



CANARY WHARF  
GROUP PLC

NQ.PA.04

# North Quay Design Guidelines

Allies and Morrison  
July 2020



# NORTH QUAY

OUTLINE PLANNING APPLICATION

**DESIGN GUIDELINES** - NQ.PA.04





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# 1

## INTRODUCTION

# 1.1

## FOREWORD

Thank you for taking the time to engage with the proposals for North Quay, London (the "Site").

This foreword explains:

1. Why the Proposed Development is being advanced;
2. What applications are being made;
3. What documents have been submitted; and
4. What happens next.

### 1.1.1

## WHY THE PROPOSED DEVELOPMENT IS BEING ADVANCED

### Why are we making a planning application?

The Site benefits from an exant planning permission (LPA ref. PA/03/00379) for 372,660sqm of office floorspace and 5,324sqm of retail floorspace which was implemented in 2016. In 2017, the Applicant submitted a full planning application (PA/17/01193) for a mixed use office, residential and retail development; a significant element of which was providing a new substantial headquarters office building for a specific tenant. This application was withdrawn in December 2017.

The Applicant, Canary Wharf (North Quay) Limited ("the Applicant"), a wholly owned subsidiary of the Canary Wharf Group, has been reconsidering how the Site might be developed in the context of changing market and planning requirements. Following extensive engagement with stakeholder groups and the local community, the proposals have been established and will now be taken forward as an Outline Planning Application (OPA).

**1.1.2****WHAT APPLICATIONS ARE BEING MADE****The Proposed Applications**

The Applicant" is submitting applications for Outline Planning Permission ("OPP") and Listed Building Consent ("LBC") to enable the redevelopment of the North Quay site, Aspen Way, London ("the Site").

Two separate applications are being submitted as follows:

- Application NQ.1: Outline Planning Application (all matters reserved) ("OPA") - Application for the mixed-use redevelopment of the Site comprising demolition of existing buildings and structures and the erection of buildings comprising business floorspace, hotel/serviced apartments, residential, co-living, student housing, retail, community and leisure and sui generis uses with associated infrastructure, parking and servicing space, public realm, highways and access works; and -
- Application NQ.2: Listed Building Consent Application - Application to stabilise listed quay wall and any associated/necessary remedial works as well as demolition of the false quay in connection with Application NQ.1.

Together the development proposed under Applications NQ.1 and NQ.2 are referred to as the "Proposed Development".

At the time of making the OPA, the Applicant is unable to determine exactly how much of the Proposed Development is likely to come forward in which land use. For this reason, the description of development provides the Applicant with flexibility as to the uses that could be undertaken on the Site.

However, in order to ensure that the level of flexibility is appropriately restricted, the OPA seeks approval for three Control Documents which describe the principal components of the Proposed Development, define the parameters for the Proposed Development (the "Specified Parameters") and control how the Proposed Development will come forward in future. They provide the parameters, design principles and controls that will guide future reserved matters applications ("RMAs"). These Control Documents are – (1) the Development Specification; (2) the Parameter Plans; and (3) the Design Guidelines:

- The Development Specification sets out the type and quantity of development that could be provided across the Site (including setting a maximum floorspace across the Site);
- The Parameter Plans set the parameters associated with the scale, layout, access and circulation and distribution of uses classes and public space for the Proposed Development. They also establish the Development Zones and Development Plots across the Site; and

- The Design Guidelines set the design principles and controls for future development.

Together, these documents set out the information required to allow the impacts of the Proposed Development to be identified with sufficient certainty as future RMAs will be required to demonstrate compliance with the Specified Parameters and controls in these Control Documents.

The Design Guidelines are only prescriptive when it comes to preserving the essential broad principles. With regard to matters of detail, they aim to be flexible and encourage creativity. The development outcome that the Masterplan is intended to facilitate will be the work of many hands, rather than the vision of a single design team.

In order to help stakeholders to understand what sort of development an OPP might allow to come forward, we have developed and assessed a scheme which is 'indicative' of what could be built based on the Specified Parameters. This is referred to throughout the application documents as the "Indicative Scheme" and is a realistic reflection of what could come forward. This Indicative Scheme is only indicative, and the actual detail of each part of the Proposed Development (known as Development Zones and Development Plots) to be built will come forward only when RMAs are made. In short, the OPP would set the rules which the detailed Reserved Matters proposals would need to comply with.

## **Site Description**

The North Quay site ("the Site") is located in the north of the Isle of Dogs, within the administrative boundary of the London Borough of Tower Hamlets (the "LBTH"), at Canary Wharf. It is bounded by Canary Wharf Crossrail Station to the south, Aspen Way (A1261) to the north, Hertsmere Road to the west and Billingsgate Market to the east. The West India Quay Docklands Light Railway ("DLR") station and Delta Junction are located on the western side of the Site and the Site also incorporates parts of North Dock, Upper Bank Street and Aspen Way.

The Site is 3.28 hectares (ha) in area. Currently the Site comprises mostly cleared land, being previously used as a construction laydown site for the Canary Wharf Crossrail Station. There are some temporary uses currently on site, including the LBTH Employment and Training Services, WorkPath and advertising structures.

A Grade I Listed brick dock wall (Banana Wall) exists below the surface of part of the Site, which originally formed the dockside until it was extended over to the south.

Existing access to the Site for vehicles is from Upper Bank Street to the east and Hertsmere Road to the west, which both link to Aspen Way. The Site is not currently accessible to the public, however pedestrian routes are located on each side of the Site (Aspen Way, Hertsmere Road, Upper Bank Street, and the western part of the dockside to the south). The Aspen Way

footbridge which leads to South Poplar also lands on the southern side of Aspen Way. The Site is highly accessible by public transport. The West India Quay DLR station is located on the Site, the Poplar DLR station is accessed directly from the Aspen Way Footbridge, the Canary Wharf Crossrail Station is located immediately to the south of the Site, beyond which are the Canary Wharf underground and DLR stations. The Site's PTAL varies from 5 ('very good') to 6a ('excellent'), with improved PTAL closer to Upper Bank Street. The score is expected to improve to 6a across the entire Site by 2021 owing to the planned opening of the Crossrail Station.

Beyond the Site, 1 West India Quay (the Marriot Hotel (107m AOD) and residential building (41m AOD)) are located to the west, adjacent to the DLR tracks. Beyond these, along Hertsmere Road is a cinema, museum, shops, restaurants and other leisure facilities, forming part of the West India Quay Centre.

Billingsgate Market is located to the east of the Site, on the opposite side of Upper Bank Street. Billingsgate Market is identified as a Site Allocation (4.2: Billingsgate Market) for redevelopment in LB Tower Hamlet's Local Plan.

To the north of the Site on the other side of Aspen Way are the Tower Hamlets College and The Workhouse leisure facility. They comprise part of a Site Allocation (4.1: Aspen Way) for redevelopment in LB Tower Hamlet's Local Plan. In close proximity to these there are lower rise residential properties (some with shops beneath them) as well as the Poplar Recreation Ground.

Beyond the Crossrail station and Crossrail Place to the south of the Site is the Canary Wharf commercial area, with the buildings closest to the Proposed Development including the HSBC (200m AOD), Bank of America and One Canada Square buildings (235m AOD).

### **Listed Building Works**

Towards the south of the Site, the edge of the dock is defined by a quay wall known as the Banana Wall. The brickwork has a profile and counterfort buttresses, on a gravel bed. The Banana Wall was constructed between 1800-1802 and was Listed Grade I in 1983.

The Proposed Development will span over the Banana Wall with piles on either side of the wall providing support to the new structures. The new structures will leave a void or compressible material above to avoid permanent loading of the wall. The adjacent existing false quay deck will be removed and replaced (to the extent required). The excavation of the basement may require stabilisation works to be undertaken to ensure there are no impacts to the Banana Wall. Remedial works to the Banana Wall will also be undertaken if required.

### 1.1.3

## WHAT DOCUMENTS HAVE BEEN SUBMITTED

### The Application Documents

The applications described above are each supported by a number of application documents. The paragraphs below explain the purpose and intent of each of the key submitted documents, how they inter-relate with one another and how they have been used to define the Proposed Development.

Some of the documents are submitted for approval whilst others provide supporting information for the applications. The Control Documents (including this document) that are to be approved identify the maximum parameters and the design guidelines of Proposed Development

### The Control Documents – for Approval

The applications described above are each supported by a number of application documents. The paragraphs below explain the purpose and intent of each of the submitted documents, how they inter-relate with one another and have been used to define the Proposed Development.

Documents that contain the guidelines and details of the Proposed Development, are submitted for approval whilst others provide background information.

- The **Parameter Plans** define the extent of the proposed routes, spaces and building envelopes across the OPA Site against a series of minimum or maximum dimensions. Each of these component parts is identified as a Development Zone which is identified by letters (e.g. Development Zone NQ.A) and which is then further broken down into Development Plots, each of which is identified by letters and a number (e.g. Development Plot NQ.A1).
- The **Development Specification** sets out a written account of the Parameter Plans and details the description of the Proposed Development and the type and quantity of development that could be provided within each of the Development Zones and Development Plots across the OPA Site within the defined land uses.
- The **Design Guidelines** are intended to provide guidance for future design teams involved in the preparation of Reserved Matters applications for the development of any of the Development Zones and Development Plots. RMAs will need to demonstrate compliance to the Design Guidelines if they are to be considered acceptable



## Listed Building Consent Documents – for Approval

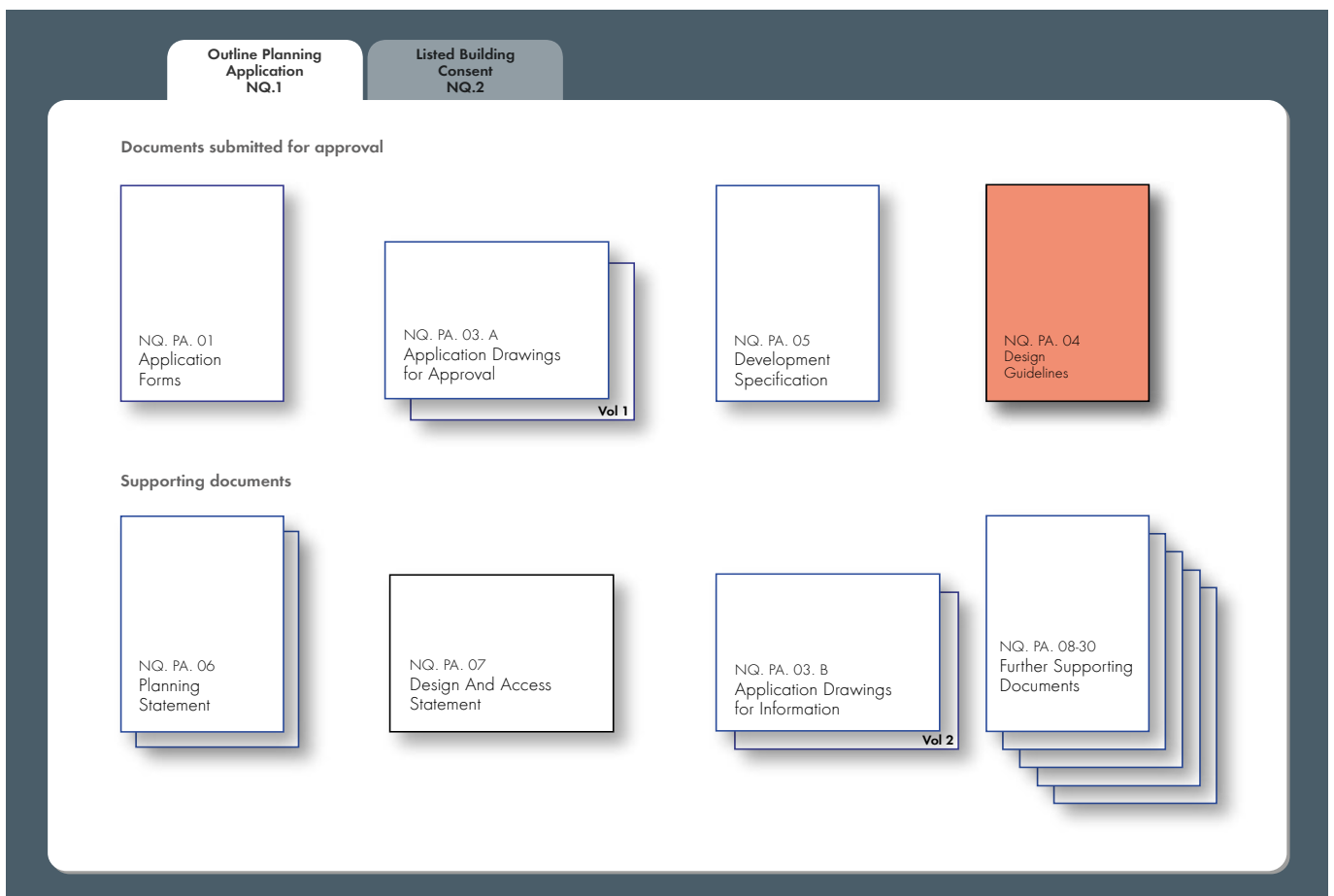
The drawings and documents set out in full the details of those elements of the Proposed Development for which Listed Building Consent is required.

## The Key Supporting Documents

The documents that are submitted in support of one or more of the applications set out information to help the consideration of the proposals. A full suite of supporting documents is submitted in support of the OPA and Listed Building Consent applications (Table 1).

The Planning Statement (NQ.PA.06) sets out how the development responds to relevant planning policies and guidance. It also sets out the benefits expected to flow from the Proposed Development.

The Design & Access Statement (NQ.PA.07) explains the design evolution of The Masterplan. This document describes how the amount, scale, layout, appearance, landscaping, and inclusive design and community



Ref.	Submission Material
<b>Application NQ.1 – Outline Planning Application</b>	
NQ.PA.01	APPLICATION FORMS
NQ.PA.02	COMMUNITY INFRASTRUCTURE LEVY ADDITIONAL INFORMATION FORM
NQ.PA.03	APPLICATION DRAWINGS
NQ.PA.03.A	VOLUME 1 - DRAWINGS FOR APPROVAL
NQ.PA.03.B	VOLUME 2 - DRAWINGS FOR INFORMATION
NQ.PA.04	DESIGN GUIDELINES
NQ.PA.05	DEVELOPMENT SPECIFICATION
NQ.PA.06	PLANNING STATEMENT
NQ.PA.07	DESIGN AND ACCESS STATEMENT
NQ.PA.08	ENVIRONMENTAL STATEMENT
Volume 1	Main Report
Volume 2	Townscap
Volume 3	Technical Appendices
NQ.PA.09	ENVIRONMENTAL STATEMENT NON TECHNICAL SUMMARY
NQ.PA.10	TRANSPORT ASSESSMENT
NQ.PA.11	FRAMEWORK TRAVEL PLAN
NQ.PA.12	RESIDENTIAL TRAVEL PLAN
NQ.PA.13	DELIVERY AND SERVICING PLAN
NQ.PA.14	STATEMENT OF COMMUNITY INVOLVEMENT
NQ.PA.15	HERITAGE ASSESSMENT
NQ.PA.16	SUSTAINABILITY STATEMENT
NQ.PA.17	ENERGY STATEMENT
NQ.PA.18	AFFORDABLE HOUSING STATEMENT
NQ.PA.19	VIABILITY APPRAISAL
NQ.PA.20	INTERNAL SUNLIGHT AND DAYLIGHT ASSESSMENT
NQ.PA.21	ECONOMIC AND REGENERATION STATEMENT
NQ.PA.22	SITE WASTE MANAGEMENT PLAN
NQ.PA.23	FOUL SEWERAGE AND UTILITIES ASSESSMENT
NQ.PA.24	AVIATION SAFEGUARDING ASSESSMENT
NQ.PA.25	RADIO AND TV INTERFERENCE ASSESSMENT
NQ.PA.26	ARCHAEOLOGY DESK BASED ASSESSMENT
NQ.PA.27	ECOLOGICAL IMPACT ASSESSMENT
NQ.PA.28	GROUND CONTAMINATION DESK STUDY AND RISK ASSESSMENT
NQ.PA.29	FLOOD RISK ASSESSMENT AND DRAINAGE STRATEGY
<b>Application NQ.2 – Listed Building Consent Application</b>	
NQ.LBC.01	APPLICATION FORMS
NQ.LBC.02	APPLICATION DRAWINGS
NQ.LBC.03	HERITAGE ASSESSMENT
NQ.LBC.04	DESIGN AND ACCESS STATEMENT
NQ.LBC.05	PLANNING STATEMENT
NQ.LBC.06	OUTLINE SEQUENCE OF WORKS REPORT FOR BANANA WALL LISTED BUILDING CONSENT
NQ.LBC.07	ARCHAEOLOGICAL DESK BASED ASSESSMENT

**Table1 - Supporting documents to OPA and LBC applications**

safety issues, have been developed, in consultation with stakeholders and the local community. The Appendices comprise details of the Access Design and Section 7 demonstrates how the design principles could be implemented through the Indicative Scheme.

As noted above, the Indicative Scheme provides an indication of what the overall Proposed Development could look like. It is not submitted for approval, but shows one way in which a development of the type and scale proposed might fit within the Specified Parameters, for which OPP is being sought.

An Environmental Statement has also been submitted as part of the OPA. This document contains the technical environmental assessments that have been undertaken to understand the likely significant environmental and socioeconomic effects of the Proposed Development and to inform and enhance environmental performance using mitigation and monitoring measures where feasible. The Environmental Impact Assessment has been based on the Control Documents, assessing worst case scenarios as relevant to each technical topic, and where appropriate it has taken into consideration the Indicative Scheme.

Additional technical reports that fall outside of the ES complete the suite of supporting application documents.

Table 1 on the adjacent page provides a complete schedule of the documents which support the applications being made.

### **What is the basis for the consideration of future 'Reserved Matters' applications?**

Whilst not made at this time, RMAs may be made for Development Plots and/or Development Zones in the future. Reserved Matters applications will be considered on their merits in the context of the OPA (in particular the Parameter Plans, the Development Specification and Design Guidelines).

# 1.2

## PURPOSE OF GUIDELINES

The purpose of this document is to determine a design language for the Proposed Development and to establish a robust framework that encourages high quality and rich diversity when it comes to the detailed design process.

