially as North Quay is a very important Site. They did mention that due to the nature of the LBTH process, all of the schemes within the area (North Quay, New City College, The Workhouse and DLR Depot) were not currently being reviewed in

unison and this could be a concern. They were however impressed that Poplar was frequently mentioned within the presentation and felt like the wider area was being properly considered. They could visualise that North Quay would help to "move" Poplar into Canary Wharf.

They wanted to see an improvement in north-south cycle connections over an upgraded Aspen Way Footbridge. The north-south connection would benefit from a provision of cycle ramps to enable bikes to be wheeled over the various level changes.

The panel liked that North Quay was a mixed use scheme and understood the flexibility required. They did not see an issue generally with residential buildings on Aspen Way, however they would not like to see family units placed on the lower levels of NQ01. The panel asked what the proposals for social housing within the scheme would be. The Client stated that the affordable housing proposals had not yet been settled given the stage of the scheme and that decisions were still being made on the overall residential proposals. They noted that they were however aware of the council's affordable housing requirements and would be looking to follow the council's unit mix requirements in the social rented tenure. They noted that 35% affordable housing was a minimum target.

JANUARY 2020

FEBRUARY 2020

MARCH 2020

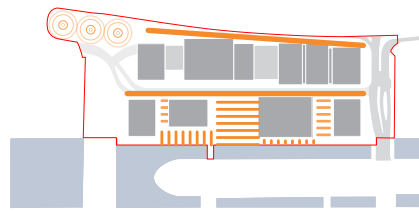
JULY 2020



Precedent image for Aspen Way Gardens, planting and seating areas



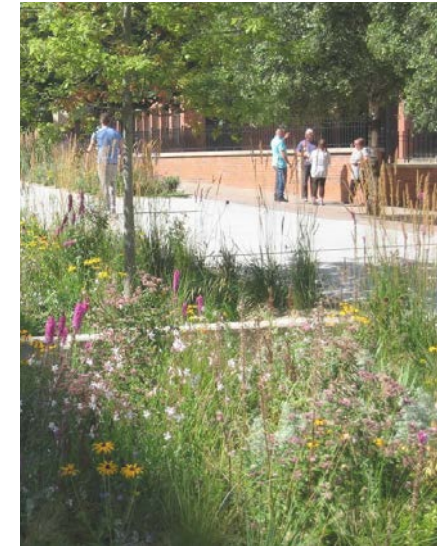
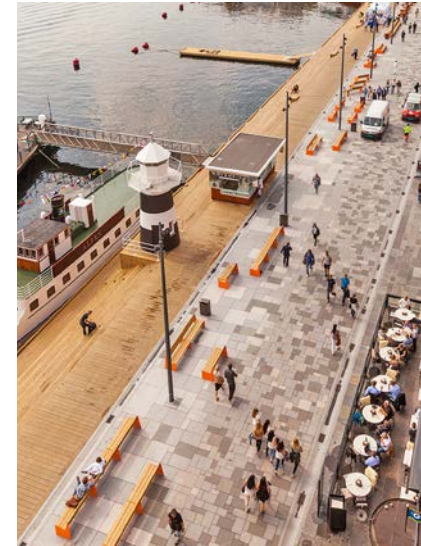
Illustrative plan of the Delta



Location plan of these precedent images and their connecting spaces



Quayside precedent images showing public realm along the waters edge



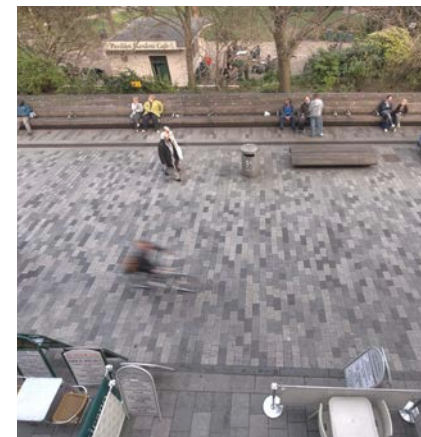
Planting and colours along Aspen Way



Precedent image of an undercroft space similar to The Delta



Tree planting precedent image provided along the North Quay Way



Precedent material palette for the North Quay Way



Aspen Way cycle path and pedestrian route seating and planting precedent image



Further Pre-application meeting, 30th January 2020

On 30th January 2020, a further pre-application meeting was held with LBTH planning, design and transport officers. Discussions at this meeting focused on the developing Indicative Scheme landscape and public realm design and environment.

Allies and Morrison provided an overview of the North Quay Way and updated site access parameters and explained that the principal access, including for servicing, would be from the west though vehicles could use the eastern access from Upper Bank Street, though likely to be limited to drop off. The North Quay Way would then be pedestrian and cycle friendly, with a Santander docking station at Delta Junction and limited parking, mainly for disabled residential units.

An update on the connection to South Poplar was discussed including the existing bridge width and potential for enhancement. On the North Quay side in the Indicative Scheme would be a series of steps with a two-stage lift and the plaza area could double up as dwell space because it is south facing. It was agreed that an option would also be produced to show a single twin lift transition of the levels.

Further discussions were also held on the Development Specification area tables, residential mix and securing the provision of the minimum 0.4ha of open space defined in the LBTH Site Allocation.

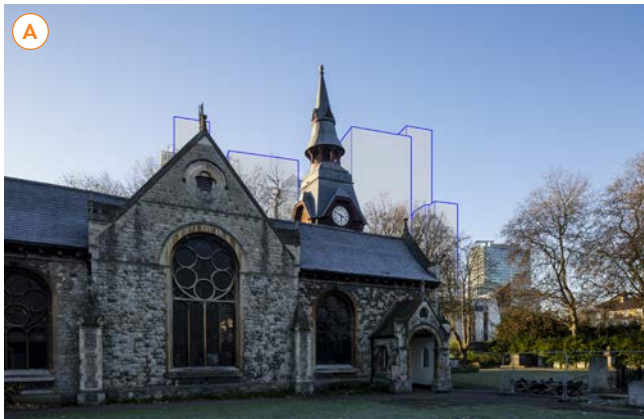
Further work was also undertaken in this period to assess the emerging Indicative Scheme in the context of a number of local townscape views with input from the townscape consultant. Wireline massing views were created within photographs taken from St Matthias churchyard (A) and open spaces around Poplar High Street (B and E), Trinity Gardens (C) and All Saints Churchyard (D). These helped to establish the potential visual impacts of the scheme from these key open spaces within the local community. These then helped to inform discussions with LBTH around the proposed heights and massing of Development Plots and relationships with the existing buildings within Canary Wharf beyond.

Feedback from sunlight/daylight and acoustics consultants (GIA and Sandy Brown Associates) led to refinements of residential building layouts in the Indicative scheme and adjustments to the maximum parameter envelope. The assessment of early layouts for NQ01 reported some apartments with reduced

levels of daylight, which led to a redesign of the building shape. A cruciform plan layout was produced and refined to enable additional daylight to enter individual apartments and improve the number of dual aspect apartments, which improved results and therefore informed GIAs further reporting in the OPA.

LBTH officers confirmed they were broadly satisfied with the height strategy for the Indicative Scheme but are concerned about how alternative massing scenarios within the parameters have the potential to create a wall of development if not properly controlled. It was agreed that A & M would show an alternative Indicative Scheme to position the tallest buildings on the northern edge. This alternative Indicative Scheme would then be tested in the key views (to also include additional ones from Ben Johnson Bridge and Battle Park) so these can inform Design Guidelines.

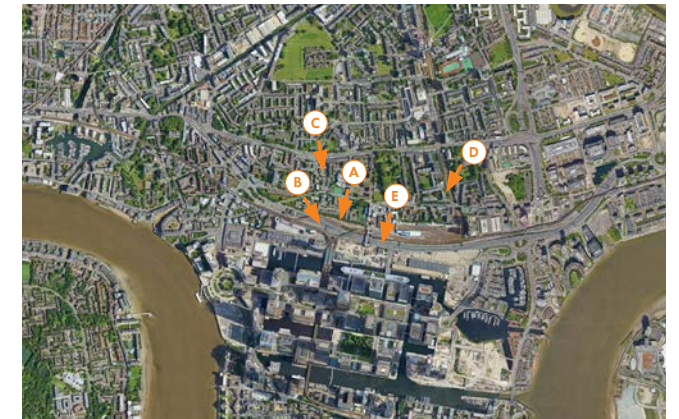
Officers queried whether it was appropriate to maintain flexibility for a single building over the road next to Upper Bank Street for a large floorplate office. It was agreed that the Design Guidelines would need to provide clear guidance on the quality of the space beneath.



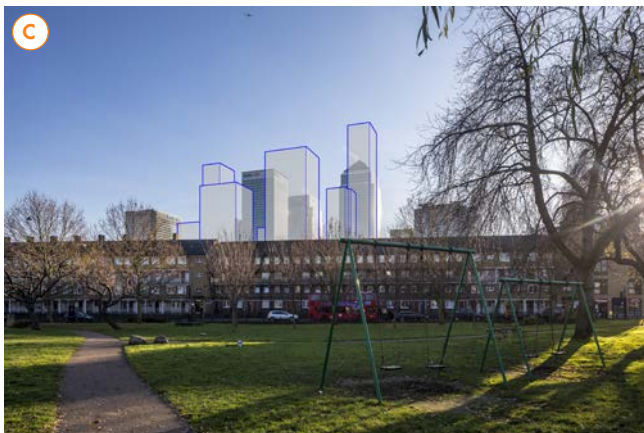
Wireline massing in view behind Church of St Matthias



Wireline massing behind Poplar High Street (west)



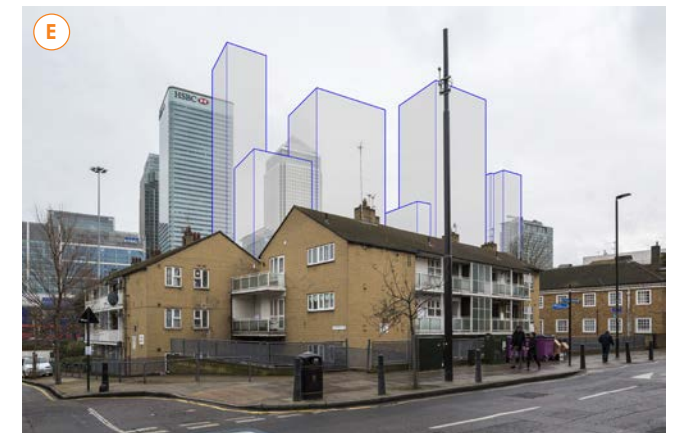
Location plan of verified wireline massing views