This document sets out a series of design guidelines that any future Reserved Matters Application, for the development of any of the Development Zones and/or Plots defined in the Parameter Plans or open spaces between them, should follow unless there is a good and justified reason to depart from them.

The Masterplan places importance on the spaces or 'places' that are to be created, including streets, parks, spaces and water areas, rather than only the individual buildings that enclose them. Development Plots defined within Development Zones will provide the edges to these spaces.

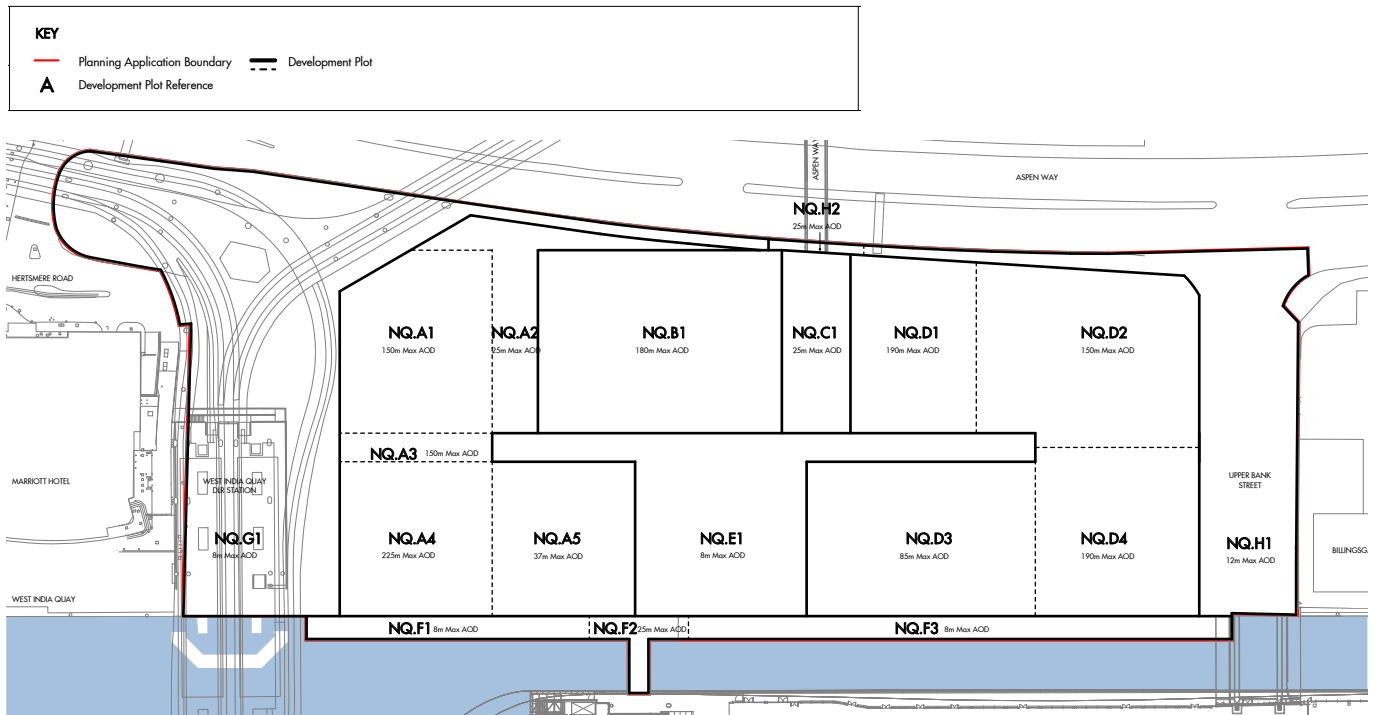
The Design Guidelines have therefore been organised around defining each place within the Masterplan rather than each Development Zone. The Design Guidelines for each place are intended to provide guidance for future design teams. They are intended to ensure the overriding design philosophy is followed, yet allowing sufficient flexibility to encourage variety and richness within the Proposed Development. They should not inhibit creativity but instead establish a platform from which to begin a detailed design.

As such, an Architect considering the design of a building or open space within a Development Plot would need to demonstrate how their design approach meets the Design Guidelines set out for places surrounding it.

For ease of understanding, the elements of the Design Guidelines which are higher priority are identified in **bold text**. The analysis of each place and their significant characteristics which may influence the surrounding Development Zones are indicated in plain text.

In order to test and validate the OPA, an Indicative Scheme showing the potential location of buildings, uses and open spaces has been produced. This scheme provides a vehicle for examining the possible architectural, environmental, technological, operational and social impacts of the project. It remains schematic but it conforms to the development parameters as defined in the Development Specification, Parameter Plans and Design Guidelines (i.e. the Specified Parameters). It has been essential in testing these development parameters. The Indicative Scheme is not a design template or submitted for approval; it represents one possible way the principles as defined in the above listed documents could be interpreted/achieved and developed into a design.

This Indicative Scheme and its Development Plots have been used to generate the images and diagrams for the Design Guidelines. In some instances these Development Plots are used as reference in the Guidelines to help illustrate the point.



Development Plots from the Indicative Scheme (NQ.A1, NQ.A2 & NQ.A3 for example) shown within their Development Zone boundaries defined in the Parameter Plans (A, for example)

## Demonstrating Compliance

This document acts as one of the Control Documents for the OPA. With each RMA that follows the approval of the OPA, the designers should demonstrate how their proposals are 'compliant' with the Parameter Plans and the Design Guidelines and the Development Specification. This should take the form of a Design Compliance Statement.

As the Design Guidelines are intended to act as guidance rather than strict code, the intention is that each RMA should always set out to meet all the relevant key points within the Design Guidelines that are applicable to the proposals contained within that application.

It is however recognised that there may occasionally be instances where there is a good and justified reason to depart from one or more of the guidelines. In these instances, a clear and robust justification should be made for the reasons for deviation from the guidance enclosed in this document. This justification should be put forward for reasonable consideration and acceptance by the LPA.

# 1.3

## STRUCTURE OF THE DOCUMENT

The public realm of North Quay will be composed from a collection of unique places, each with their own distinct characteristics and opportunities. These public places have been categorised into Routes (Section 2.0) and Spaces (Section 3.0) that are described within the Design Guidelines.

Section 2.0 and 3.0 are subdivided into chapters for the individual places as per the adjacent diagram. The chapters provide a surrogate context for each place and offer guidelines within the broad topics of:

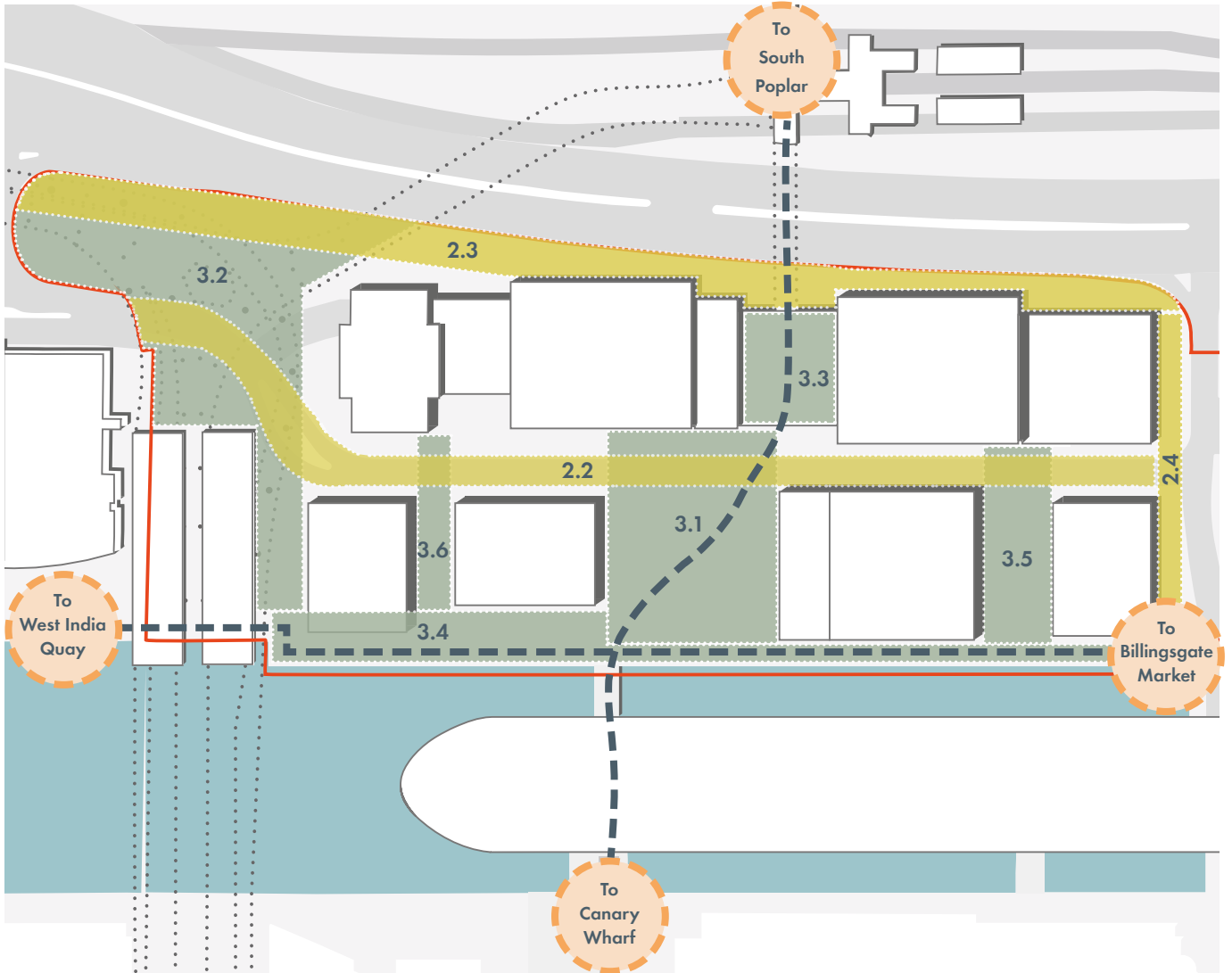
1. Character
2. Public Realm

The above are then further broken down into sub-headings specific to the particular Route or Space being covered. The Guidelines Summary overleaf gives a full list of sub-headings and a brief commentary on the intentions and reasons for each type of guideline.

Along with the sections on Routes and Spaces, there is a further section, Section 4.0, based on Building Typologies. This section covers guidelines relating to the predominant building typologies allowed for within the Masterplan. For example, 'Office Buildings' are discussed as a general grouping, where the chapter does not describe each building in detail. The aim of this section is to ensure the built fabric of North Quay is of the highest standard and that building design enhances the success of the public realm.

The last section 5.0 General Guidelines contain a collection of guidelines on buildings and landscape which are common to various Development Zones and Development Plots across the Masterplan. In addition a Waste & Circular Economy summary sets out the key objectives for each Reserved Matters which come forward.

A Glossary at the end captures all the key terms used throughout the document, with an explanation of each for clarity.



The different Routes and Spaces within the Masterplan shown here in the context of the Indicative Scheme, each annotated with their corresponding chapter number

## ROUTES

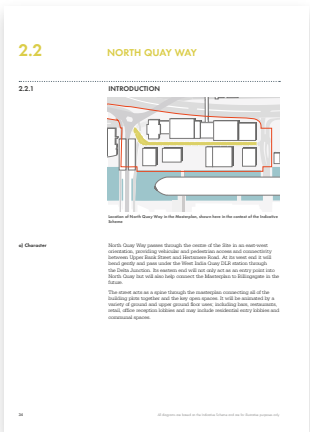
- 2.2 North Quay Way
- 2.3 Aspen Way Gardens
- 2.4 Upper Bank Street pathway
- Key routes formed from interconnecting Routes and Spaces

## SPACES

- 3.1 Quay Square
- 3.2 The Delta
- 3.3 Poplar Plaza
- 3.4 Quayside
- 3.5 Dock Square
- 3.6 Garden Square

1 Character

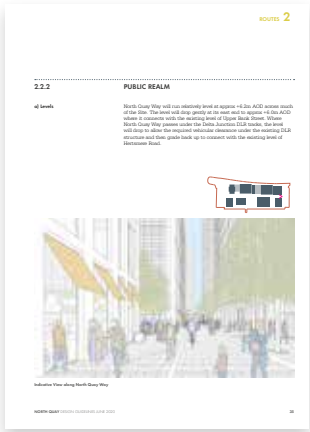
This section aims to capture the essence of each place; its primary function, the use of adjacent buildings and its character.



2 Public Realm

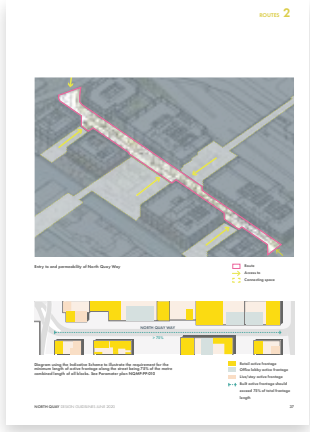
a) Levels

This section deals with the particular opportunities and challenges set by the varying levels across the Site. The intention is to promote a positive and inclusive attitude to any level differences and to optimise accessibility into buildings and the public realm.



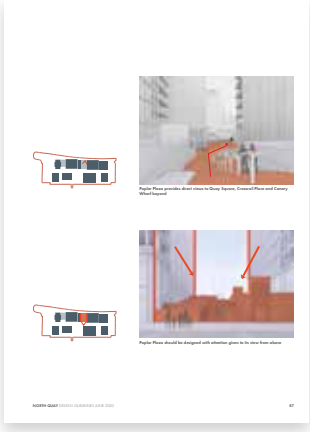
b) Access, Entries & Permeability

Here the key characteristics of each place are summarised. Diagrams to indicate key vehicle and pedestrian routes, functions, active frontages and building entrances within the Masterplan. It also describes the various entry points to each place and how it connects to adjacent areas.



c) Views

This section analyses the key views for each place, both looking towards it and away from it. It describes how the massing of the Development Zones will influence the experience of each place within the public realm. Certain guidelines protect important views between the place and its adjacent areas.





## d) Landscape

This section outlines the physical treatment of the public realm pavements, carriageways, and public places, both hard and soft.

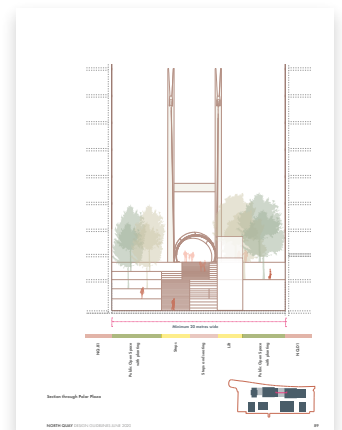
In each instance it should be read in conjunction with Chapter 2.1 Routes-Site Wide.



## e) Scale & Enclosure

This section sets out the priorities for urban places. The underlying intent is that all public places - whether routes, spaces or water areas - should be strongly defined by the built environment which surrounds it. Groups of buildings should form a unified urban 'backdrop' to each place, rather than a collection of individual and unrelated object-like buildings.

Building facades should give priority to the act of enclosure around the place that they face onto. This section deals with the built envelope within the different Development Zones and Plots, and the positive impact of these buildings the placemaking in the public realm. It encourages diversity in massing between different Development Plots enclosing a public place and responds to existing massing conditions in neighbouring areas.



# 1.4

## PRINCIPLES OF THE MASTERPLAN

A masterplan is not a graphic pattern; it is a description of vital relationships. The long-term implementation of a Masterplan of this scale and complexity depends on maintaining the flexibility implied in these relationships. There is flexibility within the layout, where roads, paths, passages and building boundaries are not fixed but can be subject to levels of deviation. These deviations are carefully considered to ensure the Masterplan can continue to ensure to secure the deep-rooted design principles which underpin it.

These principles have evolved from an understanding of the context to promote the creation of specific urban character while preserving the open-ended and dynamic possibilities required to sustain the future health of this urban quarter. Our analysis of North Quay and the contextual framework related to the development of the Site has culminated in a number of guiding principles — principles which will remain valid and robust even as inevitable changes in the financial climate, building regulations, construction practice and technology occur during its implementation.

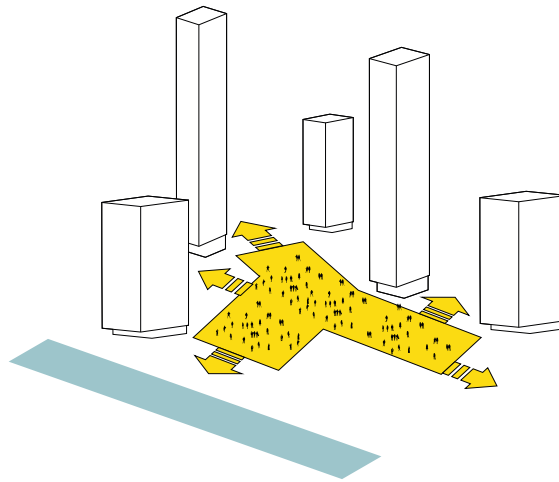
The following ten principles have been developed for this Site and represent a culmination the Applicant brief, extensive analysis of the existing Site and surrounding context, positive attributes retained from previous designs and Allies and Morrisons philosophy as masterplanners.

The principles are crucial to the development of the Site. If the guiding design principles are respected, the details of the Masterplan can evolve according to future needs without detriment to the character of the place or the wider aspirations of the vision for North Quay.

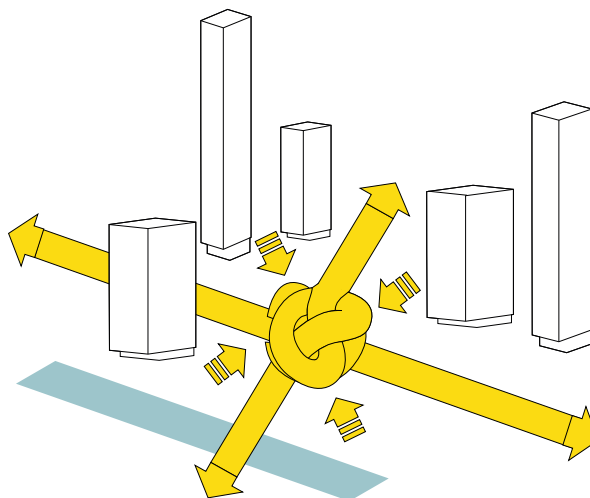
A summary of these principles is outlined below. For further explanation of how these have been applied to the Indicative Scheme, please refer to chapter 4 of the Design and Access Statement (NQ.PA.07).

**Principle One:**  
**Public Realm**

Public realm leads the design through a legible network of high quality streets, routes and safe, accessible spaces, which distribute value and a sense of place across the Masterplan.

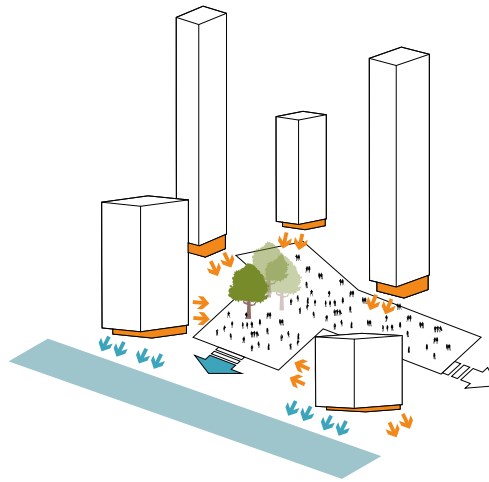
**Principle Two:**  
**Making Connections**

New and existing connections to and through the Masterplan provide resilient, convenient, inclusive and permeable routes for the surrounding communities, emphasising connections for pedestrians and cyclists from South Poplar to public transport modes while allowing on-street drop-off, to be accessible to all. Anticipate future connections to and through the Billingsgate site..



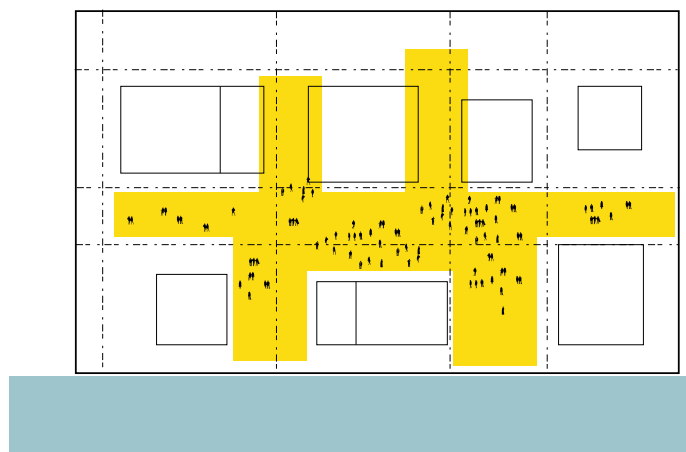
**Principle Three:**  
**Active Spaces**

A variety of public realm and building uses, bringing together live/stay, work, retail and food/beverage, will activate frontages and encourage animation of public spaces, streets and the water edge.



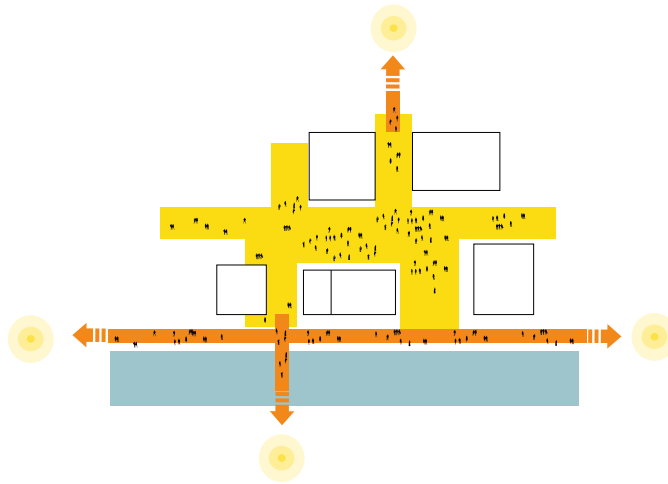
**Principle Four:**  
**A Flexible Framework**

A rational plan arrangement offers greater flexibility in terms of building typology, use and future change, thereby providing inherent robustness that will secure the long-term viability of the Masterplan.



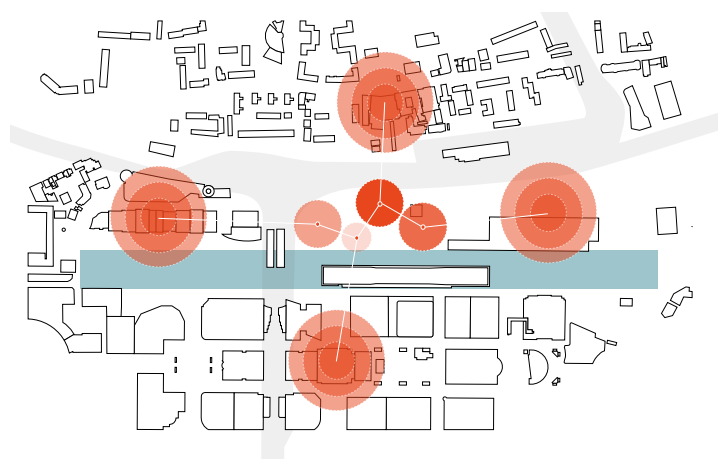
**Principle Five:**  
**Site Responsive**

Allow the unique character and history of the Site to inform the design response, maximising the potential of the accessible and active waterside and the close relationship to Canary Wharf and South Poplar.



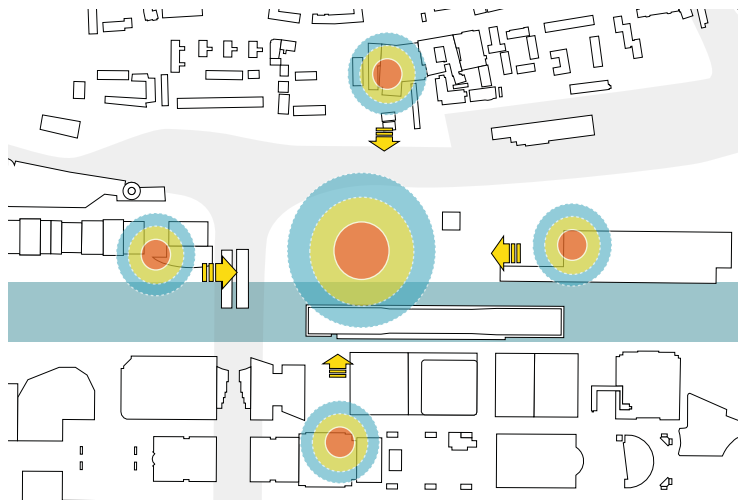
**Principle Six:**  
**Character**

Create a varied and rich character to the different spaces within the Masterplan to enhance and engage with the surrounding context.



**Principle Seven:**  
**Creating Places**

Extend the Canary Wharf business, retail and leisure hub into The Masterplan, with new public space surrounded by shops and restaurants, state of the art offices and live/stay buildings, as an attractive destination for local residents, workers and visitors from further afield.



**Principle Eight:**  
**Townscape**

Building heights and massing should be varied to provide an articulated skyline that responds to the Canary Wharf context but also provides a positive urban relationship to Aspen Way and South Poplar, with urban grain and proportions considered in relation to both the local and wider context.



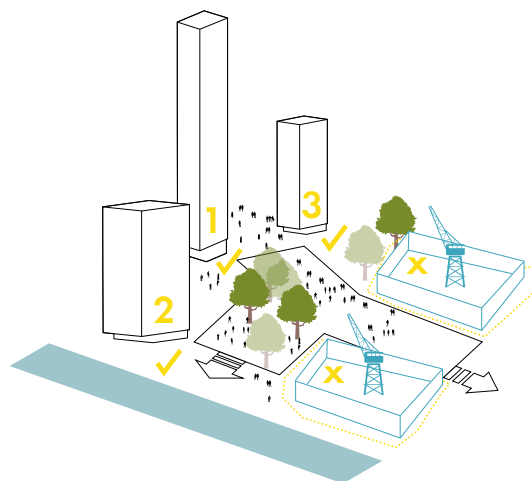
**Principle Nine:**  
**Sustainable Design**

A sensible and holistic approach incorporating biodiversity measures and sustainable strategies, to form an intelligent Masterplan response to environmental issues relating to operational and embodied energy, circular economy principles, water use, pollution, ecology and habitat, and the promotion of health and wellbeing.



**Principle Ten:**  
**Phased Delivery**

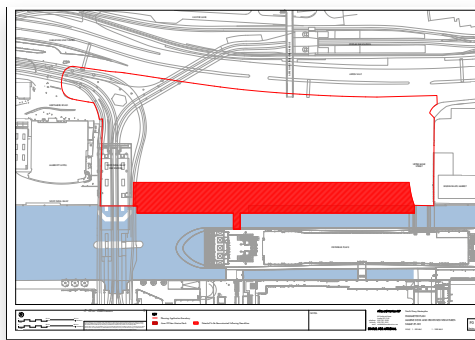
Enable a viable phasing strategy which is adaptable and amenable to future change, allows temporary alternative uses, and where each phase can successfully stand on its own.



# 1.5

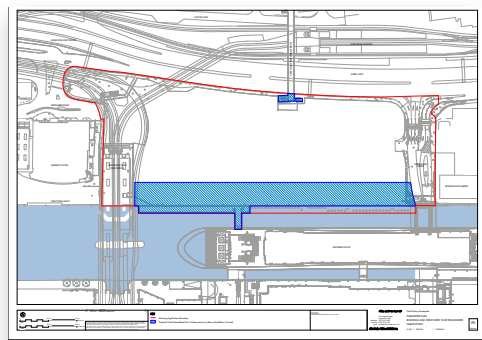
## PARAMETER PLANS

There are 11 Parameter Plans in total. Thumbnails of the full set can be seen on this double page spread. The Parameter Plans (NQ.PA.03.A) form an integral part of the Masterplan and work together with the Development Specification (NQ.PA.05) and Design Guidelines (NQ.PA.04) to control development on North Quay.