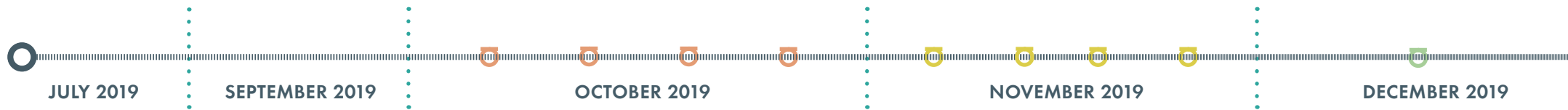
Wireline massing behind Trinity Gardens



Wireline massing in view of All Saints Churchyard



Wireline massing in view behind Poplar High Street (central)



February-March 2020: Development of the Parameters, Design Guidelines and Development Specification

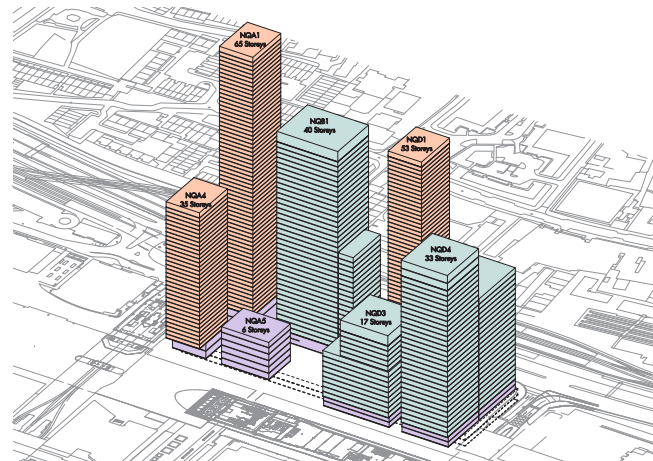
On 13th February 2020, a follow up meeting was held with the GLA to discuss the evolving masterplan design, landscape proposals and to present the emerging Energy Strategy being developed with Max Fordham. The meeting was again broadly positive, with officers pleased to see that design comments from the previous consultation had been taken on board through the design development.

On 20th February 2020, the team met again with LBTH officers to discuss Landscape, the final masterplan, Townscape, the draft Parameter Plans and Development Specification.

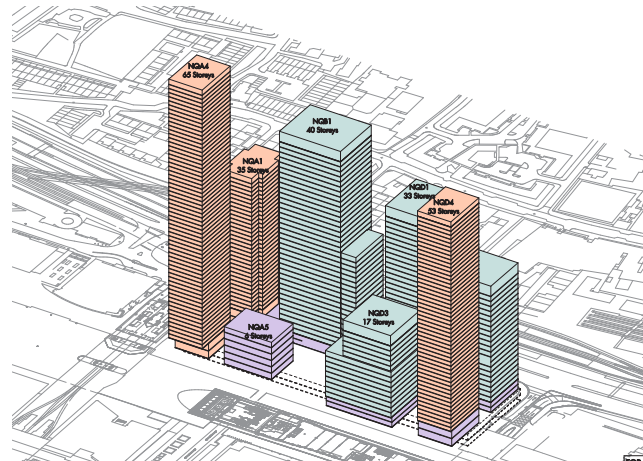
MASTERPLAN 6



Masterplan 6 - the final indicative masterplan



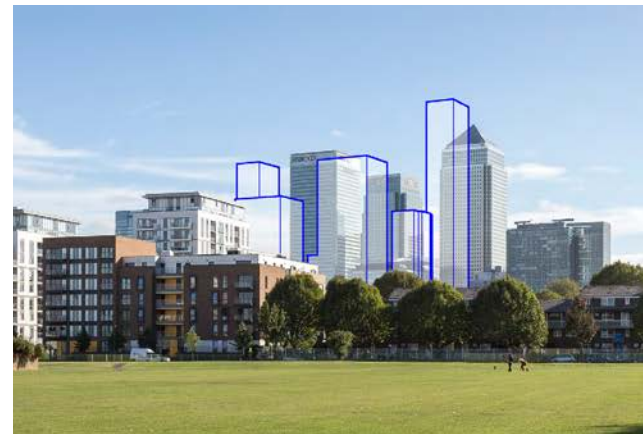
Massing model of the Alternative Indicative Scheme



Massing model of the Indicative Scheme



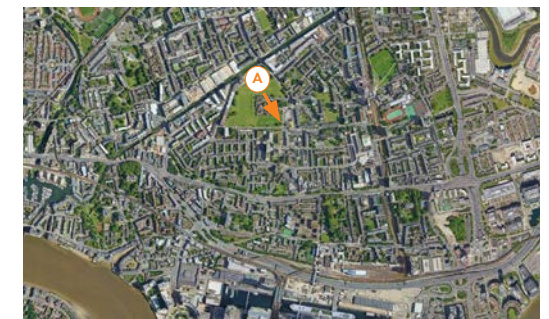
A - Alternative Indicative Scheme massing from Bartlett Park



A - Indicative Scheme massing from Bartlett Park

Following a request from officers in the previous meeting, an 'Alternative Indicative Scheme' had been developed to support the flexibility of the Parameter Plans and to enable additional testing on potential townscape impact. The Alternative Indicative Scheme demonstrated the swapping of two tall buildings from the Quayside to the Aspen Way side of the Masterplan. This was presented through a series of townscape massing images from local view positions, produced by Cityscape with input from townscape consultant Peter Stewart Consultancy.

LBTH Officers advised that they now strongly supported the Indicative Scheme but had concerns about the Alternative Indicative Scheme scenario, mostly related to the proposed height of building NQA.1 where this would be the tallest building on the Aspen Way edge of the Site and would have a greater impact on the surrounding area.



Location plan of verified wireline massing views

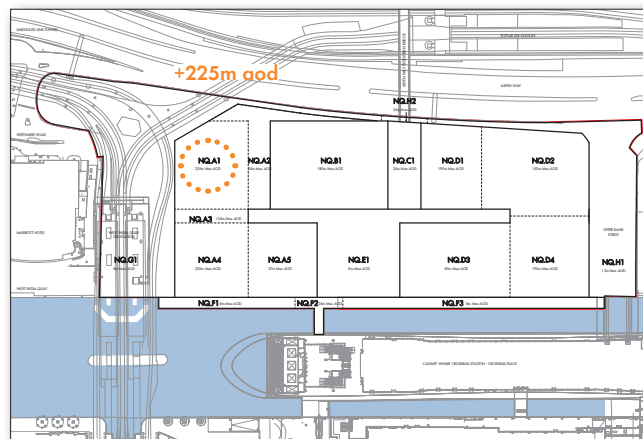


As a result of this feedback, the design team reduced the Parameter Plan maximum height of NQ.A1 down from +225m AOD to +150m AOD.

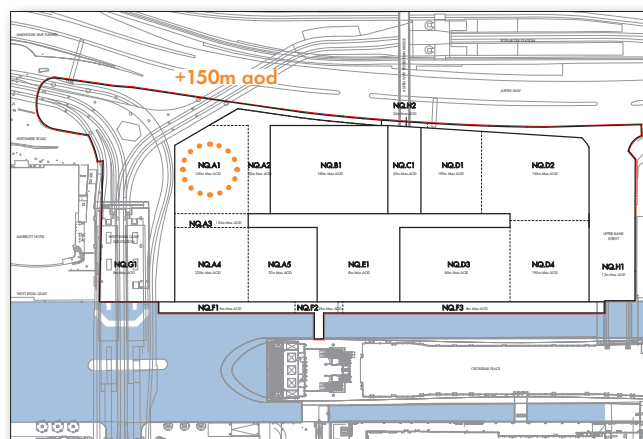
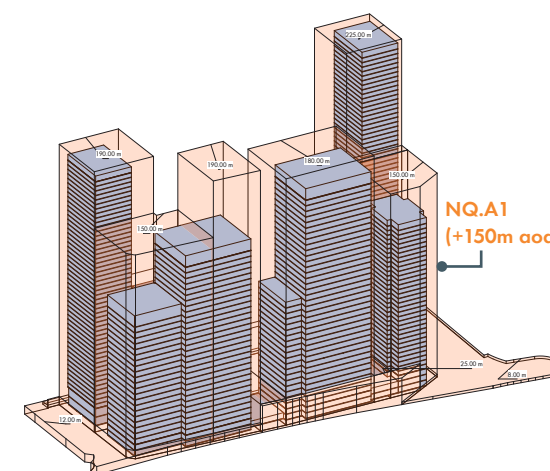
In the context of maintaining the flexibility of uses and different plot configurations that was being sought through the OPA, discussions were had with planning officers on a couple of remaining detailed points that had been raised previously.

Firstly, the way in which planning uses and active frontages would be defined in the Parameter Plans, to ensure that the Development would have an active and engaging series of frontages to each of the key spaces.

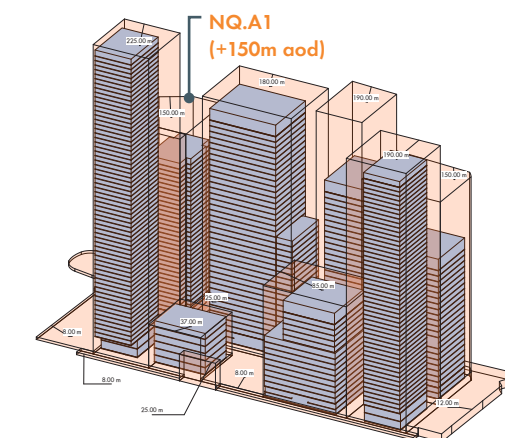
Secondly, establishing clear Design Guidelines for the east-west route through Development Zone NQD should it be developed with a building bridging over the North Quay Way. Officers wanted to ensure that this opening for the route through the plot would have an appropriate sense of generosity and scale.