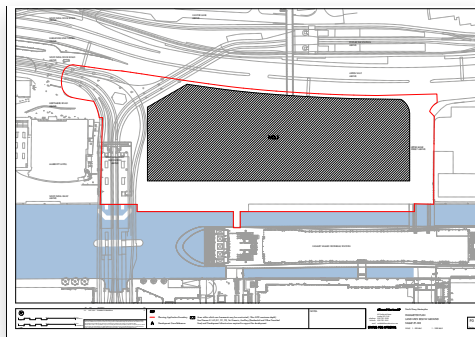
**NQMP-PP-001**

Marine deck and proposed structures



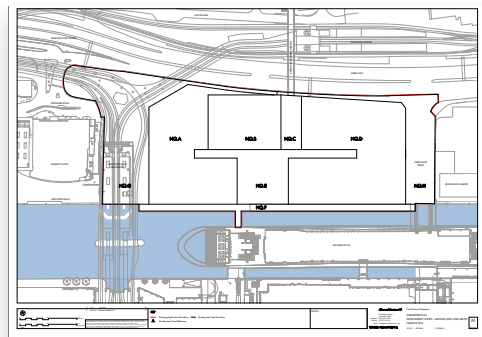
**NQMP-PP-002**

Buildings and structures to be demolished



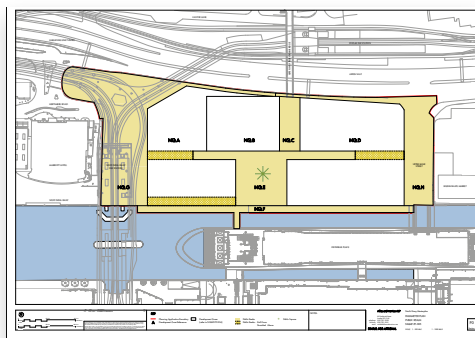
**NQMP-PP-003**

Land uses below ground



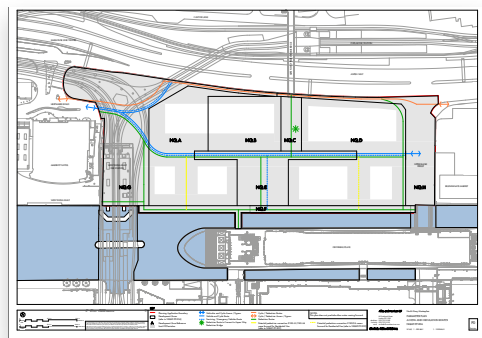
**NQMP-PP-004**

Development Zones ground level and above



**NQMP-PP-005**

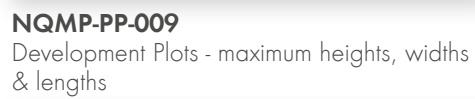
Public Realm



**NQMP-PP-006**

Access and circulation routes





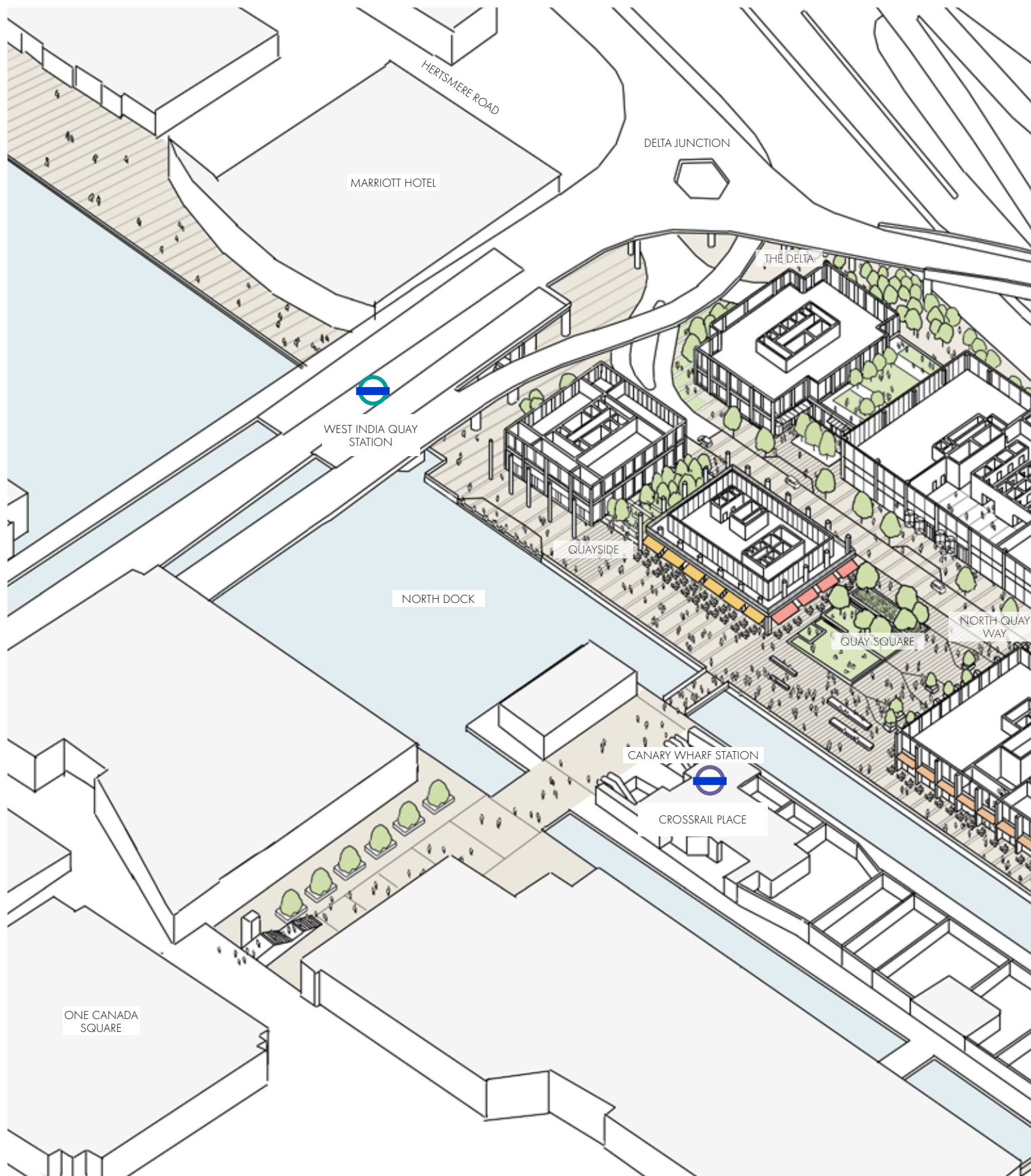


2

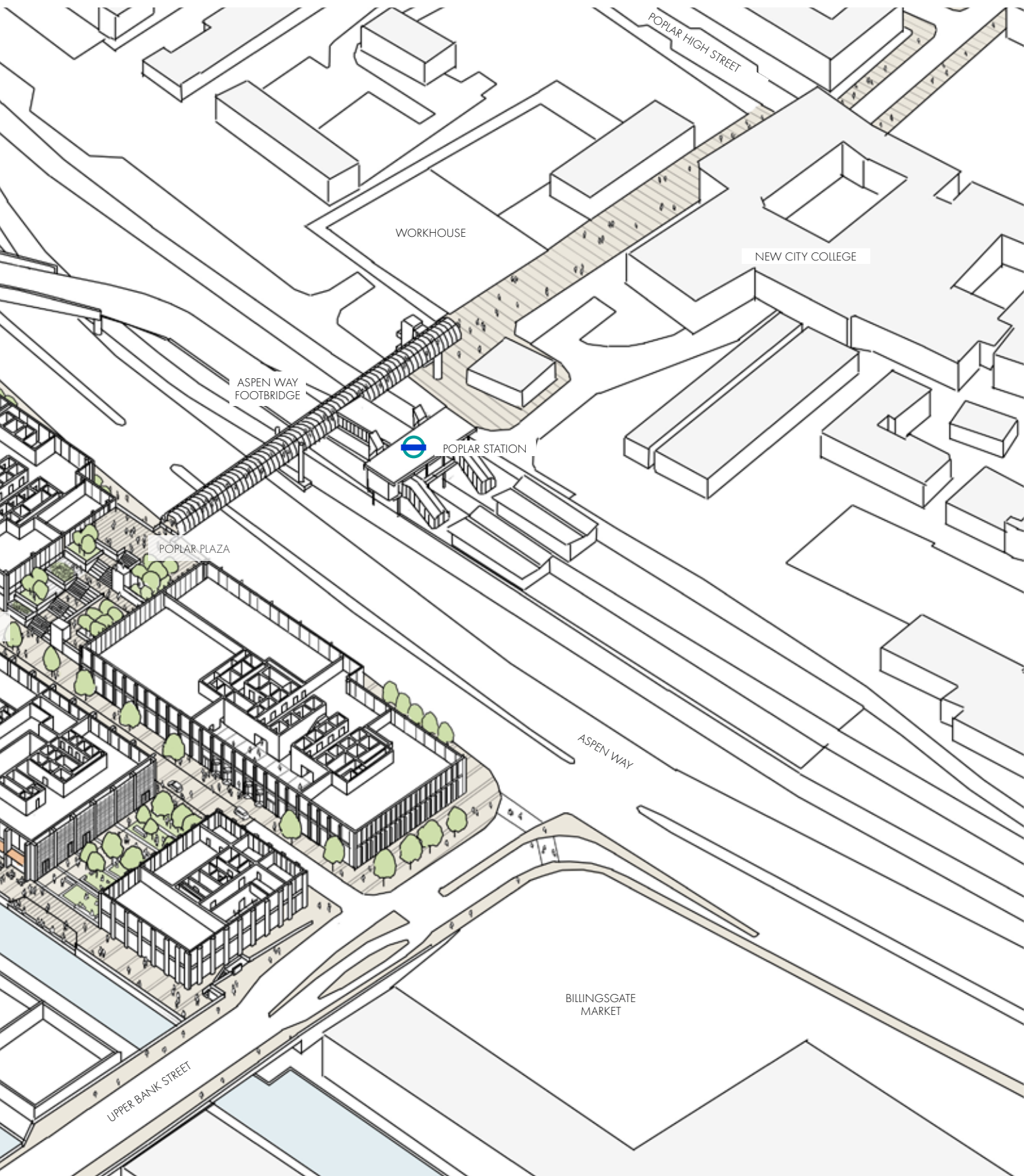
ROUTES

# 2.1

## ROUTES - SITE WIDE



Improved connections to and from the local surrounding area





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## 2.1.1

## INTRODUCTION

### a) Character

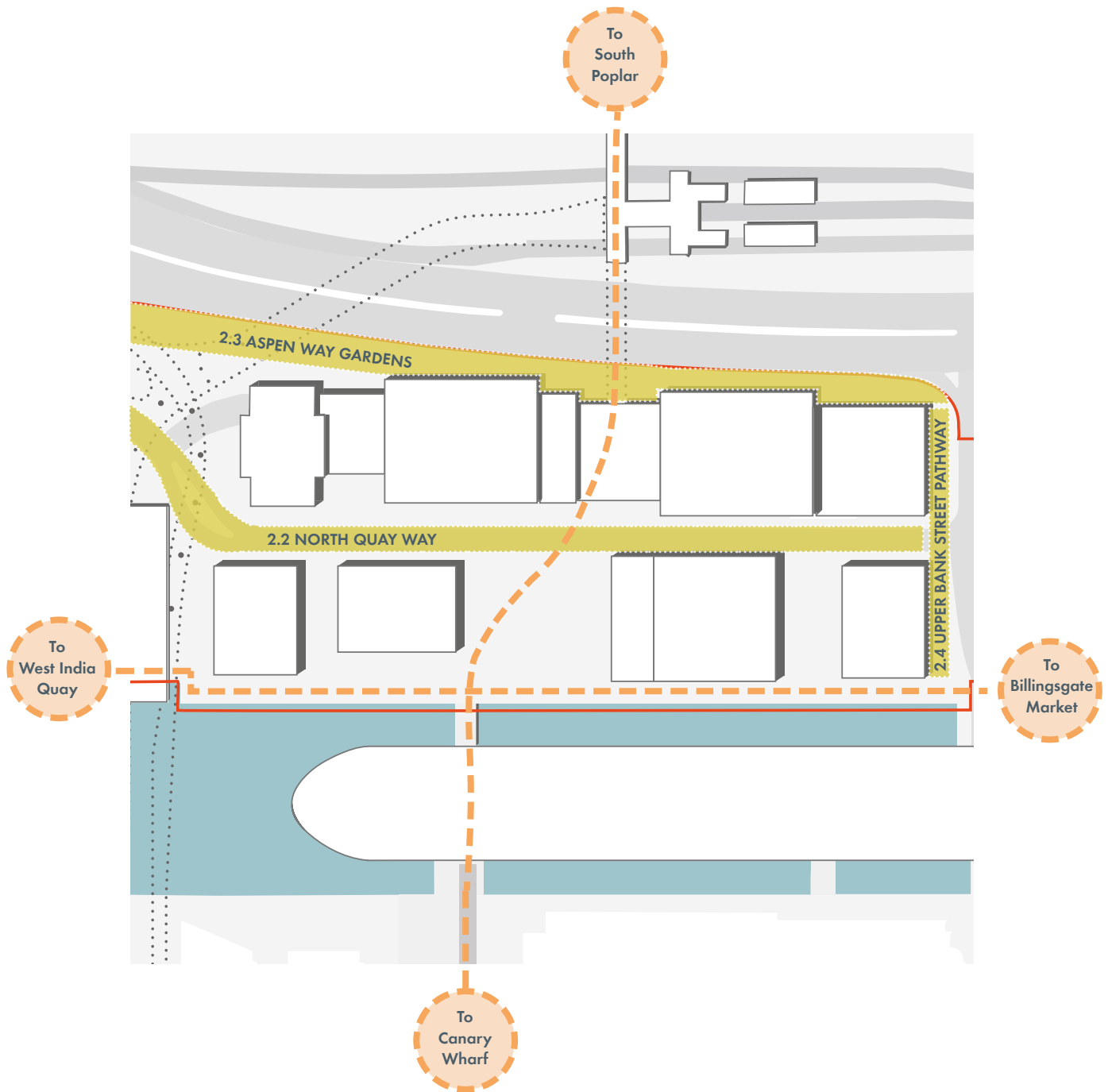
Along with the existing roads that edge the Site to the north and east, a new east-west street (North Quay Way) is positioned through the centre of the Site together with pedestrian routes and spaces which will structure a comprehensive and coherent public realm network, connecting different places and offering a wide choice of routes around and through the Site. These routes will create sufficient space for pedestrian and vehicular movement where required, taking account of access to and between the Development Zones and Plots. Parameter Plan 'NQMP\_PP\_006' defines this route network and hierarchy.

The new North Quay Way will be an important part of the public realm at North Quay. The design of this space is therefore just as critical as the design of the buildings themselves when creating a sense of place.

**All routes should be pleasant to walk along, be well lit and have street furniture and signage integrated into their design where appropriate.**

The Parameter Plans set out maximum heights for Development Plots but there are no minimum heights defined within the Control Documents.

**Development Plot frontages should, however, be of sufficient height to offer a strong definition to the route/space/water edge, and the relationship between the frontage and the place width should be considered.**



#### KEY ROUTES

- Named routes
- Key routes formed from interconnecting Routes and Spaces



Sketch examples of a variety of routes and spaces within the Indicative Scheme. The route and spaces hierarchy at North Quay will have different materials assigned to different places depending on their character and use.



.....

**b) Materials and Design**

The route and spaces hierarchy at North Quay will have different materials assigned to different places depending on their character and use.

**All paving materials should be high quality, durable and resilient, and where possible they should be natural. The palette of materials should be chosen to complement each other and their use should provide continuity between different places within North Quay.**

Material selection across the Proposed Development will be informed based on whole life-cycle carbon analysis and maximise opportunities for circular economy approaches. Residual value of existing site sourced materials will be maximised via reuse for hard landscaping or public art. The public realm will be made more adaptable by specifying moveable planters and limiting the use of adhesives and permanent fixings for timber and steel elements.

Material specification for all elements will meet the requirements set within the Canary Wharf Sustainable Design and Construction Strategy, Sustainable and Healthy Materials Brief, NQ Sustainable Procurement Plan, BREEAM requirements for Materials and Waste. Please see Sustainability Statement (NQ.PA.16) for detailed information.

Some parts of the routes will have tree planting zones that will line the edge of the footpath, lending a generosity to the width of the pavements.

**Street furniture and any ventilation grilles should be integrated into this zone with a rhythm to complement the tree planting. Tree pit grilles should be restricted to this zone and be flush with the footpath so as not to impede pedestrian circulation. Parking spaces and cycle parking should be integrated into this zone. Parking spaces and cycle parking and tree planting should also be coordinated with important building entrances to ensure sufficient footpath space is offered outside the entry point.**

**The placement of street furniture throughout North Quay (including benches, lamps, posts, kiosks, public art etc) is permitted but is not covered by Parameter Plan NQMP-PP-009 which defines the Maximum Height for Development Plots.** For indicative guidance and illustrative precedents and principles of appropriate street furniture, refer to the Design and Access Statement (NQ.PA.07).

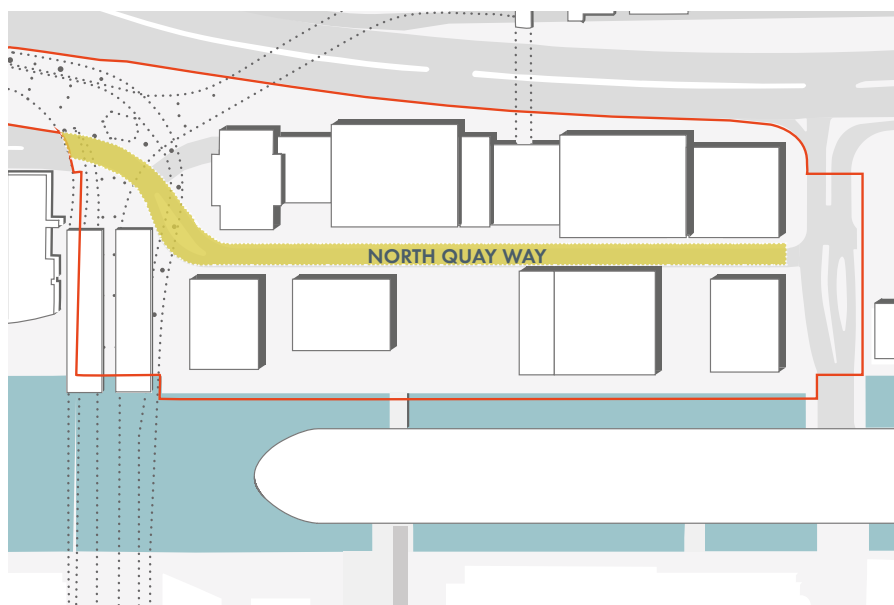
**The design of each route should be cognisant of its microclimate and take measures to ensure a pleasant environment is created for people to enjoy. Built frontages should be designed to ensure routes achieve adequate levels of daylight and sunlight where possible.**

## 2.2

## NORTH QUAY WAY

### 2.2.1

### CHARACTER



Location of North Quay Way in the Masterplan, shown here in the context of the Indicative Scheme

North Quay Way passes through the centre of the Site in an east-west orientation, providing vehicular and pedestrian access and connectivity between Upper Bank Street and Hertsmere Road. At its western end it will bend gently and pass under the West India Quay DLR station through the Delta Junction. A spur road will peel off to the north-western part of the Site with access into a below ground servicing area. Its eastern end will not only act as an entry point into North Quay but will also help connect the Masterplan to Billingsgate in the future.

The street acts as a spine through the Masterplan connecting all of the building plots together and the key open spaces. It will be animated by a variety of ground and upper ground floor uses; including bars, restaurants, retail, office reception lobbies and may include residential entry lobbies and communal spaces. A large proportion of the street should be shared surface to encourage the space to be pedestrian friendly, multifunctional and offer areas for events in conjunction with the various public squares.

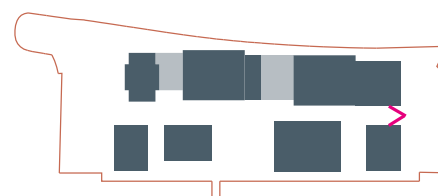
## 2.2.2

## PUBLIC REALM

### a) Levels

North Quay Way will run relatively level at approx +6.2m AOD across much of the Site. The level will drop gently at its east end to approx +6.0m AOD where it connects with the existing level of Upper Bank Street. Where North Quay Way passes under the Delta Junction DLR tracks, the level will drop to allow the required vehicular clearance under the existing DLR structure and then grade back up to connect with the existing level of Hertsmere Road.

**The design of the public realm will need to be assessed in line with the BREEAM Communities Technical Manual 2012 to meet the required BREEAM rating.**



Indicative View along North Quay Way

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## b) Access, Entries & Permeability

North Quay Way will be a fairly busy route with frequent pedestrian movement and occasional vehicular traffic; including cars, taxis and servicing vehicles. It is expected that pedestrian flows will be as high passing across the route from Quay Square through to Poplar Plaza as they will be moving east-west along its length.

The street will be relatively long but its length is broken up by Quay Square, Garden Square and Poplar Plaza. These key civic spaces will punctuate the length of the route and offer a varied character to the street.

**For residential and office buildings with main frontages onto North Quay Way, main entrances to lobbies should be accessed from the street. Where Quay Square, Garden Square and Poplar Plaza meet with the street it is acceptable for the entrance doors to be located so as to engage with the plaza immediately adjacent.**

This will establish an animated character along the street and therefore help to encourage the development of a local community.

Entrances should include one of the following in the design to slow the movement of air from outdoors to indoors within mechanically ventilated main building entrances:

- Building entry vestibule with two normally-closed doorways.
- Revolving entrance doors.

**The minimum combined length and Permitted Uses to provide active frontages along the street is defined in Parameter Plan NQMP-PP-010 (Land Use Building Frontages - Ground, Mezzanine & First Floor Only).** This will establish North Quay Way as an active and lively street and limit the amount of servicing and support spaces on the street frontage.

**Retail units and residential and office entrances should have visually transparent facades which maximises glazed ground floor facade.**

This would help to animate the street edge and enhance the natural surveillance of the area.

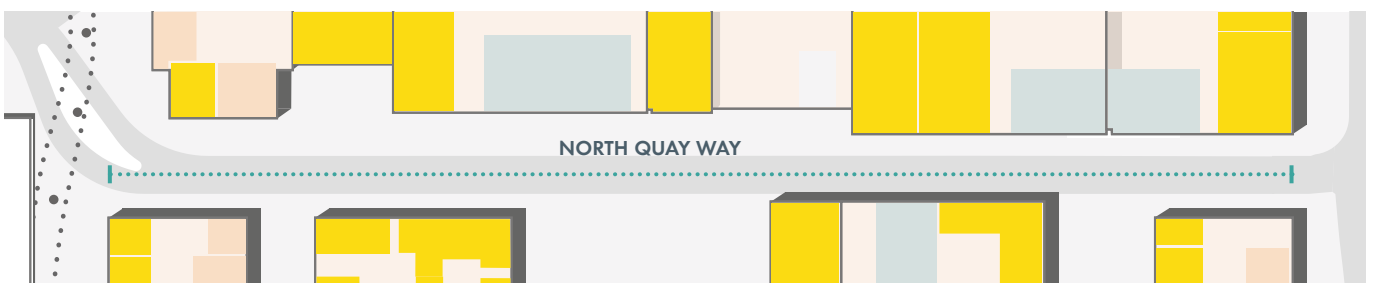
**Any frontage allocated for servicing should be designed to minimise detrimental effects on the public realm. Where ventilation grilles are unavoidable, they should be limited in extent and integrated into the architecture of the building.**

**Parameter Plan NQMP-PP-006 establishes routes passing through Development Zones NQ.A and NQ.D. These routes may be covered if passing through a building structure, or may be open to the sky and between buildings. If covered, these routes should have a minimum clear height of 9 metres and a minimum clear width of 9 metres. This is to ensure that they are both maintained as legible and generous east-west pedestrian routes through the Masterplan. Refer to Section 2.4.2.**



Entry to and permeability of North Quay Way

- Route
- ➔ Access to
- Connecting space



Example active frontage diagram using the Indicative Scheme. See Parameter plan NQMP-PP-010 for requirements.

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage
- Built active frontage. For requirements see Parameter Plan NQMP-PP-010



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### c) Views

The long vista from east to west is terminated by the elevated West India Quay DLR station passing over North Quay Way. The view the other way from west to east is intended to offer visual continuity beyond Upper Bank Street in to the future redevelopment of Billingsgate.

**The landscaping of Quay Square along North Quay Way should be designed to allow views through to Crossrail Place and legibility of the connection through to Canary Wharf.**

**Poplar Plaza is an important transition/arrival point to and from the bridge onto North Quay Way and the landscape should be designed to provide visual legibility from the street to the connection with Aspen Way footbridge.**

The opening up of the route towards Quay Square will help to connect the street with the Quayside. Plots NQ.A5 and NQ.D3 play a particular role in street-level legibility in this part of the Masterplan. They provide an intermediate scale framing Quay Square and provide animation to its edges.

**The design of these buildings and landscape at these intersections should recognise Quay Square, reflecting the presence of this civic space from within the street composition.**

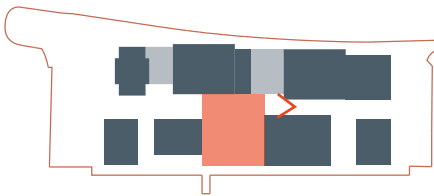
The secondary open spaces of Dock Square and Garden Square will offer views and routes through to the Quayside and Canary Wharf beyond from North Quay Way.

**The massing and built frontage of adjacent plots should provide layered views to the Quayside when viewed from North Quay Way.**

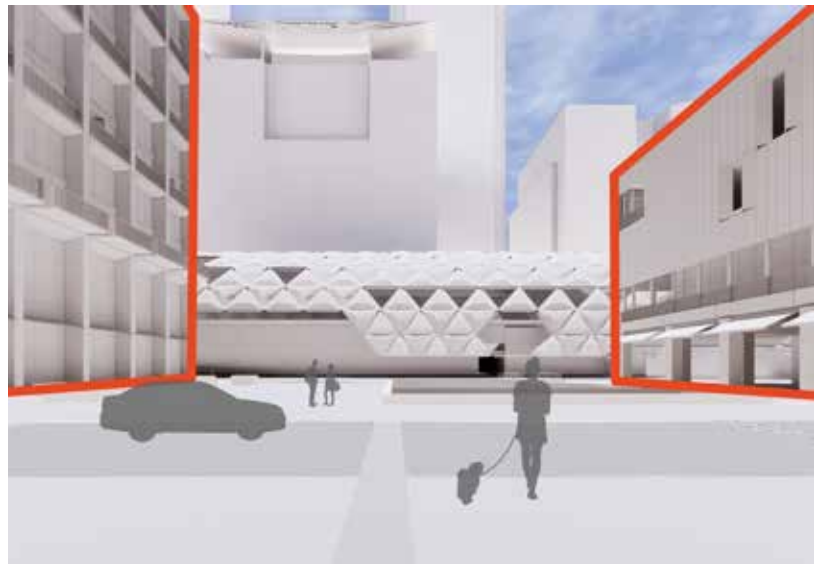
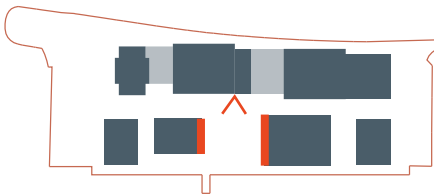
**Landscaping of these spaces should be designed to enhance views of the Quayside and signal the presence of an attractive public space and pedestrian route which lead to the water's edge. These spaces should not be a residual or left-over space, but should be designed to provide a positive amenity in accordance with the specific qualities of the Site.**

**Where Development Plots NQ.A3, NQ.D2 and NQ.D4 may build over North Quay Way, a minimum clear height of 9 metres should be provided from the ground plane to the soffit of the space under the buildings in these areas (see Design Guideline 2.4.2.c). Minimum widths of the opening for the route should be in line with Design Guideline 2.2.2.e(i).**

This is to ensure that an appropriate scale and generosity is provided to this key east-west route running through building plots. These openings should maintain visual legibility through to North India Quay DLR station and the connection to Billingsgate.



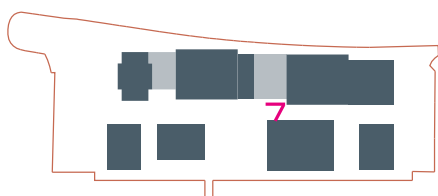
Quay Square along North Quay Way allows for key connection between the street and Quayside



Buildings NQ.A5 and NQ.D3 provide an intermediate scale framing Quay Square



Indicative View along North Quay Way looking into Quay Square







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#### d) Landscape

North Quay Way will be a pedestrian and low key trafficked route running through the Site.

It should have a tree planting zone on at least one side with inset parking/drop off bays/cycle parking spaced along this zone.

**The carriageway should have a raised area at footpath height where it passes through Quay Square.** This will help to encourage cross flow between Quay Square and Poplar Plaza.

**The design of the landscape should enhance the experience of moving through one continuous space. There should also be some level of differentiation between the footpath and the carriageway for those with visual impairments.**

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#### e) Scale and Enclosure

##### i) Massing:

The massing of buildings on North Quay Way will vary according to specific design parameters, with overall maximum building heights of +37m AOD to +225m AOD and frontages ranging in length. This variation should be used to create visual diversity along the length of the street and between individual buildings.

**The distance between any individual buildings should be sufficient to avoid overlooking of habitable rooms and maintain privacy of any residential units. For these purposes, the appropriate horizontal separation should be no less than 18m (excluding any projecting balconies).**

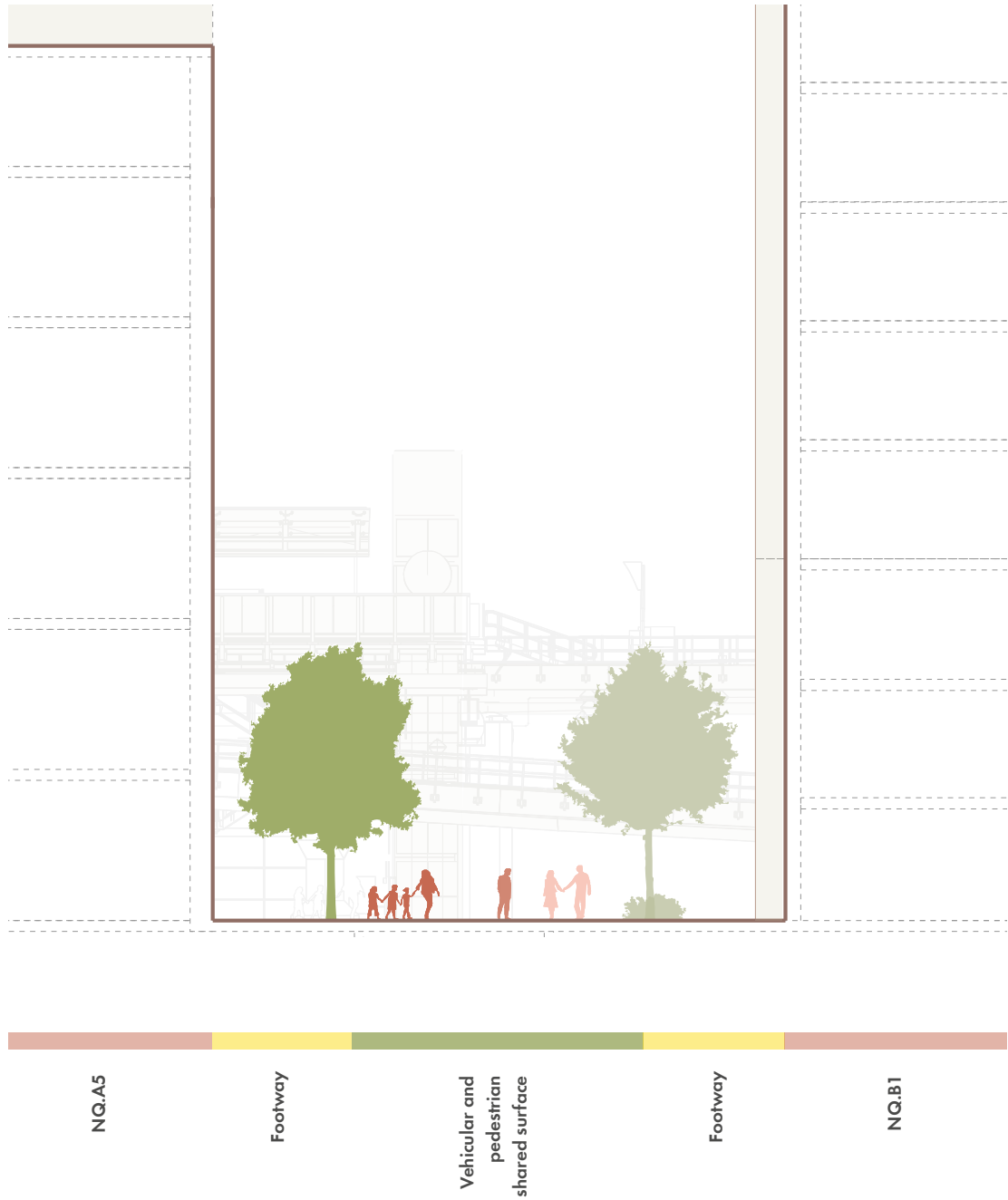
This would improve the aspect from the individual unit while also opening up views between the taller elements on the Site.

**The exception to this is Garden Square adjacent to NQ.A5 where a smaller 12m offset is acceptable due to the low height of the plot. Elevations above ground and upper ground levels should be designed to avoid overlooking to any residential uses in NQ.A4.**

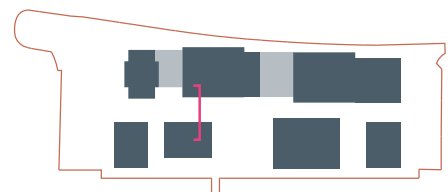
##### ii) Building Lines & Frontages:

**Building frontages along the northern edge of North Quay Way should be designed to consider reflected sunlight into the street.**

This would direct reflected sunlight to the public realm and building frontages on the south side.



Section through North Quay Way

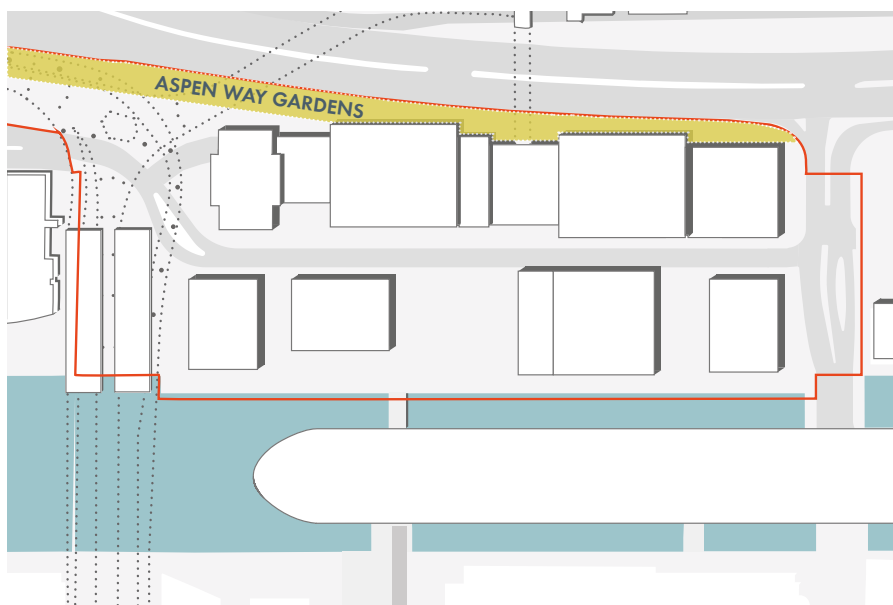


## 2.3

## ASPEN WAY GARDENS

### 2.3.1

### CHARACTER



Location of Aspen Way Gardens in the Masterplan, shown here in the context of the Indicative Scheme

Aspen Way is an existing multi-lane busy primary road forming the northern edge to the Site. It has a very urban character with limited pedestrian movement along its edge with the Site, principally limited to connecting Poplar DLR station with Canary Wharf.

With the extension of the Aspen Way Footbridge connection into the Site and development along the southern side of Aspen Way there is an opportunity to create a more positive edge with connectivity between Hertsmere Road via the Delta and Upper Bank Street and through to Billingsgate in the future. This will form part of a key east-west cycle and pedestrian route.

It should be developed as a landscaped linear space that enables the east-west cycle route and pedestrian connectivity whilst also creating opportunities for informal dwell spaces and engagement. It should be designed to be connected and integrated with The Delta at its western end. The building frontages should respond positively to the space and the facades should undulate in relation to its neighbour to create visual interest. There is a potential that the existing stair and lift could be removed in the future which would improve the space and offer greater permeability from east to west.



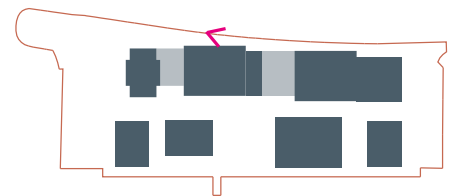
## 2.3.2

## PUBLIC REALM

### a) Levels

The levels of Aspen Way Gardens are informed by the existing levels of the public footpath along Aspen Way. At its junction with Upper Bank Street, this is approx. 4.6m AOD and gently falls along its length to approx. 4.0m AOD at The Delta. There is an opportunity to undulate the landscape between the public footpath and building edges to create softening of this urban edge through landscaping. **This should include a legible east-west route or routes for pedestrians and cyclists.**

**The design of the public realm will need to be assessed in line with the BREEAM Communities Technical Manual 2012 to meet the required BREEAM rating.**



Indicative View along Aspen Way Gardens

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## b) Access, Entries & Permeability

Aspen Way Gardens will, for the foreseeable future, remain a busy urban edge with frequent vehicular traffic along Aspen Way. It is important however that the buildings do not turn their back to this and offers up a positive street frontage. The establishment of safe and well-lit pedestrian and cycle routes through Aspen Way Gardens will encourage east-west movement throughout the day along this edge. This can be positively enhanced by establishing cyclist access doors along the north edge of the Masterplan from Aspen Way Gardens.

**For office buildings with main frontages onto North Quay Way, access for cyclists should be available from Aspen Way Gardens.**

This is to encourage active use of the cycle route along Aspen Way.

**The minimum length of active frontage along the ground floor of Aspen Way Gardens is defined in Parameter Plan NQMP-PP-010.**

This would establish Aspen Way Gardens as an active and lively street frontage and limit the amount of servicing and support spaces on street frontage. This would establish an animated character along the route and therefore help to develop a community looking across to South Poplar.

**Retail units and office spaces looking out to Aspen Way should have visually transparent facades which maximises glazed ground floor facade.** This would help to animate the street edge and enhance the natural surveillance of the area.

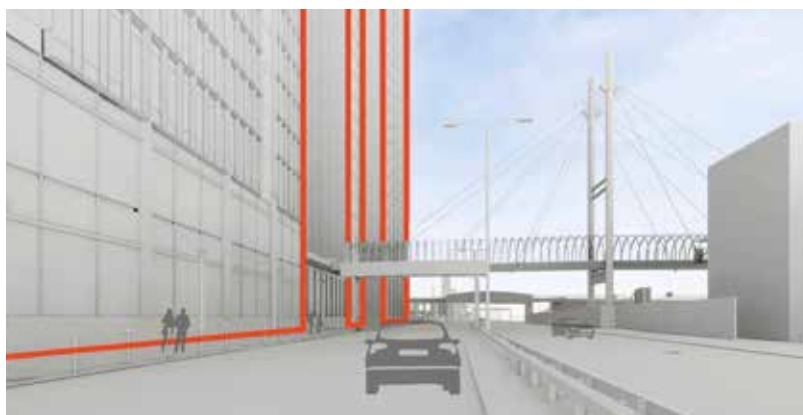
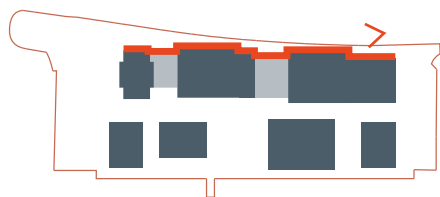
**Any frontage allocated for servicing should be designed to minimise detrimental effects on the public realm. Where ventilation grilles are unavoidable, they should be limited in extent and integrated into the architecture of the building and/or the landscape.**

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## c) Views

The view of North Quay on approach along Aspen Way will be oblique and will be animated by the dynamic composition of the building edges on the edge of Aspen Way Gardens.

Conversely views from South Poplar to the north will be more straight



Buildings on North Quay Way provide a varying edge and a varying skyline to create visual interest

on and the spaces between buildings and their different architectural articulation are important to avoid the perception of a 'wall' of taller buildings.

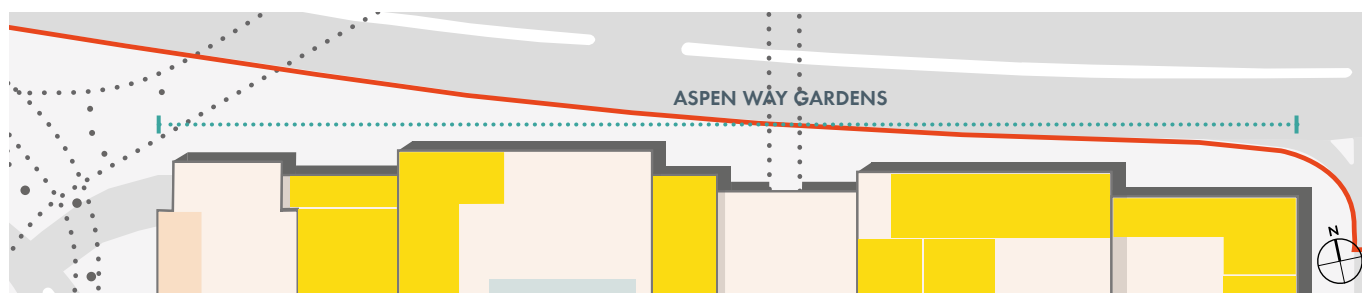
**Each building should be designed to read independently and contrast from its adjacent neighbour through its scale, height, positioning and/or materials. If Development Plots NQ.D1 and NQ.D2 are developed as a single building, then the north elevation should be similarly broken down into at least two vertical parts through a change in scale, height, positioning and/or materials.** This would create a visual interest in the skyline of North Quay when viewed from the north and offer a complimentary collection of buildings to the wider Canary Wharf cityscape.

**Attention should be paid to the dialogue between the different buildings and the interstitial space created between them.** This is to establish a relationship between buildings and to ensure each individual building forms part of a harmonious whole.

**East facing elevations along Aspen Way Gardens should be designed to avoid solar glare effect to vehicles driving westbound along Aspen Way.**

#### d) Landscape

Parameter Plan NQMP-PP-006 establishes public cycle and pedestrian route(s) within this space, linking West India Dock Road and Hertsmere Road through to the northern edge of Billingsgate. As the pedestrian and cycle route(s) meet The Delta, they should be integrated with the landscape design there. **All planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.**



Example active frontage diagram using the Indicative Scheme. See Parameter plan NQMP-PP-010 for requirements.

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage
- Built active frontage. For requirements see Parameter Plan NQMP-PP-010

The design of pedestrian and cycle route(s) should be integrated with the detailed design and evolution of the landscape along Aspen Way Gardens, and should be designed so that natural surveillance of the route is maximised. Cycle parking should be evenly distributed and carefully integrated into the landscaping in this area.

The space should be designed to contribute positively to the chain of public realm spaces within the Masterplan. Landscaping of this area should be designed to clearly and positively define the edge of Aspen Way and signal the presence of North Quay.

The landscape of Aspen Way Gardens should include soft planting and trees to create the sense of a linear garden to North Quay that positively contributes to improving sustainable drainage and air quality along this edge of the Masterplan.

Opportunities for public art and temporary installations should be encouraged.

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#### e) Scale and Enclosure

The Masterplan will provide a key development in making a positive urban relationship between the lower existing buildings in South Poplar and the Canary Wharf Estate. **There should be a distinction between the large individual footprints and scale of the existing Canary Wharf towers and the finer grain of the Site.**

The distance between buildings should be sufficient to avoid the overlooking of any habitable rooms and to maintain privacy of any residential units. **For these purposes, the appropriate horizontal separation should be no less than 18m between buildings (excluding any projecting balconies).** This will also open up long views in between the taller elements towards Canary Wharf and allow daylight and sunlight penetration into South Poplar.