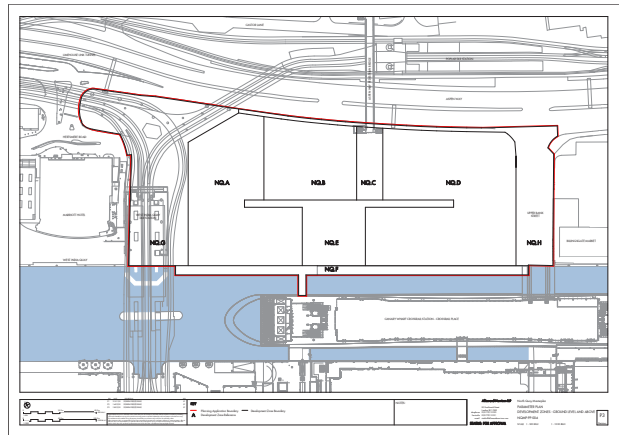
Evolution of Development Sub Zones Maximum Heights Parameter Plan



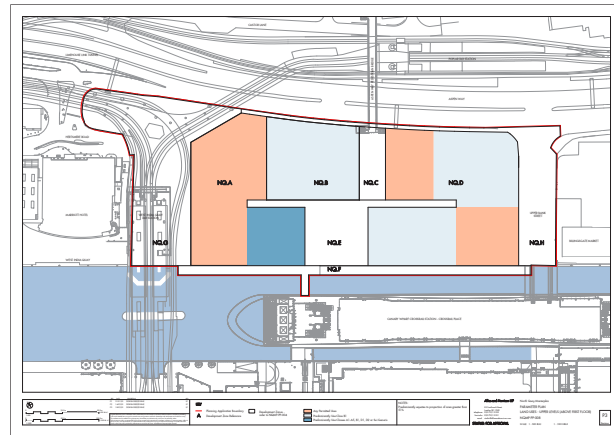
Reduction of height to NQ.A1 Development Sub Zones Maximum Heights Parameter Plan



Reduction of height to NQ.A1 Maximum Parameter Model

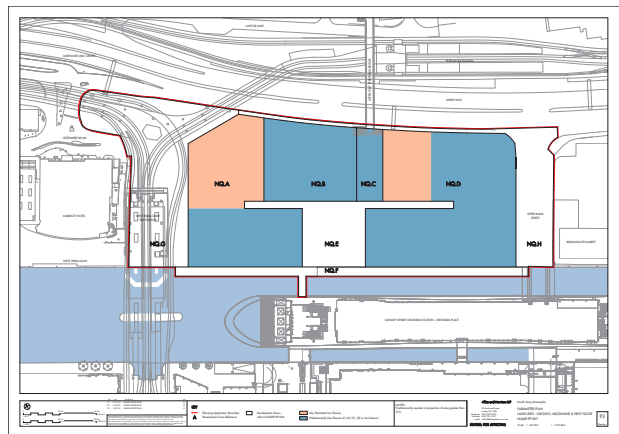


Parameter plan refinements - Development Zones

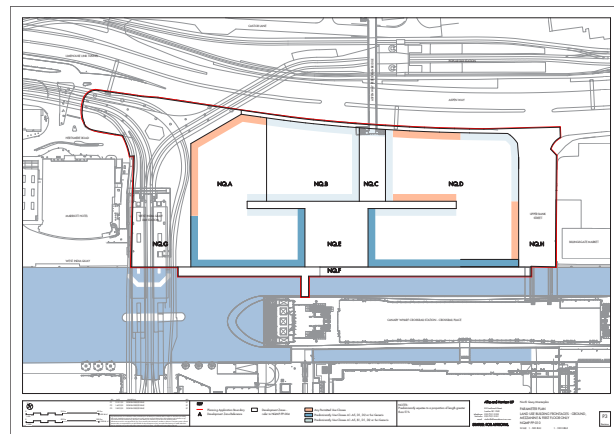


Parameter plan refinements - Upper Floor Uses

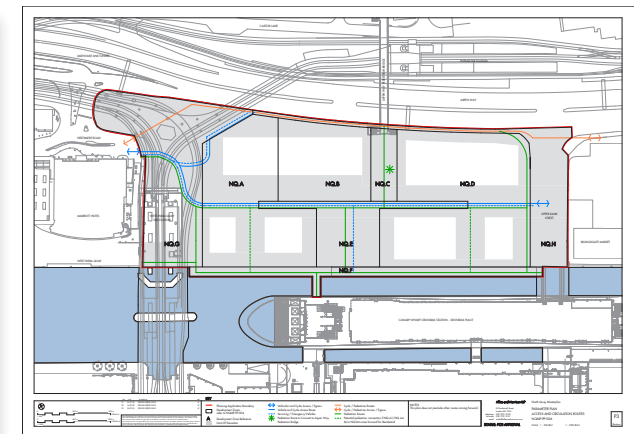
Following these discussions, final refinements were made to the Parameter Plans, alongside further development of the Design Guidelines and Development Specification areas and uses tables. As these 'Control Documents' need to work together to control the build out of the Masterplan, it is important to ensure that they were consistent in message and aligned in describing the requirements and guidance that should be followed.



Parameter plan refinements - Ground to First Floor Uses



Parameter plan refinements - Ground to First Floor Building Frontages



Parameter plan refinements - Access and Circulation



On 4th March 2020, a follow up meeting was held with Transport for London and DLR. Allies and Morrison, Steer and LDA Design presented an overview of the latest scheme proposals, including servicing, wider walking and cycle connections, Delta Junction proposals, development interface with Aspen Way, the proposed cycle link and changes to Upper Bank Street junction. Discussions were also held around the Aspen Way Footbridge, DLR infrastructure protection, crane access and short-stay cycle parking requirements and public realm impact. Overall this was a positive and productive meeting.

March 2020: Public exhibitions and further engagement

Three further public exhibitions were held in early March 2020 in locations to the north, south and west of the Site. Nearly 100 people attended across these three events. Most attendees, 93%, were local residents and 17% of those residents also worked in the area.

74 people provided written feedback and all but 5 people completed the forms at an event with the remaining forms provided by community stakeholders submitting a survey online.

The key themes that emerged from these consultations included open space, land use mix and routes and connections. A very large majority of people supported the overall approach to the outline proposals (97%). The most supported scheme principles were public space, access to the water and the improved connection between South Poplar and Canary Wharf.

Leisure - participants wanted to see community and social spaces, markets and arts as the top three priorities. It was noted that a new type of offering would be more interesting and to not just provide “more of the same”.



Boards from public consultation - March 2020



Photo from public consultation on 9th March 2020


JANUARY 2020

Infrastructure – Some concerns were raised over increased vehicular traffic on Hertsmere Road and potential disruption. It was suggested that better cycle routes are needed around the Canary Wharf estate, particularly the north-south links and it is important to ensure a through route from Poplar during construction.

Heritage – it was noted that character to celebrate the heritage of the Site would be welcomed.

FEBRUARY 2020

Pre-Application written feedback: March 2020

A pre-application letter was received from LBTH on 13th March 2020 concluding their officers' feedback from the formal local authority pre-application process. Comments within this letter have informed the completion of the Indicative Scheme design, the Parameter Plans and the Design Guidelines.

MARCH 2020
JULY 2020

Wind Mitigation Workshop May 2020

Following wind tunnel testing of the Maximum Parameter Model and Indicative Scheme, a wind mitigation workshop was undertaken to resolve residual unsuitable wind conditions. The wind analysis found that mitigation measures including bushes and shrubs in 2m by 2m planters at entrance locations, solid/porous screens, planting or art sculptures at least 1.5m height, and additional planting on terraces improved wind conditions throughout the Site for the Indicative Scheme.

5.1.4 CONCLUSION OF DESIGN DEVELOPMENT

The iterative process of developing and refining the design of the Indicative Scheme, the Parameter Plans and the Design Guidelines has been shaped and informed by a comprehensive process of consultation and engagement with the local authority, other statutory consultees and stakeholders as well as local residents and business representatives. The next chapter sets out these concluded proposals as the Masterplan for North Quay.

WHAT WE'VE HEARD SO FAR

Over 150 people took part in our consultation on the emerging proposals in November 2019. The key themes raised by participants included:

- ◆ The need for improved routes and safe access through and around the site for both pedestrians and cyclists.
- ◆ The creation of new public spaces was considered a top priority, particularly the inclusion of green spaces, lawns and planting, and places to sit and relax.
- ◆ Participants would like to see a mix of cultural and leisure uses provided - including family friendly facilities and events - alongside homes and offices.
- ◆ Importance of creating a place that feels open, inclusive and welcoming to all.

As a result of our initial concepts and the feedback received, the key principles of our approach have evolved as described opposite.

RESPONSE TO CONSULTATION



IMPROVED CONNECTIONS

New routes will be formed through the site, connecting the development to South Poplar, the DLR, Crossrail and creating a strong east-west connection.



HIGH QUALITY PUBLIC SPACES

A series of new hard and soft landscaped public spaces will be provided including a substantial new square.



WORKSPACES

A variety of new workspaces will be delivered that complement the existing offices of Canary Wharf.



SUSTAINABLE HOMES

There will be the opportunity to provide a range of sustainable homes including homes for sale and rent, affordable homes and student accommodation. All new homes will be net zero carbon and climate resilient.



RETAIL AND LEISURE

Leisure and recreation uses will be provided to extend Canary Wharf's offer and provide benefits to the wider community.



IMPROVED PUBLIC REALM

Significant improvements will be made to the public realm around the edges of the site, including the experience along Aspen Way.



NEW STREETS

North Quay will comprise a series of streets for pedestrians and cyclists which will link in with the wider area, including a new east-west route and north-south through the site.