**The scale and heights of the buildings either side of Poplar Plaza should be designed to acknowledge this as a key connecting route and space between buildings along Aspen Way.**

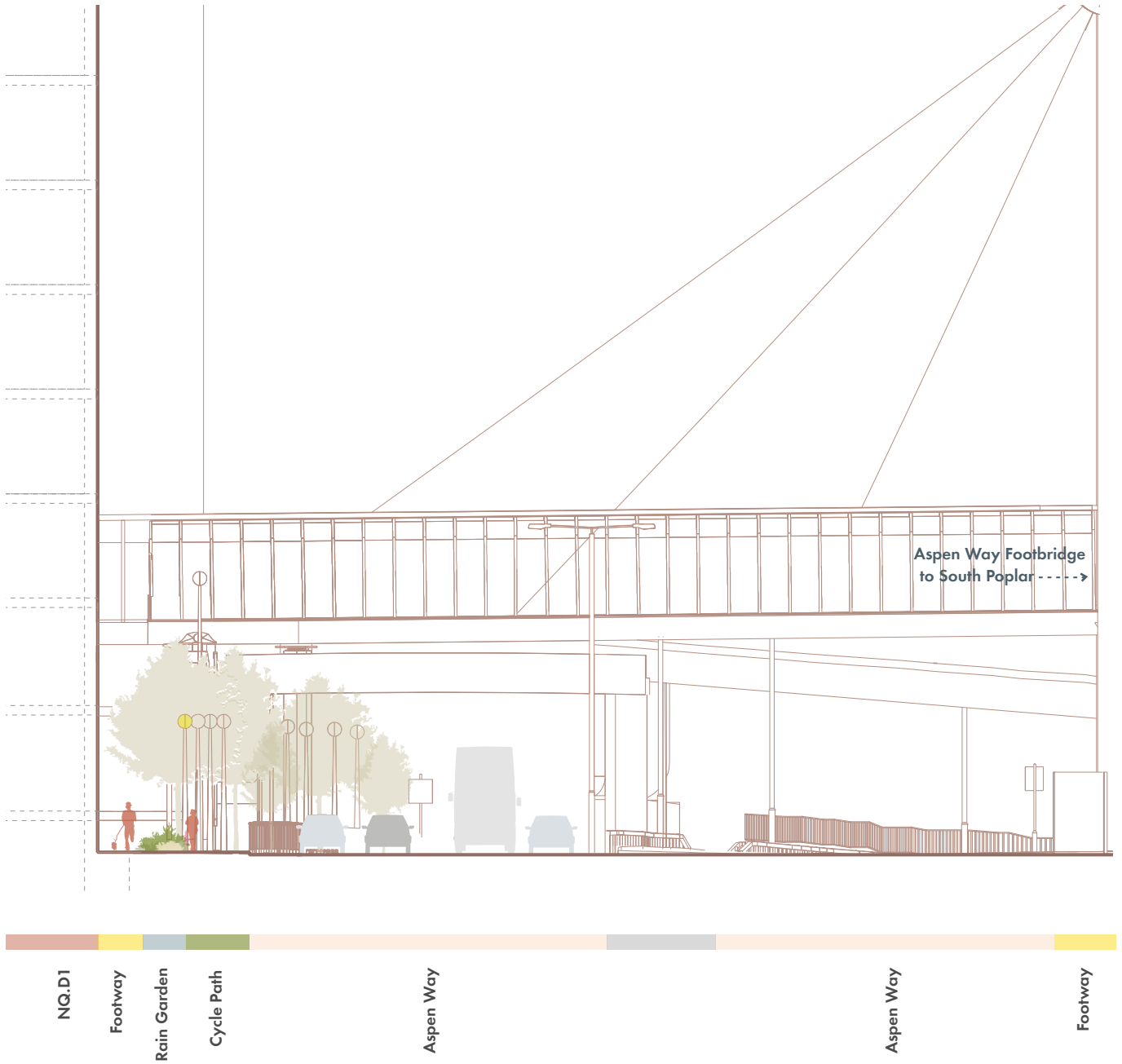
**The alignment of the northern edges of buildings along Aspen Way Gardens should vary such that no two adjacent buildings are in continuous alignment in plan.**

This is to create variety to the width of Aspen Way Gardens and to form pockets of landscape within this linear space.

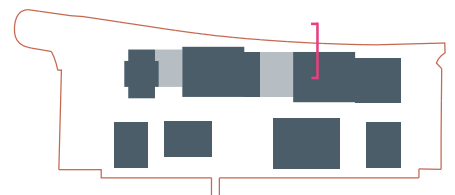
**The heights of the northern elevations of buildings along Aspen Way Gardens should vary such that adjacent buildings should have a minimum 20 metre height difference between each other.**

This is to ensure that the buildings do not form the perception of a continuous 'wall' along Aspen Way and create an articulated and varied skyline in townscape views from South Poplar. Refer to Section 4.2.3.





Section through Aspen Way Gardens and Aspen Way

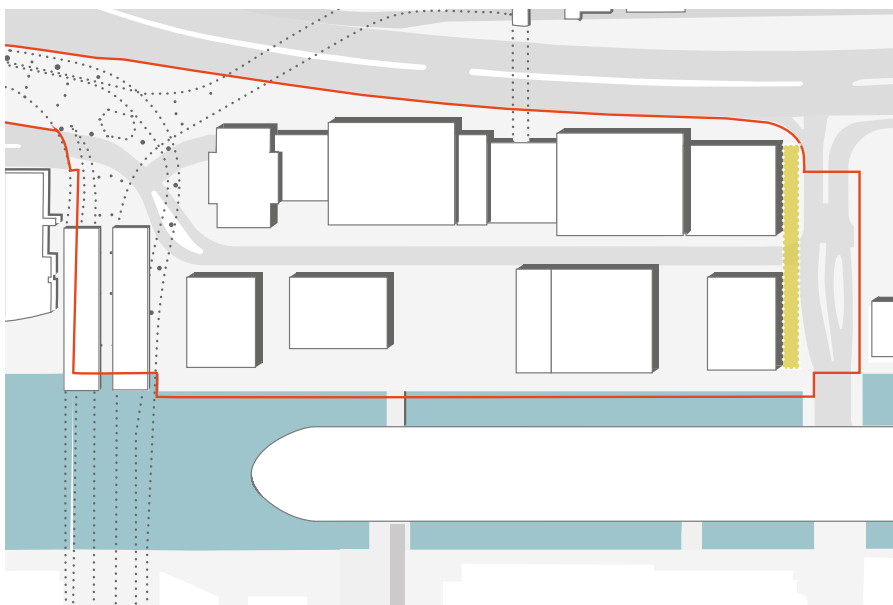


## 2.4

## UPPER BANK STREET PATHWAY

### 2.4.1

### CHARACTER



Location of the pathway of Upper Bank Street in the Masterplan, shown here in the context of the Indicative Scheme

Upper Bank Street is an existing primary road along the eastern edge of the Site forming an important vehicular entrance and exit to the Canary Wharf estate from Aspen Way. The street rises up from Aspen Way, crossing over the dock and passing through Crossrail Place. Billingsgate is located on its eastern side.

With development on the Site and future development on Billingsgate there is an opportunity to create active frontages/entrances on to Upper Bank Street to provide activation to this piece of street and also to enhance cycle and pedestrian crossing for east-west permeability.

## 2.4.2

## PUBLIC REALM

### a) Levels

The greatest difference in ground levels throughout the Site will occur along Upper Bank Street. The levels are informed by the existing levels of the footpath and highway along Upper Bank Street as it connects Aspen Way through to the Canary Wharf Estate. At its junction with Aspen Way, this is approx. 4.6m AOD and then rises with a steady gradient to approx. 6.0m AOD where it will interface with North Quay Way, rising further to approx. 9.8m AOD as it crosses through Crossrail Place.

There is an opportunity to manage the change of levels between the existing footpath and building edges to create positive building frontages that will address Upper Bank Street and soften this urban edge through tiered or layered landscaping.

**Due to the level differences inherent along Upper Bank Street, there may be a vertical tolerance of up to 500mm between internal ground floor levels and external levels, whilst offering a level threshold.**

**The design of the public realm will need to be assessed in line with the BREEAM Communities Technical Manual 2012 to meet the required BREEAM rating.**

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## b) Access, Entries & Permeability

Upper Bank Street already occupies a key position in connecting Aspen Way to Canary Wharf. It will become one of the main gateways to the Site, with drop off routes likely to pass through here in addition to other vehicular movement. The Masterplan will also help to make the environment along Upper Bank Street an attractive option for pedestrians and cyclists.

Access to the Site is currently provided for vehicles from Upper Bank Street. The Masterplan proposes a connection to Upper Bank Street from North Quay Way in a similar location. **Safe and legible access should be provided from Upper Bank Street to North Quay Way for vehicles, pedestrians and cyclists. The remodelled footpaths of Upper Bank Street should connect to those of North Quay Way.** The design of this pedestrian route should mitigate any negative effects the level difference has on the public realm and accessibility. This would help to increase pedestrian traffic on to Upper Bank Street as it would aid the flow of people from North Quay Way.

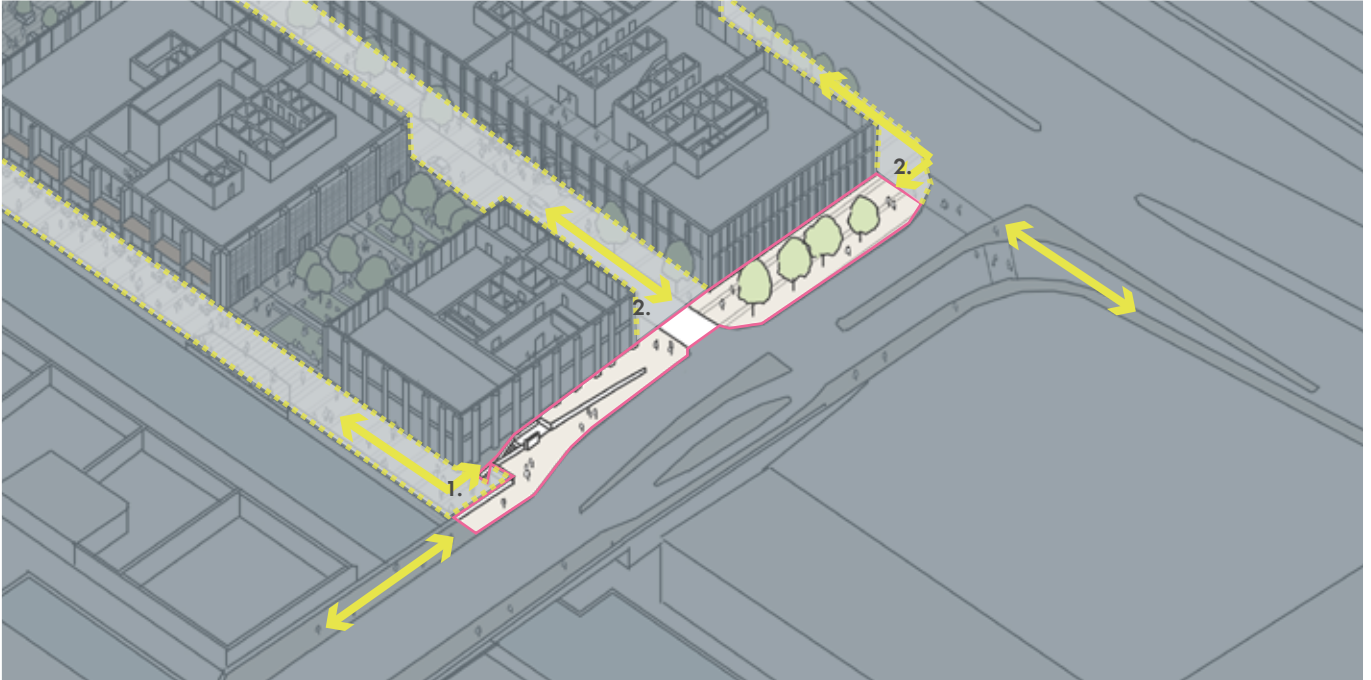
The intention for Upper Bank Street is to re-classify it as a place with active frontages and main entrances. **The new buildings and public realm should help to signal the presence and importance of North Quay Way.** This will help Upper Bank Street to read as an entrance to both North Quay and Canary Wharf. This should also anticipate the future re-development of Billingsgate.

To the south of North Quay Way, there are currently railings and a vertical drop along the interface of Upper Bank Street with the Site. **Access for pedestrians should be provided from Upper Bank Street down to the Quayside adjacent to or within Development Plot NQ.D4. This should be fully inclusive access to the Quayside.**

**The building frontages along Upper Bank Street should be predominantly active frontage.** The requirements for active frontages and Permitted Uses to provide active frontages along the street are defined in Parameter Plan NQMP-PP-010 (Land Use Building Frontages - Ground, Mezzanine & First Floor Only). This would establish Upper Bank Street as a more active and lively street and limit the amount of servicing and support spaces on street frontage.

**Entry levels should have visually transparent facades and should maximise glazed ground floor facade.** This will help animate the street edge and enhance the natural surveillance of the area.

**Any frontage allocated for servicing should be designed to minimise detrimental effects on the public realm. Where ventilation grilles are unavoidable, they should be limited in extent and wherever possible integrated into the architecture of the building.**



#### Entry to and permeability of Upper Bank Street pathway

1. Fully inclusive access from the Site up to Upper Bank Street pathway
2. Step free access from the Site to Upper Bank Street pathway

- Route
- ➔ Access to
- Connecting space

The area of Upper Bank Street within the red-line boundary includes the security infrastructure into the Canary Wharf Estate which is adjusted slightly to allow secure access into the Site without compromise the secure line. The junction between North Quay Way and Upper Bank Street is designed to be “left in, left out” i.e. vehicles can only enter the Site when travelling north on Upper Bank Street. The same security standard for elements which are part of the Hostile Vehicle Mitigation (HVM) for the Canary Wharf Estate will cover all security elements introduced as part of the Proposed Development.



The building frontages along Upper Bank Street should be predominantly active frontage.  
See Parameter plan NQMP-PP-010

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage

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### c) Views

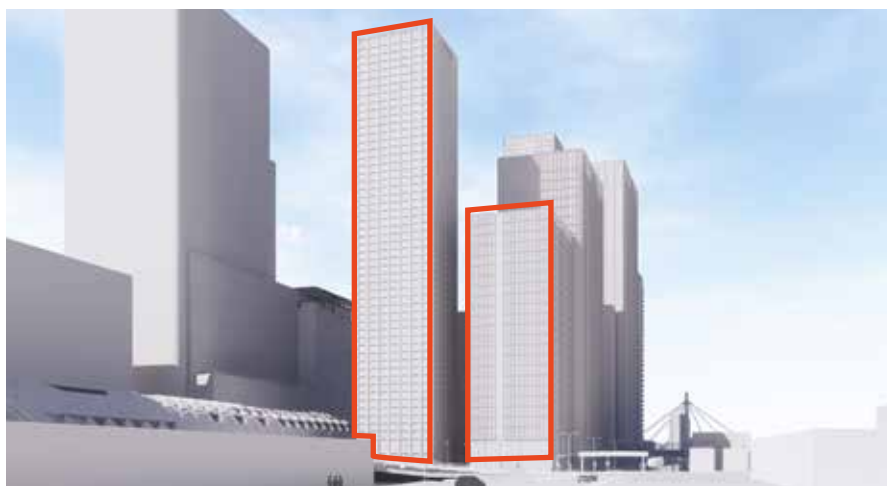
Due to the current open arrangement of Billingsgate, there is a long-distance view of the Upper Bank Street edge of North Quay on approach along Aspen Way from the east.

**Each building should be designed to read independently and contrast from its adjacent neighbour through its scale, height, positioning and/or materials.**

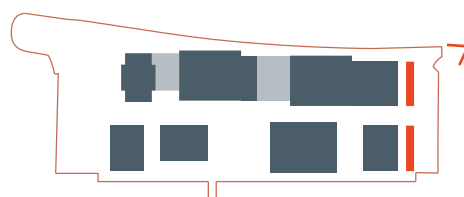
This would create a visual interest in the skyline of North Quay and offer a complimentary collection of buildings to the wider Canary Wharf cityscape.

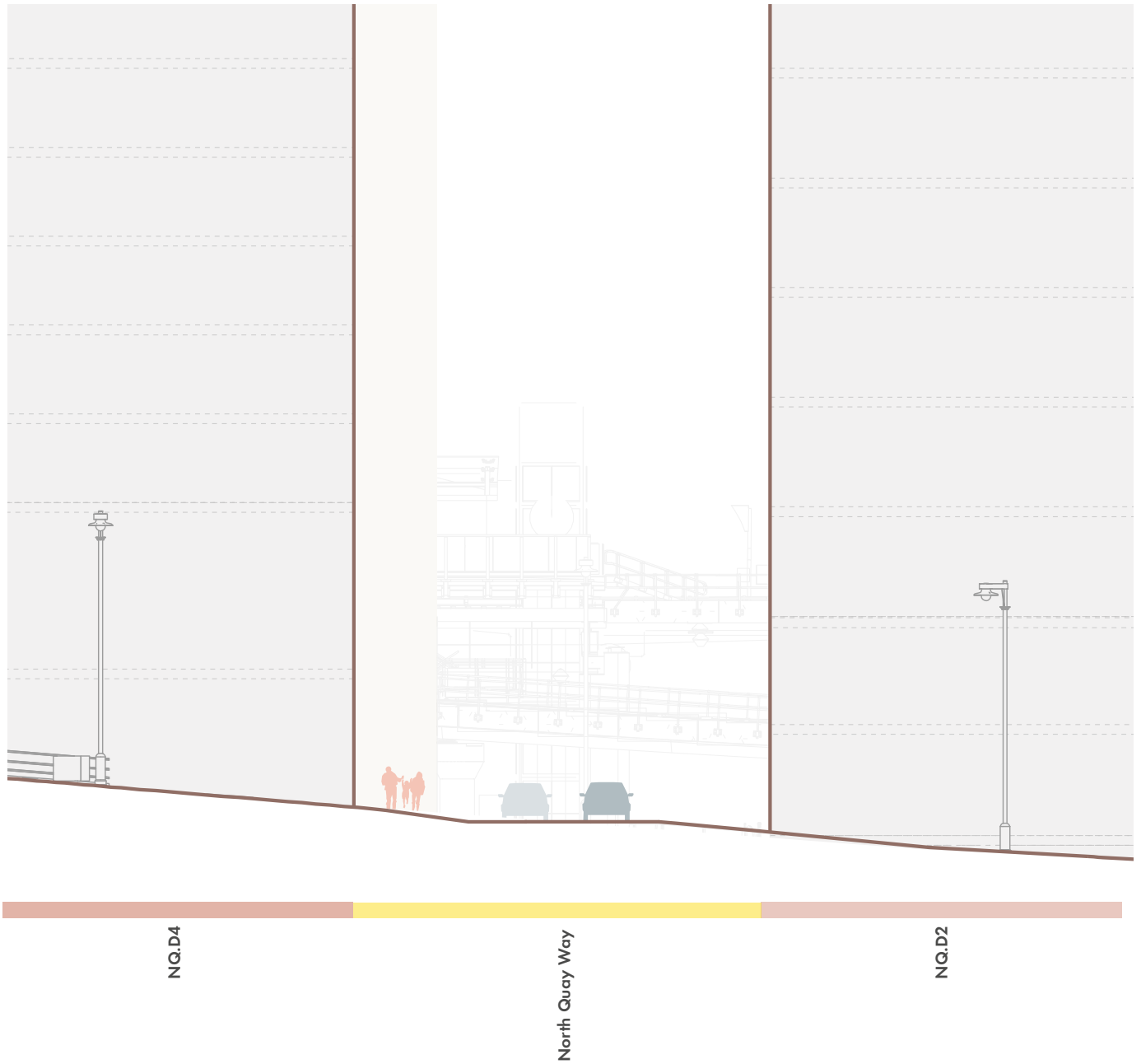
**Attention should be paid to the dialogue between the different buildings and the interstitial space created between them.** This is to establish a relationship between buildings and to ensure each individual building forms part of a harmonious whole.

**East facing elevations along Upper Bank Street should be designed to avoid solar glare effect to vehicles driving westbound along Aspen Way.**

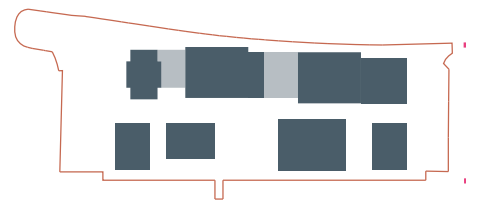


Buildings should read independently to break down the east elevation





Elevation along Upper Bank Street showing the Indicative Scheme

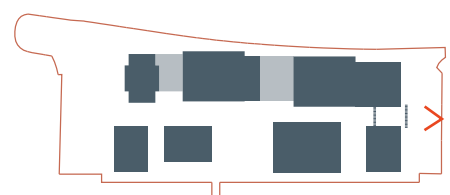


**Any single building that occupies the full elevation to Development Plots NQ.D2 and NQ.D4 along Upper Bank Street should be detailed in such a way as to articulate and break down the scale of the elevation in to a number of parts.** This is such that it does not appear monolithic and also registers the presence of North Quay Way as a key route passing through the Site.

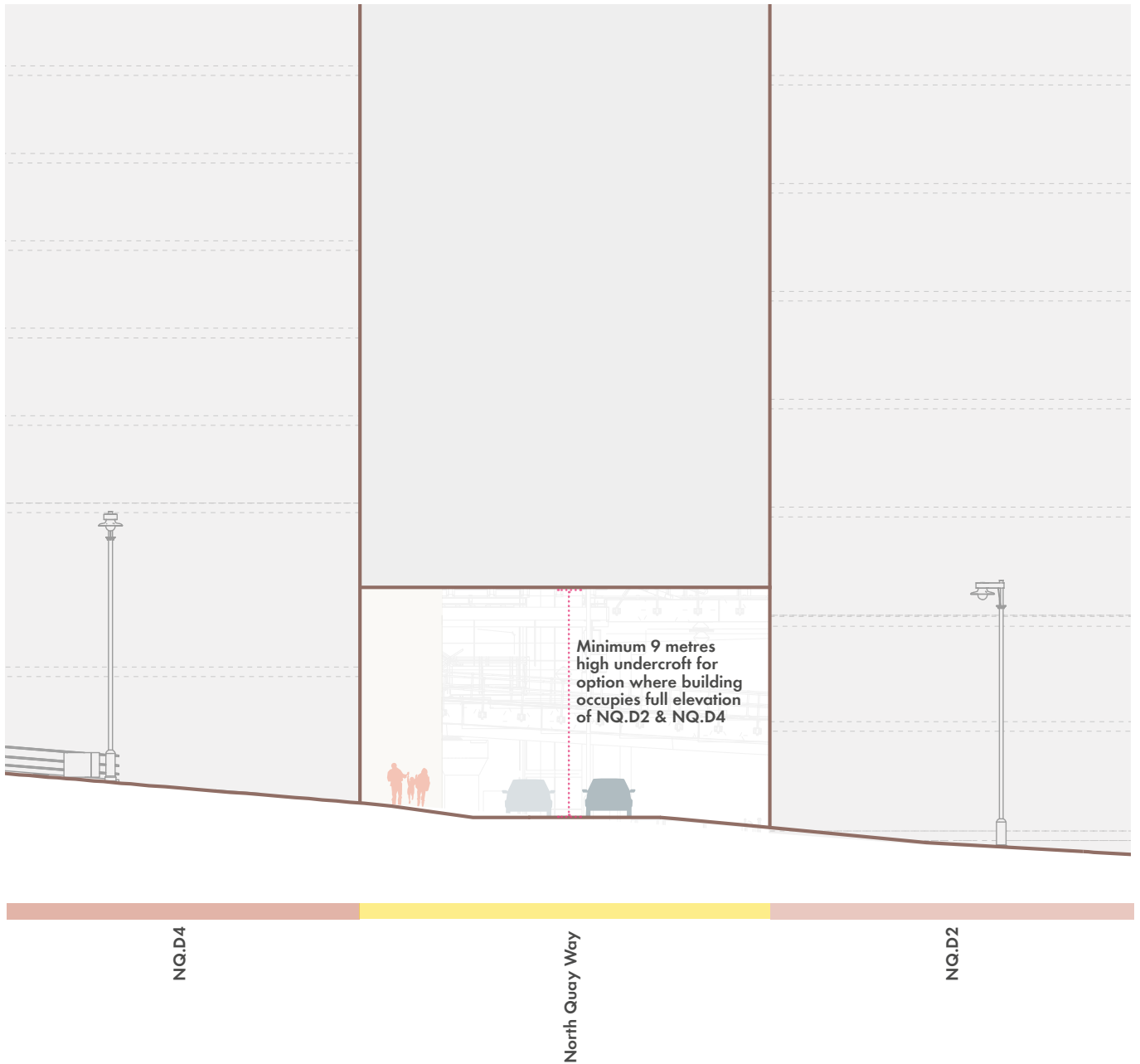
**Any single building that occupies the full elevation to Development Plots NQ.D2 and NQ.D4 along Upper Bank Street should enable a minimum height over North Quay Way of 9m or more, from ground to building soffit.** This should be when measured in the centre of the opening. This will provide a generous route into North Quay and reduce the sense of the enclosure. Refer to Parameter Plan NQMP-PP-006 for locations of routes.



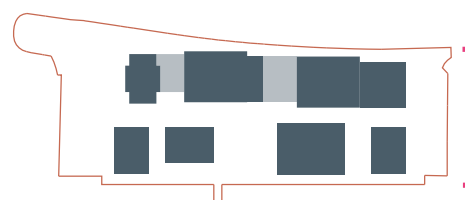
Example image: any single building that occupies the full elevation to Development Plots NQ.D2 and NQ.D4 along Upper Bank Street should create an open and positive connection with North Quay Way.







Elevation along Upper Bank Street showing the option where a single building occupies the full elevation



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#### d) Landscaping

Upper Bank Street is currently a very functional road connection linking Aspen Way with Billingsgate, North Quay and the Canary Wharf Estate with no active frontages along its length north of Crossrail Place. There is an opportunity through this Masterplan to enhance this.

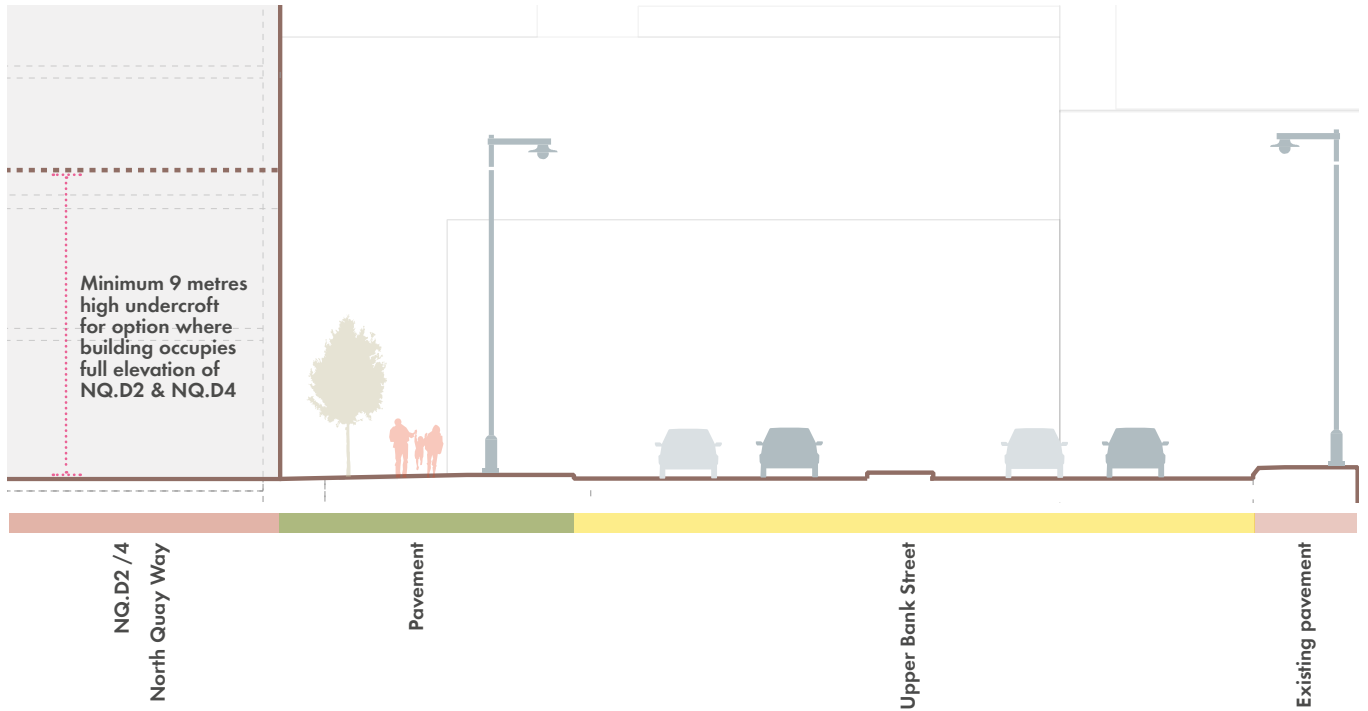
**Upper Bank Street pathway should be designed to contribute positively to the chain of public realm spaces within the Masterplan. Landscaping of this area should be designed to clearly and positively signal the presence of North Quay and enable accessible pedestrian connections to the Quayside. The pavement should be a minimum of 2m wide.**

**The design of the pedestrian and cycle route(s) connection from Aspen Way Gardens and connecting with Upper Bank Street and over to Billingsgate should be fully integrated with the pedestrian and traffic movement requirements and landscape along Upper Bank Street pathway, and should be designed such that natural surveillance of the route is maximised.**

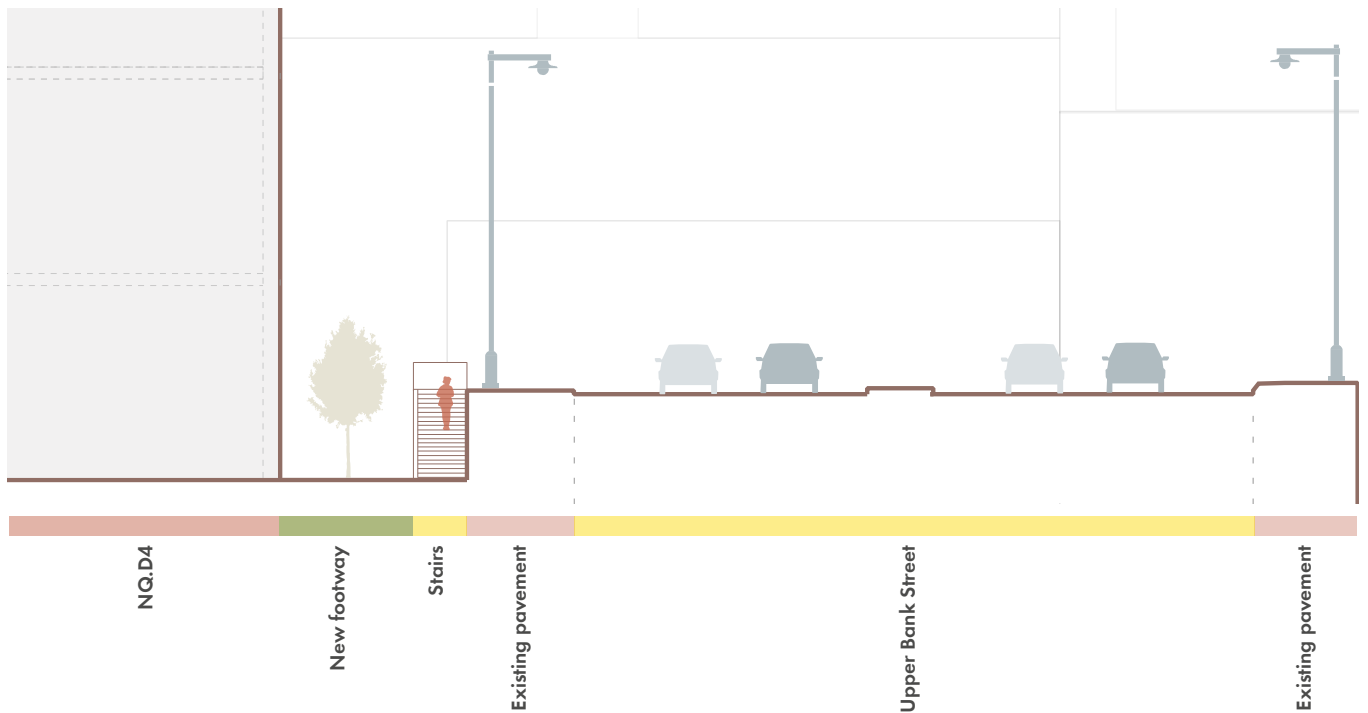
**The landscape of Upper Bank Street pathway should include tree planting to enhance the setting of North Quay along this edge of the Masterplan.**

Opportunities for informal seating around the connection with the Quayside along with public art and temporary installations should be encouraged.

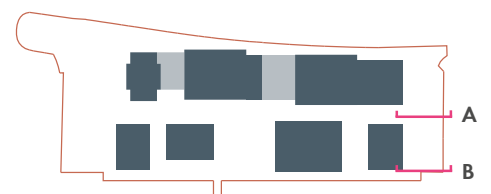
**All planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.**



Section 'A' showing level access from Upper Bank Street to North Quay Way



Section 'B' showing steps up from Quayside to Upper Bank Street





3

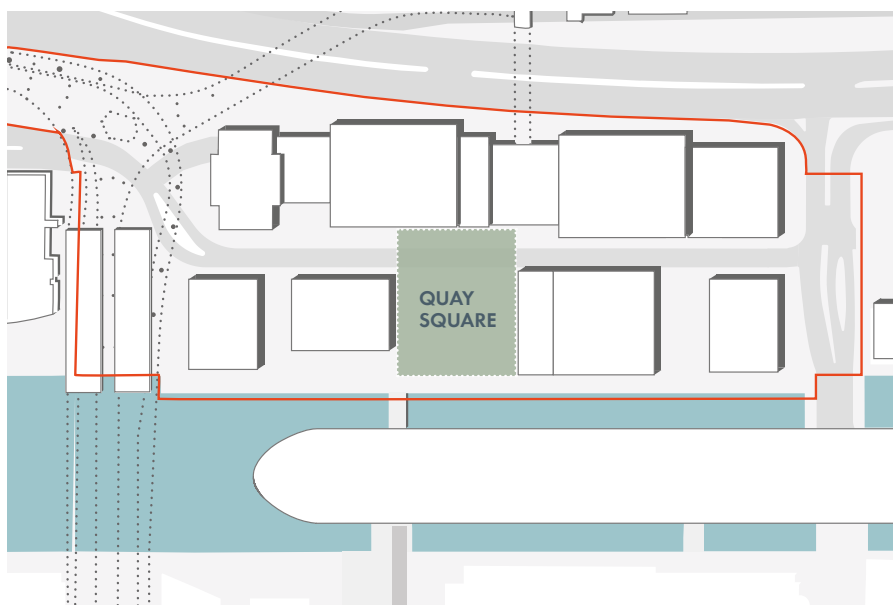
SPACES

# 3.1

## QUAY SQUARE

### 3.1.1

### CHARACTER

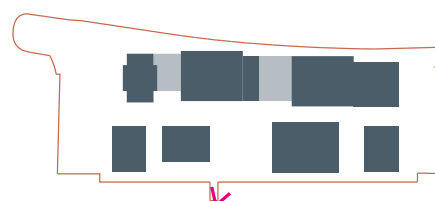


Location of Quay Square in the Masterplan, shown here in the context of the Indicative Scheme

Quay Square is the main formal open space within the Site and sits centrally in the Masterplan, edged by Development Zones NQ.A and NQ.D. It integrates with North Quay Way along its north edge and The Quayside to the south to offer the opportunity of larger event spaces. This square is the heart of the scheme and an important interface with links to Aspen Way footbridge and Crossrail Place.

It will be animated by a mix of uses at ground floor which would comprise bars, restaurants, and shops. On the upper floors setback terraces and projecting balconies will overlook the square. A rich diversity of activity will ensure that Quay Square is established as a place which is dynamic and lively throughout the day and evening.

Quay Square will be designed as a south facing square and will be akin to an European urban piazza capable of hosting a variety of active public fronting uses. It will have a combination of hard and soft landscaping and serve as a public space to dwell. It will have movement through it but also be important as a venue for a variety of events and a desirable destination for visitors to North Quay as well as residents and local workers.