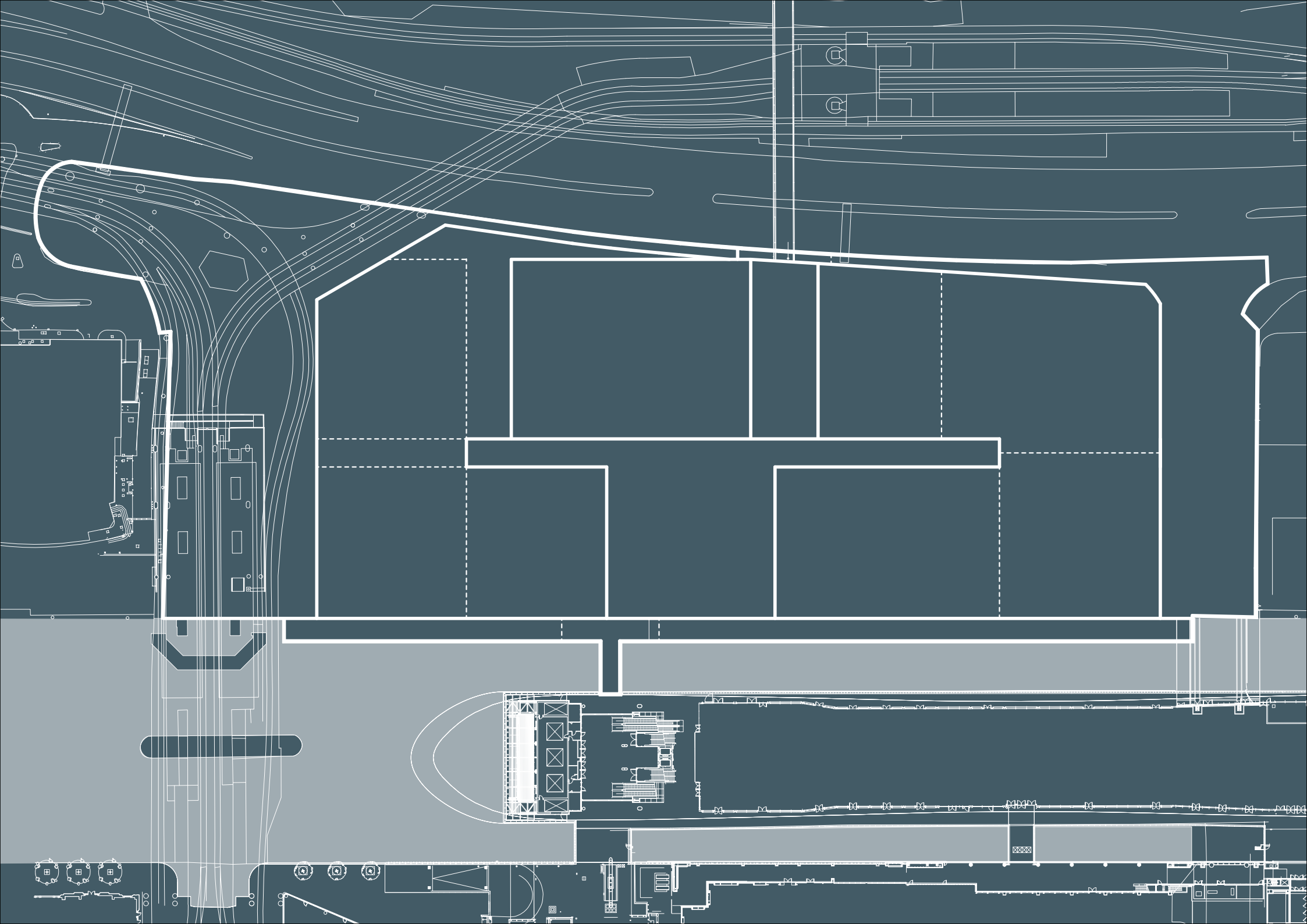
ACCESS TO THE WATERFRONT

The Dock edge will become a generous new public space for people to enjoy the waterside setting.



OPEN AND INCLUSIVE

Our proposals include spaces and amenities that will be open to the public and encourage people living and working in the area to visit North Quay.



6

THE PARAMETER PLANS

6.1 THE PARAMETER PLANS

The overarching application for the Site is made as an Outline Planning Application (or “the OPA”). An outline application does not seek permission for the details of the Proposed Development, but instead seeks to establish the principles in line with which future more detailed Reserved Matters applications will be considered, in terms of both the general scale of development and the land uses considered appropriate throughout the Site. In order to do this, the OPA sets out parameters for the layout, scale, access, appearance, and landscaping of the Proposed Development (“Specified Parameters”).

The OPA includes three control documents titled the Parameter Plans; the Development Specification; and the Design Guidelines. These documents define the Specified Parameters for the Proposed Development and their purpose is as follows:

The Parameter Plans define the extent of the proposed routes, spaces and buildings across the Site against a series of minimum or maximum dimensions. Each of these component parts is identified as a Development Zone which is identified by a three letter reference (e.g. Development Zone NQ.A) and which is then further broken down into Development Plots, each of which is identified by a three letter reference and number (e.g. Development Plot NQ.A1).

The Development Specification sets out a written account of the Parameter Plans and details the description of the Proposed Development and the type and quantity of development that could be provided within each of the Development Zones and Development Plots across the OPA Site.

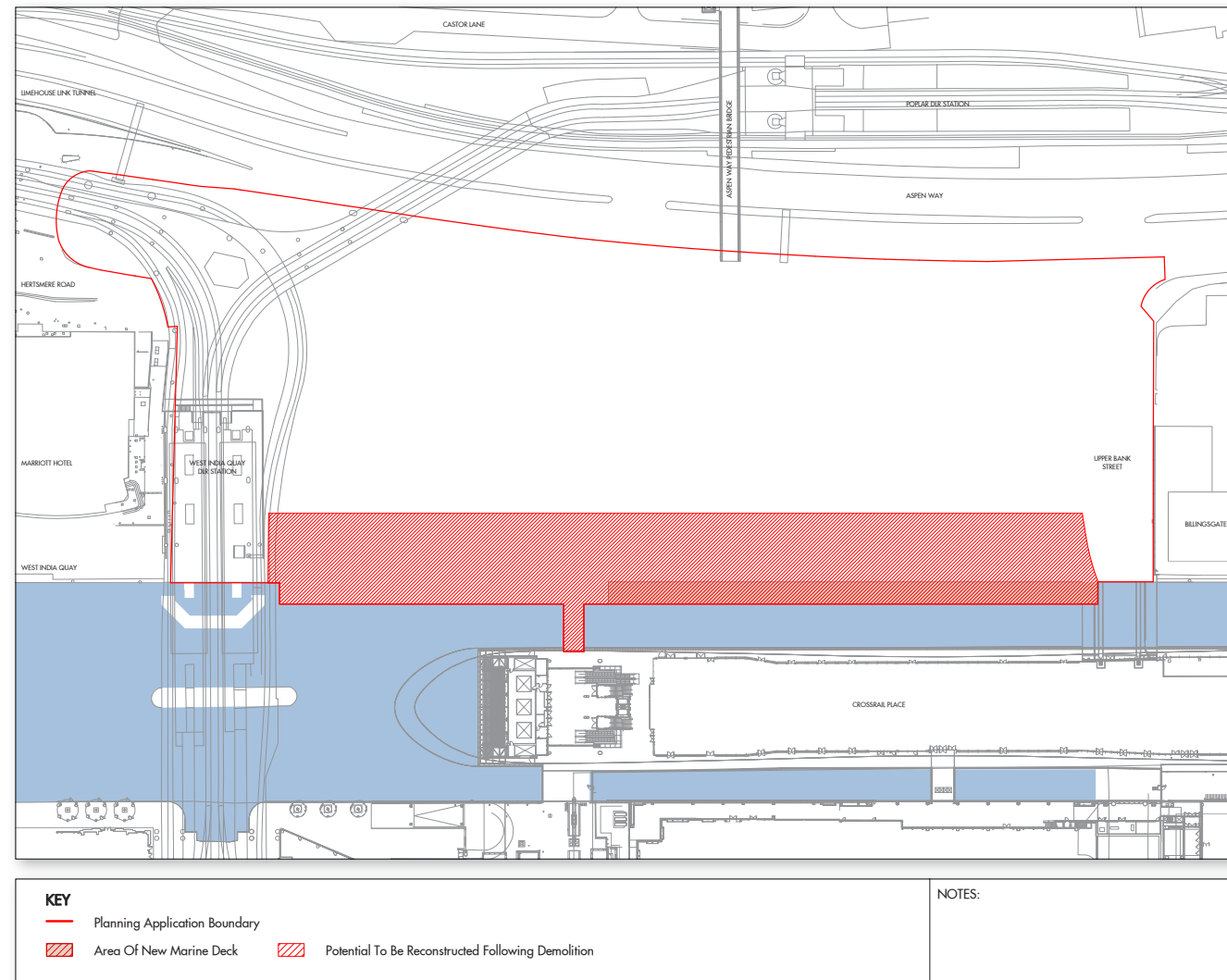
The Design Guidelines are intended to provide guidance for future design teams involved in the preparation of Reserved Matters applications for the development of any of the Development Zones and Development Plots. Reserved Matters applications are likely to need to comply with the Design Guidelines if they are to be considered acceptable.

Though each Parameter Plan drawing has a clear role to play in defining the Parameters for Development on the Site, it is important to recognise that the Parameter Plans work together as a set and that no one plan provides all of the information required for any part of the Site. They should also be read alongside the Development Specification and the Design Guidelines.

The Parameter Plans have evolved through the design development of the Masterplan and have been shaped as a result of consultations with LBTH and other key stakeholders. They have been used to test the Indicative Scheme as it has emerged and have themselves been adjusted in response to the Indicative Scheme evolution.

Over the following pages, we have set out the role for each of the Parameter Plans and describe what is shown on each drawing:

There are 11 Parameter Plans in total. The full set, can be seen through this chapter, form an integral part of the Masterplan and work together with the Development Specification and Design Guidelines to control development on North Quay.



NQMP-PP-001: MARINE DECK AND PROPOSED STRUCTURES

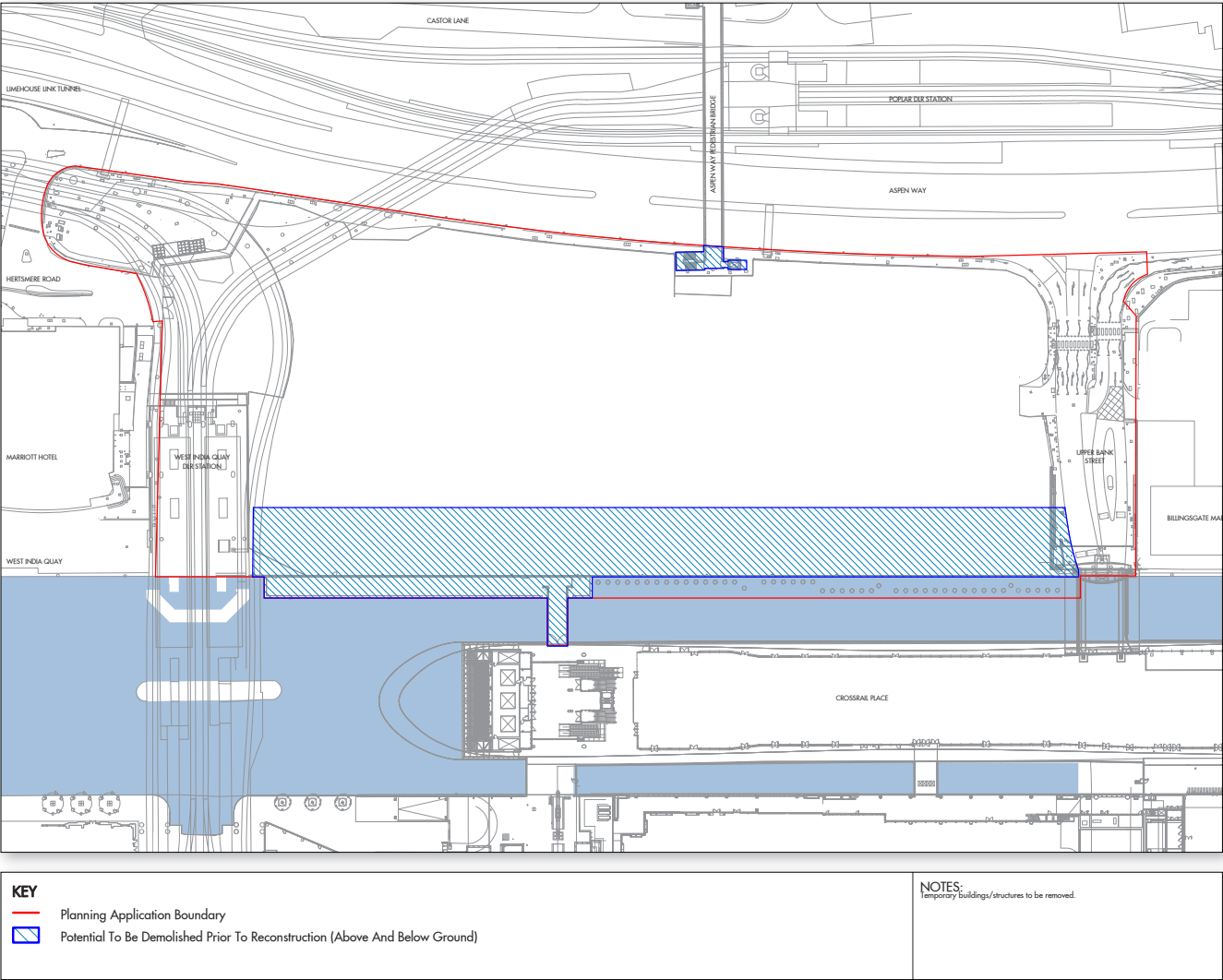
STATUS: FOR APPROVAL

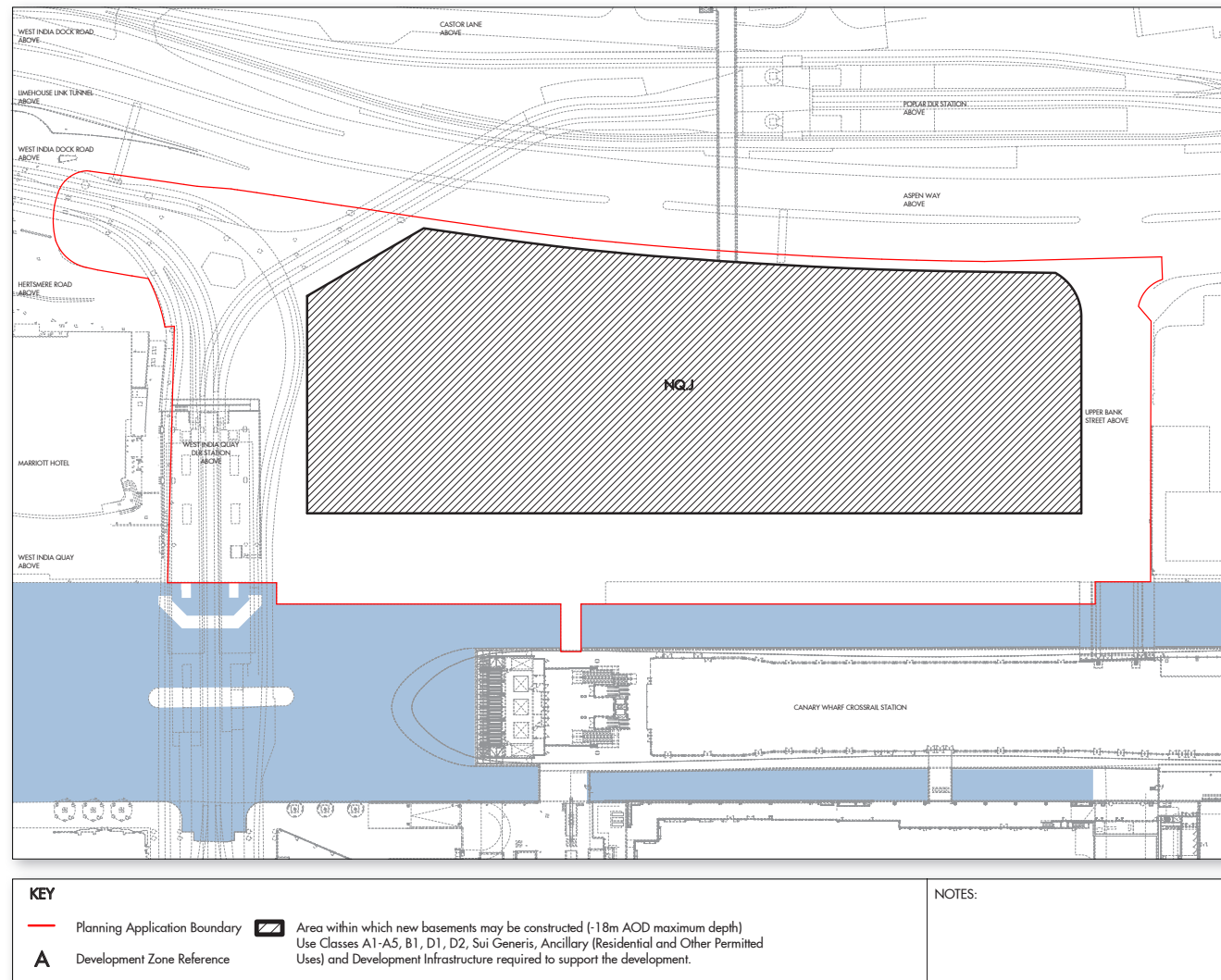
This plan drawing shows the extents of potential Marine Deck construction, built above the dock water, that may be undertaken on the Site. It defines areas for new Marine Deck construction and area where there is potential for the existing Marine Deck to be reconstructed following partial or full demolition.

NQMP-PP-002: BUILDINGS AND STRUCTURES TO BE DEMOLISHED

STATUS: FOR APPROVAL

This plan drawing shows the extents of potential demolitions that may be undertaken on the Site. Areas of demolition include the existing marine deck along the southern dock edge of the Site and the existing double storey footbridge connecting the Site to Crossrail Place. Demolitions might also include the area of the stair and lift on the southern side of the Aspen Way pedestrian bridge.





NQMP-PP-003: LAND USES BELOW GROUND

STATUS: FOR APPROVAL

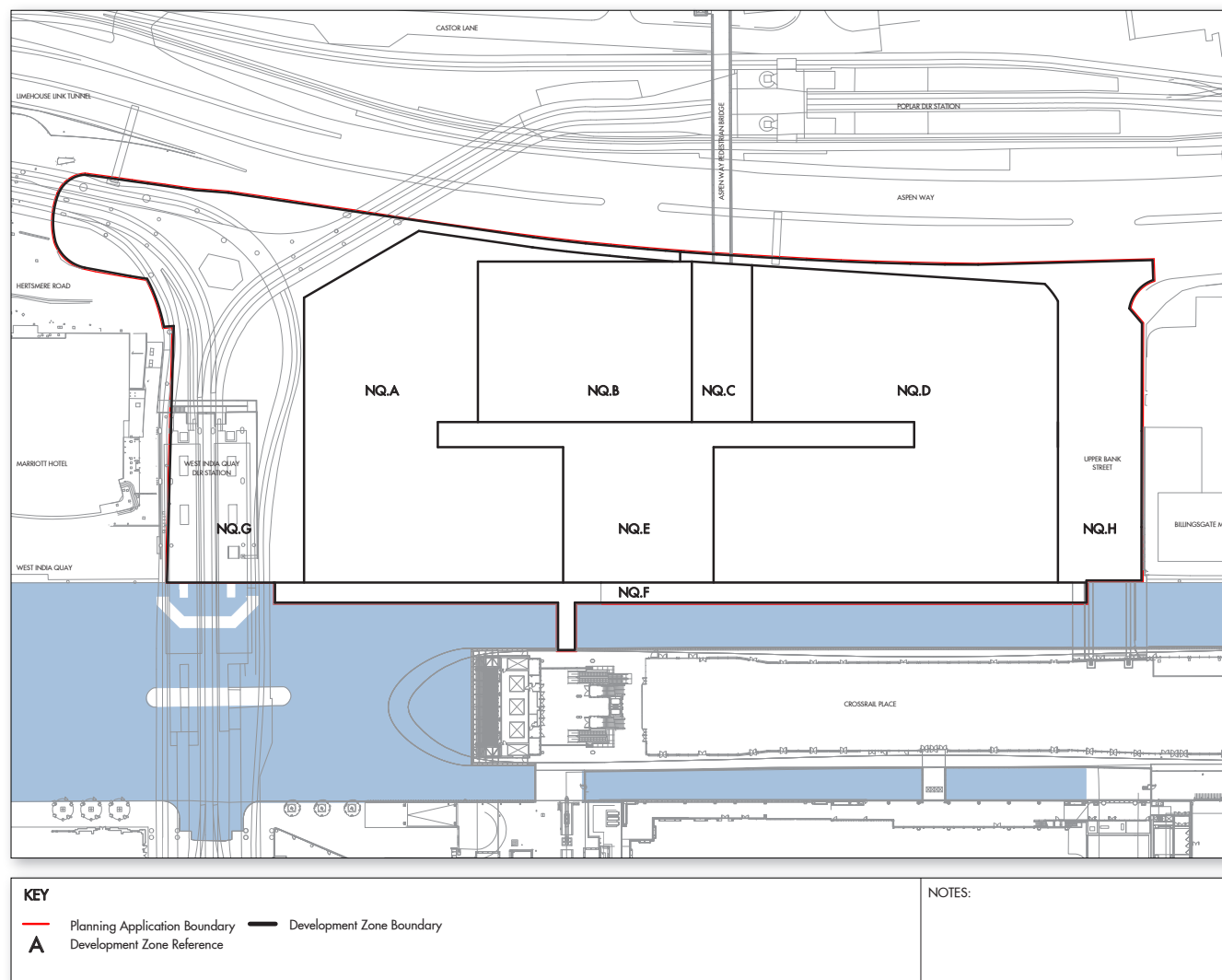
This plan drawing shows where land uses may be built on the Site below proposed ground level. The drawing defines the maximum plan area extent, maximum depth and permitted uses for usable basement levels space within the Development. The hatched area is defined as Development Zone NQ.J.

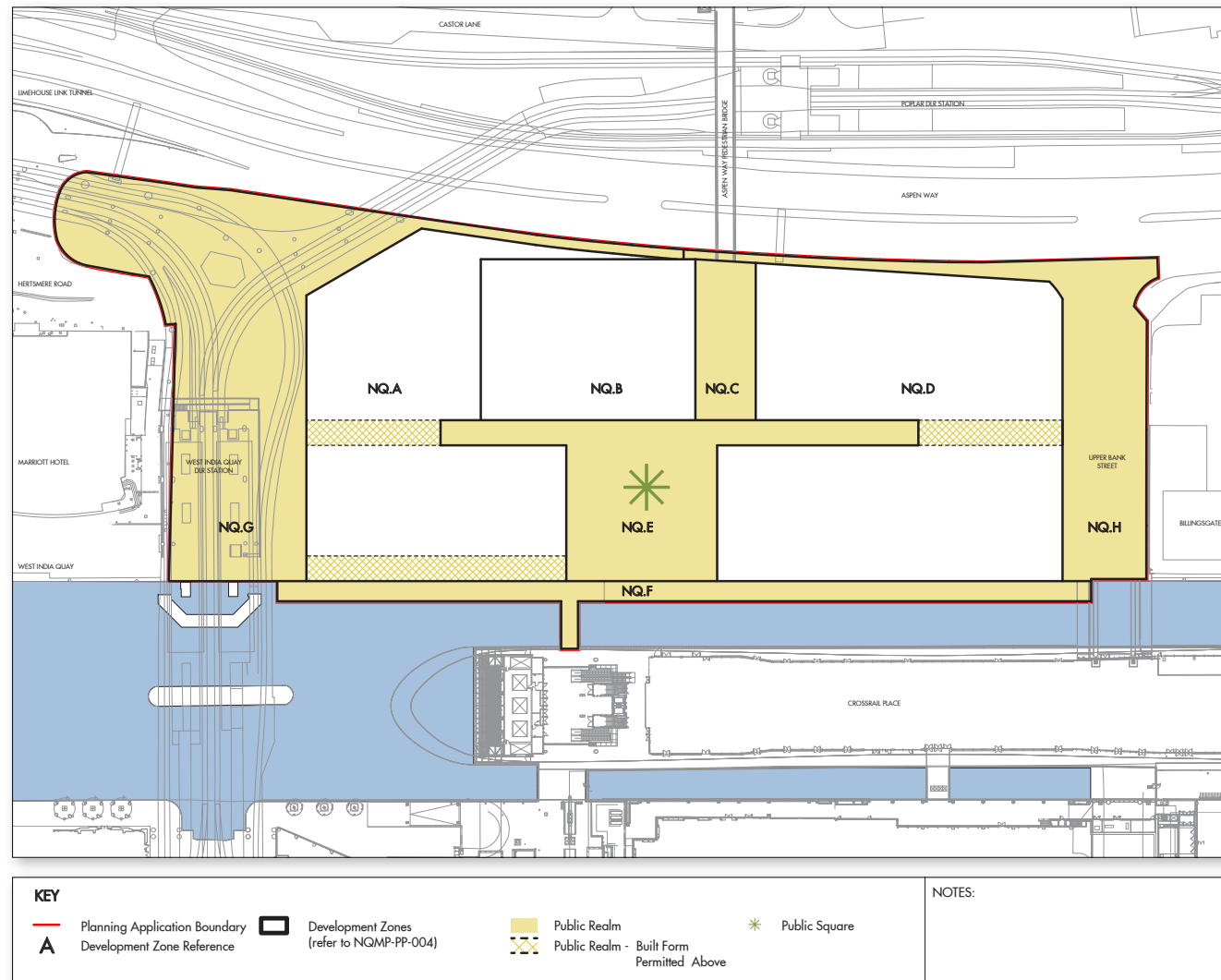
The key on the Parameter Plan identifies that the maximum depth for occupiable basement space is -18m AOD. For clarity, this maximum depth excludes any slab build ups, substructure foundations or piling, which may go below this level if required. It is also accepted that there will be below ground foundation constructions and piling outside of the hatched area.

NQMP-PP-004: DEVELOPMENT ZONES - GROUND LEVEL AND ABOVE

STATUS: FOR APPROVAL

This plan drawing shows the Zones where development may be built in the Site above proposed ground level. The Site area is divided into 8 Development Zones which are named NQ.A through to NQ.H and shows the maximum boundaries for each Zone.





NQMP-PP-005: PUBLIC REALM

STATUS: FOR APPROVAL

This plan identifies areas of publicly accessible open space or 'public realm' within the Site.

The yellow tinted areas show where open space should be provided within the Site boundary. As open space this may include street furniture, cycle racks, planting, lighting etc and small kiosks including security huts. It is not however area that may be occupied by new building structures above ground.

The yellow cross hatched areas show where publicly permeable routes should be provided within the Site boundary but where these may have building structures built above. As open space this may also include street furniture, cycle racks, planting, lighting etc and small kiosks including security huts. It may also have columns/supporting structures that hold the building structures above it. A green asterisk defines where the principal publicly accessible space should be located within the Development.

NQMP-PP-006: ACCESS AND CIRCULATION ROUTES

STATUS: FOR APPROVAL

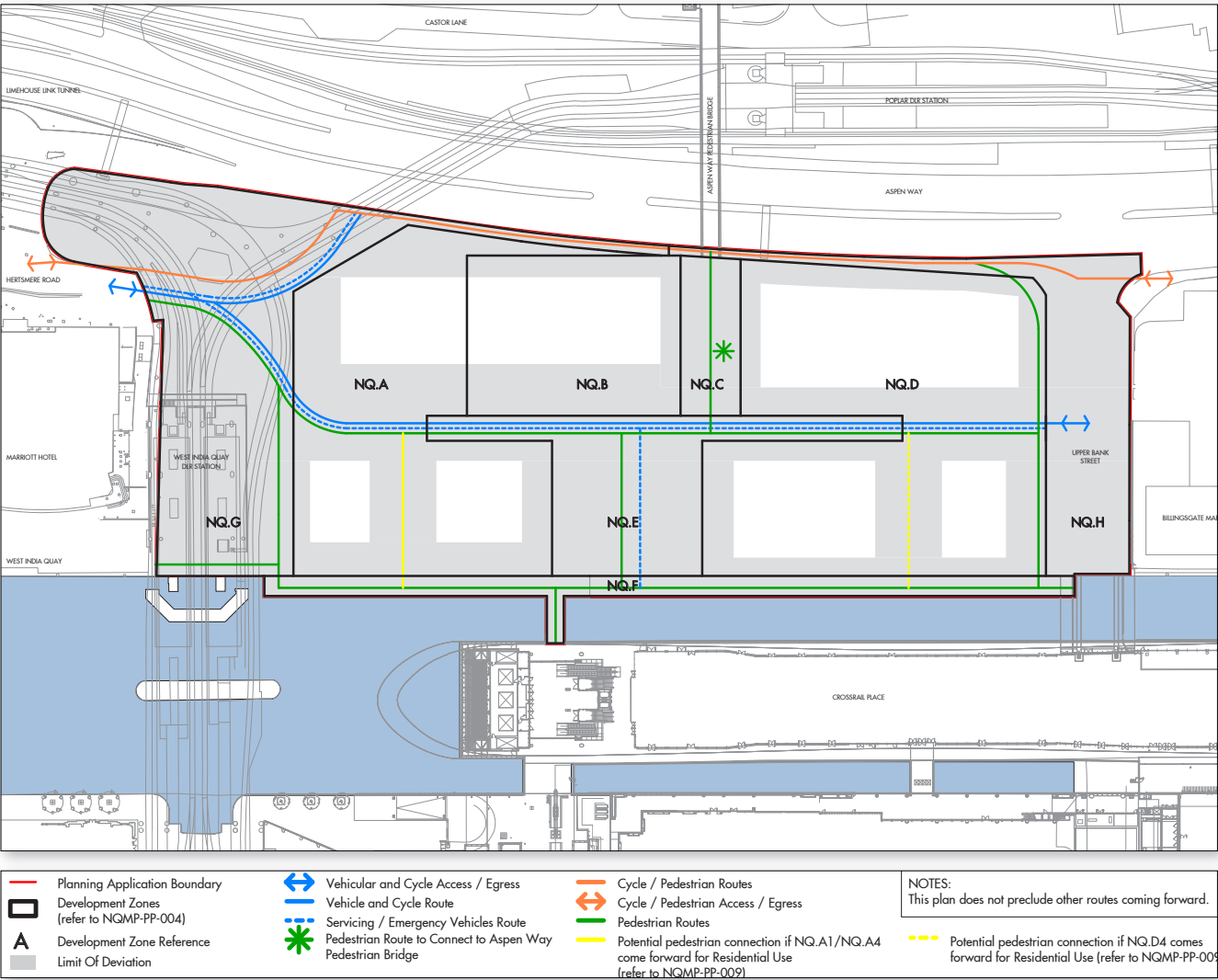
This plan defines routes that should or may be provided within the Site for pedestrians, cyclists, other vehicles, servicing and emergency vehicles. It is intended to ensure that the Site is accessible and appropriately permeable from all key approach routes and points of egress.

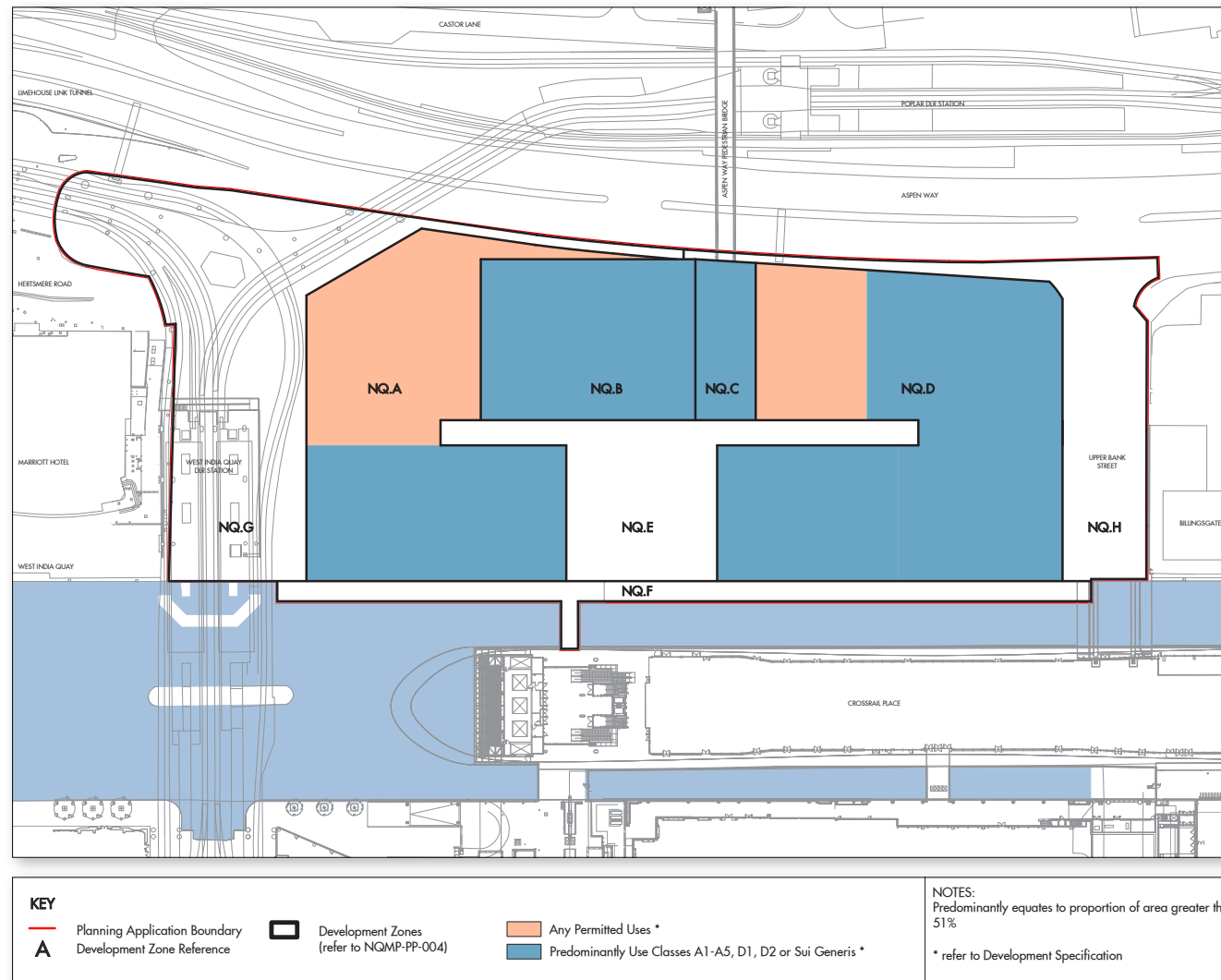
Solid blue, orange and green lines define routes that should be provided through the Site. Dashed lines and the solid yellow line are routes that may be provided depending on the final configuration in which some Development Plots are built out.

A key connection that requires a change of level is the pedestrian route connecting through NQ.C to the Aspen Way pedestrian footbridge.

Each of the routes has a limit of deviation shown as a grey zone. The coloured line represents the notional centre line of the route, but this may move or deflect within the limits of deviation, providing flexibility in the final alignment of building edges and the form of landscaping.

Guidelines for the design of these routes are provided within the Design Guidelines.





NQMP-PP-007: LAND USES - GROUND, MEZZANINE & FIRST FLOOR

STATUS: FOR APPROVAL

This plan drawing defines which permitted land uses may be built on the Site at proposed ground, mezzanine and first floor levels and where different use classes may be located.

The drawing is subdivided into Development Zones as defined in Parameter Plan NQMP-PP-004. Development within areas coloured orange may be any Permitted Use Class. Development within areas coloured blue should be Predominantly Uses from Use Classes A1-A5, D1, D2 or Sui Generis. The term 'Predominantly' is defined on the Parameter Plan drawing. Further information on permitted uses is defined in the Development Specification.

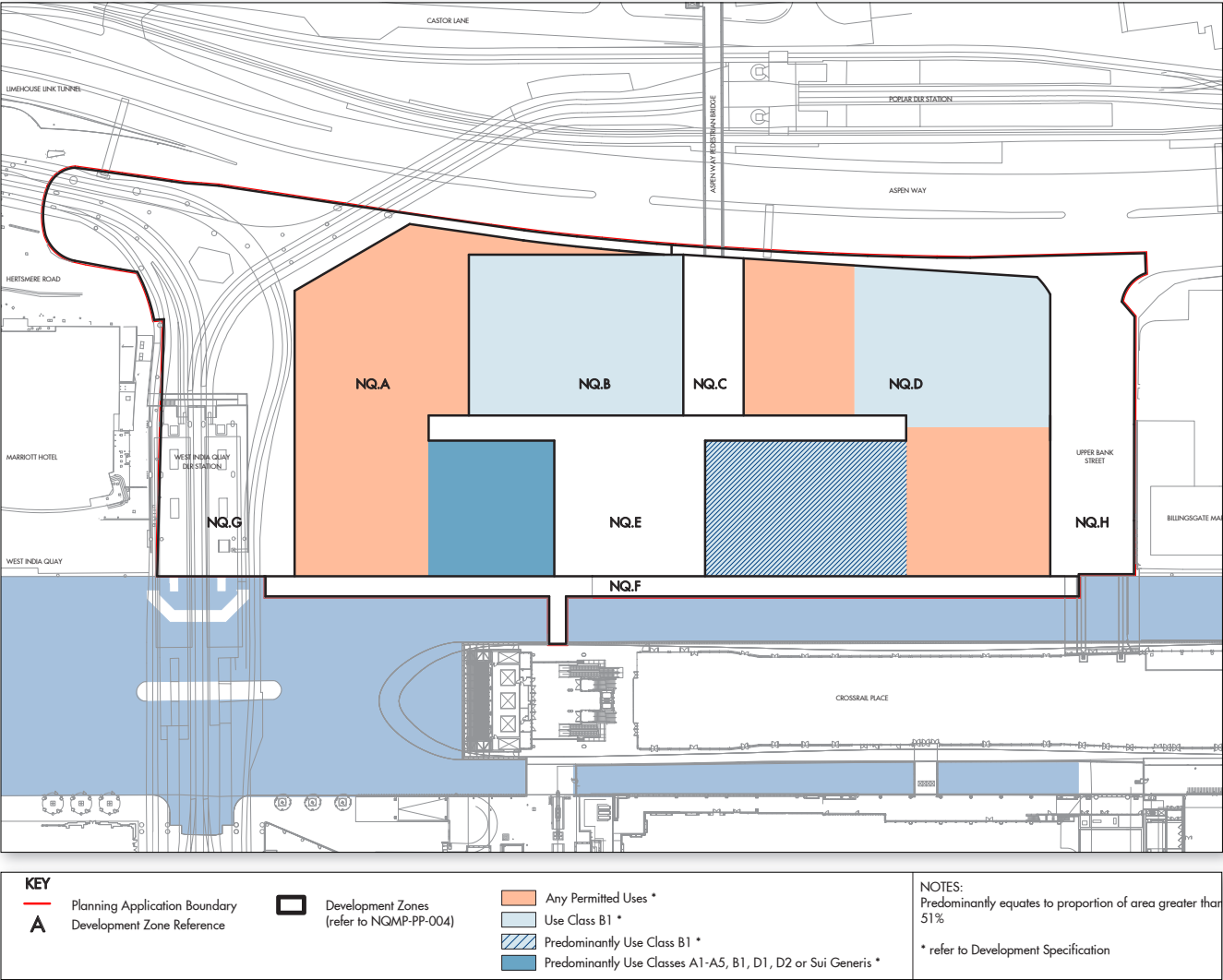
**NQMP-PP-008 LAND USES - UPPER LEVELS
(ABOVE FIRST FLOOR)**

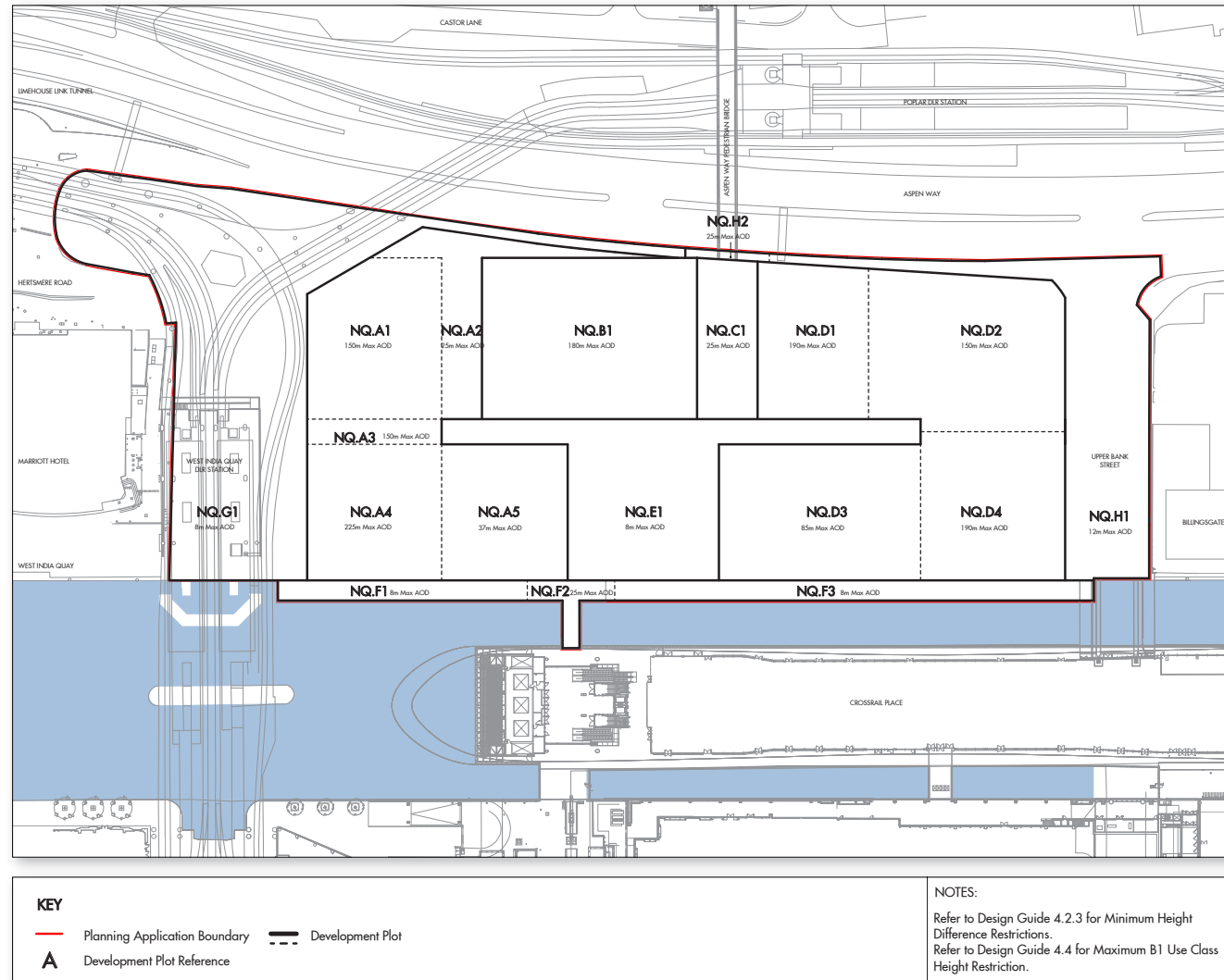
STATUS: FOR APPROVAL

This plan drawing defines which permitted land uses may be built on the Site on floor levels above first floor level and where different use classes may be located.

The drawing is subdivided into Development Zones as defined in Parameter Plan NQMP-PP-004. Development within areas coloured orange may be any Permitted Use Class. Development within areas coloured light blue should be Use Class B1. Development within areas coloured light blue with diagonal stripes should be predominantly Use Class B1. Development within areas coloured dark blue should be predominantly Uses within Use Classes A1-A5, B1, D1, D2 or Sui Generis.

The term 'Predominantly' is defined on the Parameter Plan drawing. Further information on permitted uses is defined in the Development Specification.





NQMP-PP-009: DEVELOPMENT PLOTS MAXIMUM HEIGHTS, WIDTHS & LENGTHS

STATUS: FOR APPROVAL

This plan drawing defines the maximum heights, widths and lengths for Development on the Site. The drawing is subdivided into Development Zones as defined in Parameter Plan NQMP-PP-004 and then these Zones are divided into Development Plots.

The maximum heights are measured in metres Above Ordnance Datum (AOD). Guidelines for height interrelationship of adjacent buildings is further defined within the Design Guidelines. A note on the drawing references key sections.

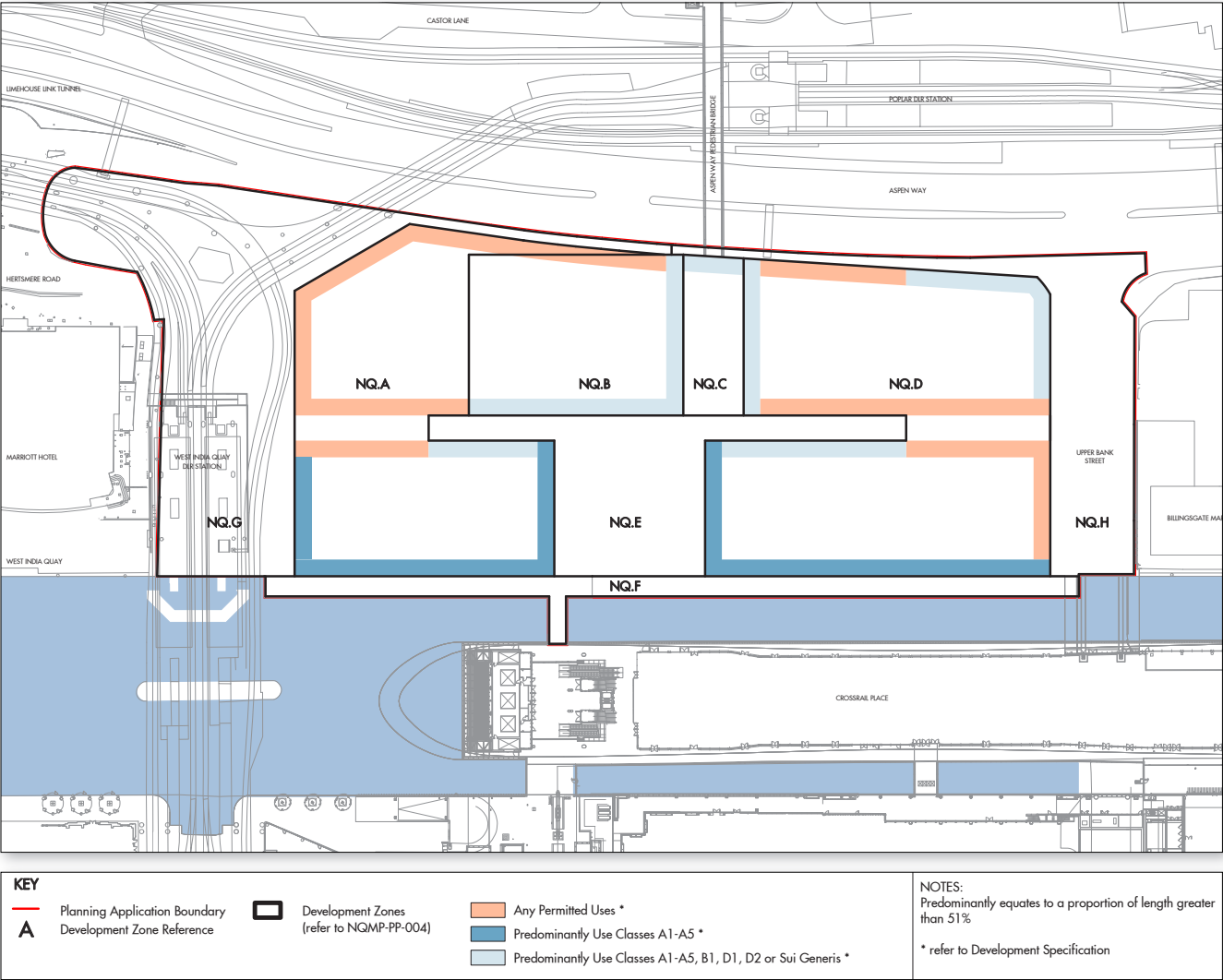
NQMP-PP-010: LAND USE BUILDING FRONTAGES - GROUND, MEZZANINE & FIRST FLOOR ONLY

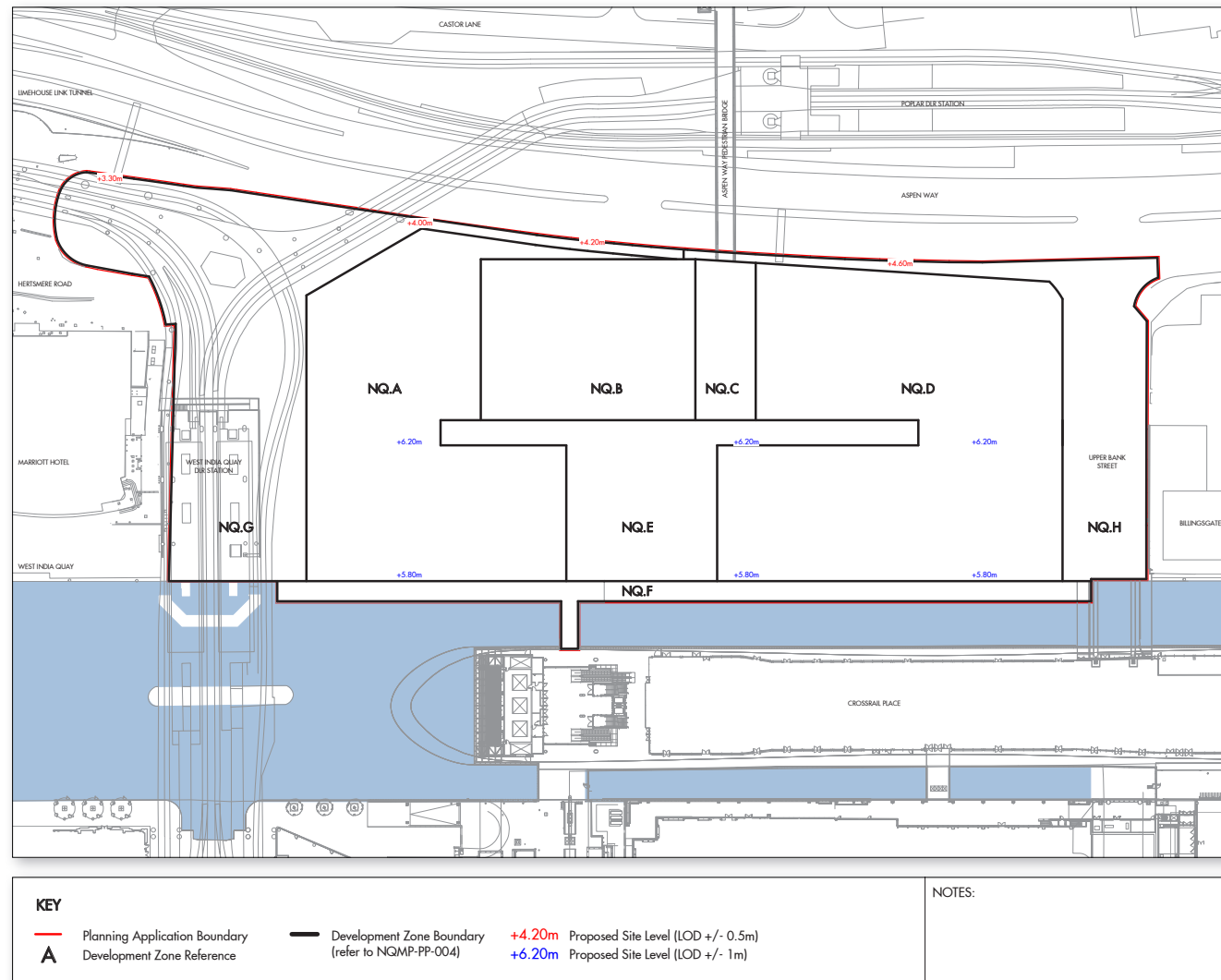
STATUS: FOR APPROVAL

This plan drawing defines which permitted land uses may be built along building frontages on the Site at proposed ground, mezzanine and first floor levels and where frontages for different use classes may be located. It is intended to ensure that a balance of active frontages is provided in the right places within the Development. Further guidance on this is provided within the Design Guidelines.

The drawing is subdivided into Development Zones as defined in Parameter Plan NQMP-PP-004. Development along frontages coloured orange may be any Permitted Use Class. Development along frontages coloured dark blue should be Predominantly Uses from Use Classes A1-A5. Development along frontages coloured light blue should be Predominantly Uses from Use Classes A1-A5, B1, D1, D2 or Sui Generis.

The term ‘Predominantly’ is defined on the Parameter Plan drawing. Further information on permitted uses is defined in the Development Specification.





NQMP-PP-011: PROPOSED SITE LEVELS

STATUS: FOR APPROVAL

This drawing shows proposed ground levels for open spaces or routes across the Site. The levels are provided with a height 'limit of deviation' which means that the final ground level in each location can vary up and down within the limits shown. This allows some flexibility for the open space design to evolve whilst ensuring that the scheme will connect appropriately into surrounding levels.