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### 3.1.2

## PUBLIC REALM

#### a) Levels

Quay Square will fall gently away from a level of approximately 6.2m AOD on North Quay Way down to the Quayside level (approximately 5.8m AOD). This level change would take place evenly across the space and therefore the space will have the impression of being almost flat.

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#### b) Access, Entries & Permeability

North Quay will offer a series of interesting public spaces which will connect to each other to provide a series of attractive and animated routes through and around the Site.

Quay Square will sit at the centre of these spaces, where the proposed east-west spine of North Quay Way connecting West India Quay DLR Station with Billingsgate meets the north-south route connecting Aspen Way footbridge to the Quayside, Crossrail Place and Canary Wharf.

**Quay Square should establish connections to adjacent spaces on all sides and should be a fully permeable space that will form the recreational and cultural heart of North Quay.**

It will adjoin and merge with Quayside on its south side and will tie together this east-west waterside route with the centre of the Site.



Indicative view towards Quay Square

**The landscaping treatment of Quay Square should integrate with The Quayside and any threshold between the two spaces should be blurred.**

This would allow these two spaces to read as one interconnected space and encourage movement between them. On its north side, Quay Square will connect to Poplar Plaza.

**There should be a clear visual connection and a legible and friendly pedestrian crossing from Quay Square to Poplar Plaza.**

This would help increase the natural surveillance of the route and provide a safe and welcoming passageway connecting South Poplar through North Quay to Canary Wharf.

The range of routes that will connect Quay Square to the surrounding context will encourage cross-flows of people across the square as they use it as part of their route. It will also be an active space throughout the day as a destination venue, with movement lines expected to be meandering as people cross back and forth to different establishments on either side.

**Quay Square should be designed to be as permeable as possible and enjoy multiple access points and routes through.**

The requirements and the permitted uses to provide active frontages around the square is defined in Parameter Plan NQMP-PP-010 (Land Use Building Frontages - Ground, Mezzanine & First Floor Only). This defines the required minimum retail frontage bordering Quay Square as predominant, however retail frontages should be maximised to these edges where possible.

**Main entrances to retail units (A1-A5 use classes) fronting Quay Square should be accessed from the square where possible. Retail facades should have visually transparent facades.**

This would help animate Quay Square and enhance the natural surveillance of the area. It would also help establish a dialogue between units on opposite sides of the square.

**Generous areas should be allocated to retail/food and beverage uses (A1-A5) for tables and chairs 'spill out' space within Quay Square.** This would establish Quay Square as an active and lively space. These areas should be flexibly planned to allow the square to be adapted/used for different events and in different configurations throughout the year. They should also be positioned to maintain clear pedestrian routes through the space for connectivity and safety.

**There should be no frontage allocated for servicing addressing Quay Square exceeding 3m linear length. Where ventilation grilles, etc, are unavoidable they should be limited in extent and integrated into the architecture of the building.**

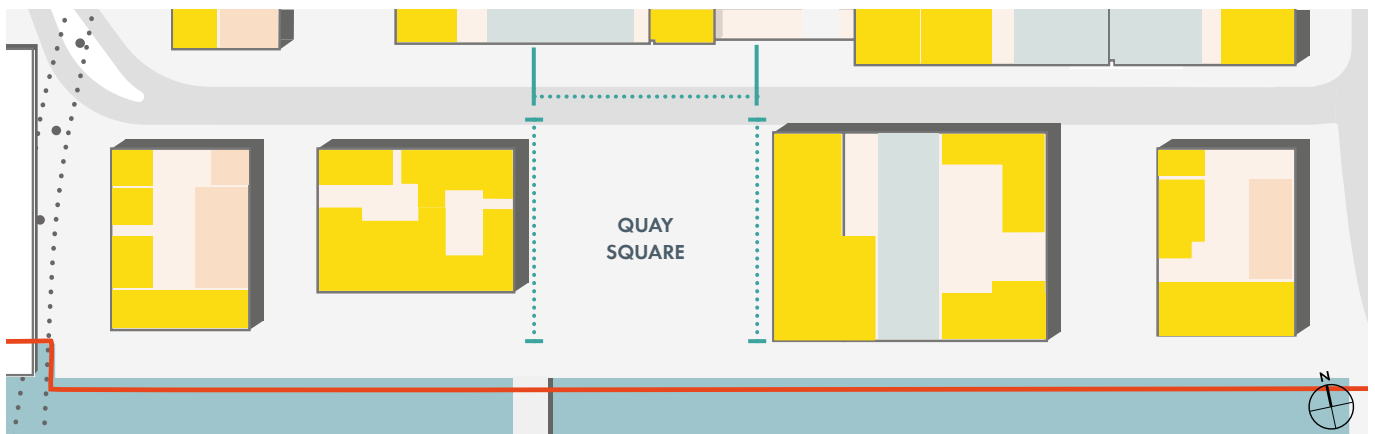
This would limit servicing and support spaces on street frontage.





Entry to and permeability of Quay Square

- Space
- Access to
- Connecting space
- Engage with waterfront



Example active frontage diagram using the Indicative Scheme. See Parameter plan NQMP-PP-010 for requirements.

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage
- Built active frontage. For requirements see Parameter Plan NQMP-PP-010

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## c) Views

The view into Quay Square emerging from Crossrail Place and Canary Wharf will be one of the most significant visual connections created between the Site and the existing Canary Wharf Estate. The presence of this key vibrant retail, food/beverage and leisure use space will attract people to the square.

**There should be a clear visual connection from Crossrail Place to Poplar Plaza across Quay Square.**

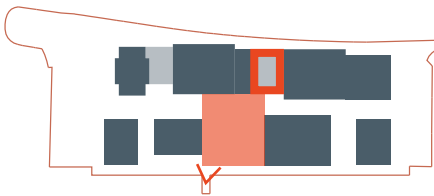
**The elevation of Development Zone NQ.B should be designed to provide a strong visual end piece to the view across from Crossrail Place.**

Quay Square will be positioned in the heart of North Quay and long views will be offered from the space looking south to Crossrail Place and Canary Wharf, east towards Billingsgate and west to West India Quay. It is important that the Masterplan should take advantage of any views to adjacent neighbourhoods where possible in order to increase the Development's integration with its urban context, and to aid natural way finding and orientation within North Quay and to/from adjacent communities.

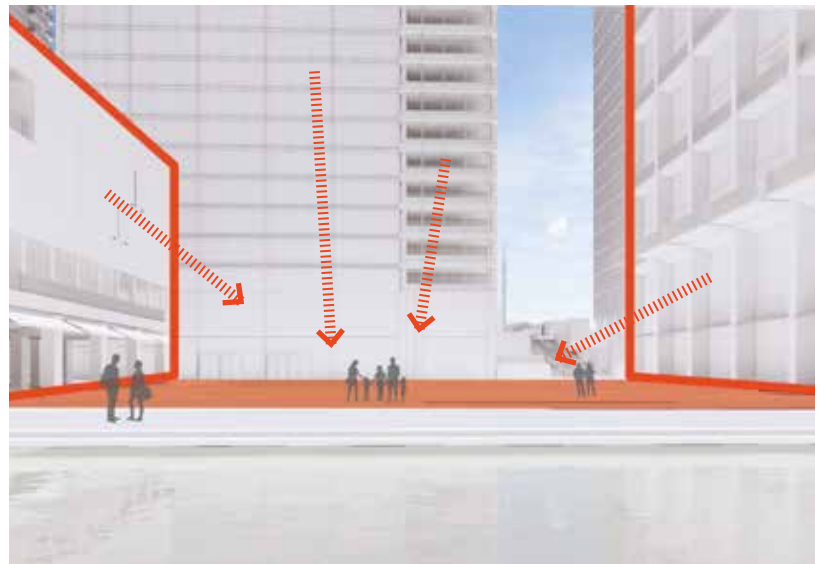
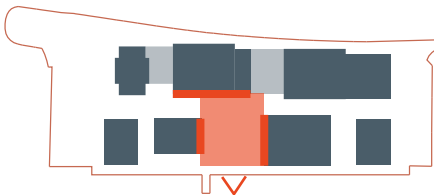
**The design of the landscaping in Quay Square and the interconnecting spaces of The Quayside, North Quay Way and Poplar Plaza should be designed to enhance the views out from the square.**

Quay Square will be overlooked by a mix of office and live/stay blocks, and views into the square from these units provide an important visual amenity.

**Quay Square should be designed with attention given to its view from above. The space should be capable of being well lit at night to maximise its use and security, and to enhance the visual amenity provided to the residential units overlooking it.**



Quay Square provides a clear visual connection from Crossrail Place to Poplar Plaza



Strong edges forming the square. The Square should be designed with attention given to its view from above

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#### d) Landscape

Parameter Plan NQMP-PP-008 defines the flexible upper floor usage of the Development Zones which will surround Quay Square. The square sits in an area where there is flexibility within the Masterplan in terms of the uses and therefore activities that will surround it.

**The detailed landscaping of the square should be informed by the predominant usage and activities in the upper floors of the surrounding Development Zones as they unfold.**

**Regardless of upper floor uses, the square should be lined with active ground level retail uses and the landscaping should therefore account for external terraces, awnings etc.**

Quay Square will be a key space within North Quay which will have many adaptations for public use. It will be a flexible space that will support a variety of functions such as performance events, concerts, artist installations, public gatherings, etc.

**It should be designed as a multifunctional space that can house large groups of people in a number of configurations. It should allow a stage to be set up in various locations. It should not be prescriptive but instead should maximise the flexibility of the square.**

The relationship between Quay Square and the uses adjacent to and overlooking it is important in establishing a unique identity and character in the square.

**Landscaping should not screen the square from the lower level units around it.**

**Any surface car parking/loading bays should not create a visual and physical barrier between the square and Poplar Plaza.**

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#### e) Scale and Enclosure

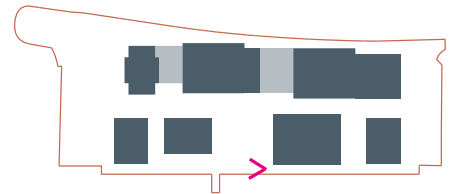
Quay Square will bring light, space and air into to the heart of North Quay. It will help the Site's transition in height, scale, and use from Crossrail Place and the Quayside to the south through to the taller buildings that form the northern edge to the Site.

The scale of the buildings in Zones A and D that frame the east and west sides of Quay Square will vary but will form a consistent enclosure to the square. These frontages will act as the edges to the square and behind them views to the taller buildings on the north of the square will be offered.

**Building frontages on the east and west edges of Quay Square should be shaped to create active vibrant frontages to frame the square.**

Views of the contrasting scales of North Quay would lend a civic richness and urban layering to the square.

**Buildings surrounding Quay Square should be designed to ensure it has a pleasant microclimate that can be enjoyed while standing still or seated, increasing the success of any outdoor events and encouraging use of outdoor seating provisions.**



Indicative view towards Quay Square

#### f) Building Lines and Frontages

It is important that the proportions of the square are sized to create a resonance between retailers in order to promote a civic space in keeping with the character of the North Quay neighbourhood. Refer to NQMP-PP-005 PUBLIC REALM Parameter Plan.

**The square should not be any narrower than 45m from east building face to west building face (excluding projecting cornices, balconies, entrance canopies, etc).**

This minimum width will ensure adequate horizontal separation whilst still offering a sense of enclosure to the square and establish a dialogue between retailers at ground level.

**Building lines along the east and west edges of Quay Square should be parallel to each other.**

This is to define Quay Square as a formal rectangular space, establishing connections to adjacent spaces in all orientations. It will also help unite Quay Square and The Quayside as an L-shaped civic space when required for large scale events.





**A single unified area of no less than 0.4 hectares of open public space should be provided across Quay Square and The Quayside.**

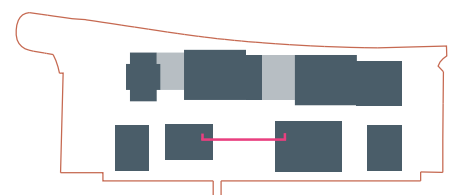
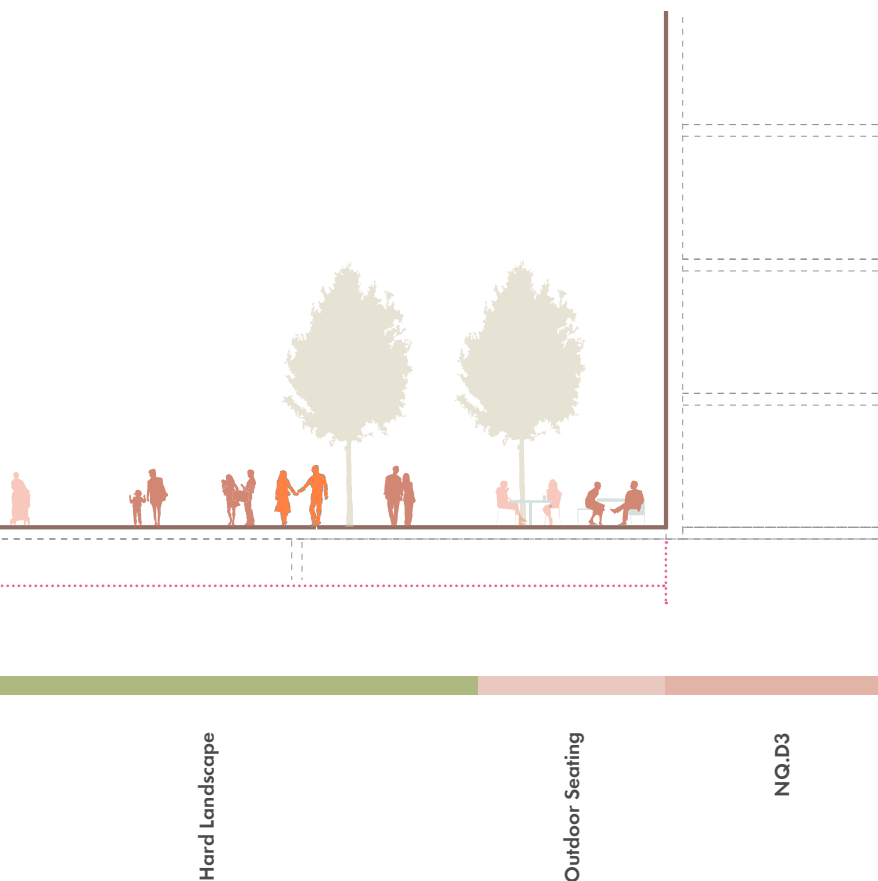
This is to meet the requirements of the key open space provision in the North Quay site allocation in the Local Plan.

**Facades should give priority to street enclosure over the object-like quality of individual buildings.**

The exception to this is Development Plot NQ.A5 where the building has the potential to become a special standalone Small Building within the Masterplan (Refer to Section 4.4)

**Built frontages should be designed to maximise natural light in Quay Square.**

**The design of the public realm will need to be assessed in line with the BREEAM Communities Technical Manual 2012 to meet the required BREEAM rating.**



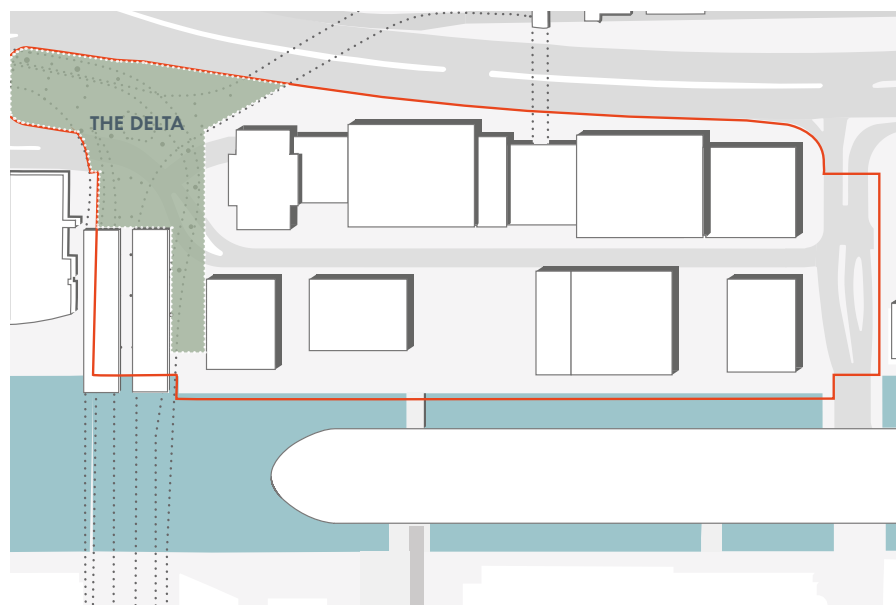


## 3.2

## THE DELTA

### 3.2.1

### CHARACTER



Location of The Delta in the Masterplan, shown here in the context of the Indicative Scheme

The Delta is an area of open space located under the existing elevated DLR tracks at the western end of the Site. The Delta is located to the south of the West India Dock Road/Limehouse Link tunnel approach and to the north of the proposed connection to North Quay Way. It provides connectivity between the edge of Aspen Way and Hertsmere Road and will form part of a key east-west cycle and pedestrian route.

The space is currently urban in character, unpleasant and therefore underused; it has the opportunity to be shaped and softened by landscape and creative lighting. It should be developed as a collection of spaces that enable the east-west cycle route and pedestrian connectivity whilst also creating opportunities for informal dwell spaces, playable landscape and engagement. It should be seen as an extension of the Aspen Way edge of the Site.

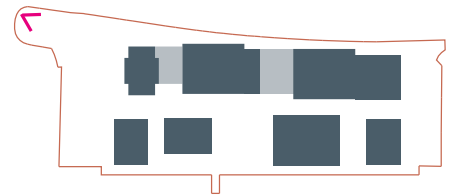
### 3.2.2

## PUBLIC REALM

#### a) Levels

The levels of The Delta are guided by existing levels as built under the DLR tracks and along Aspen Way. These ground levels could be modified locally to provide vehicular access clearance heights under the DLR to access the Site and to create a varied and interesting landscape proposal with raised planting areas.

**The ground levels of The Delta may be modified to create an enhanced landscape but should provide accessible pedestrian and cycle routes through the space and opportunities of informal play. These should also meet the spatial and technical requirements of DLR.**

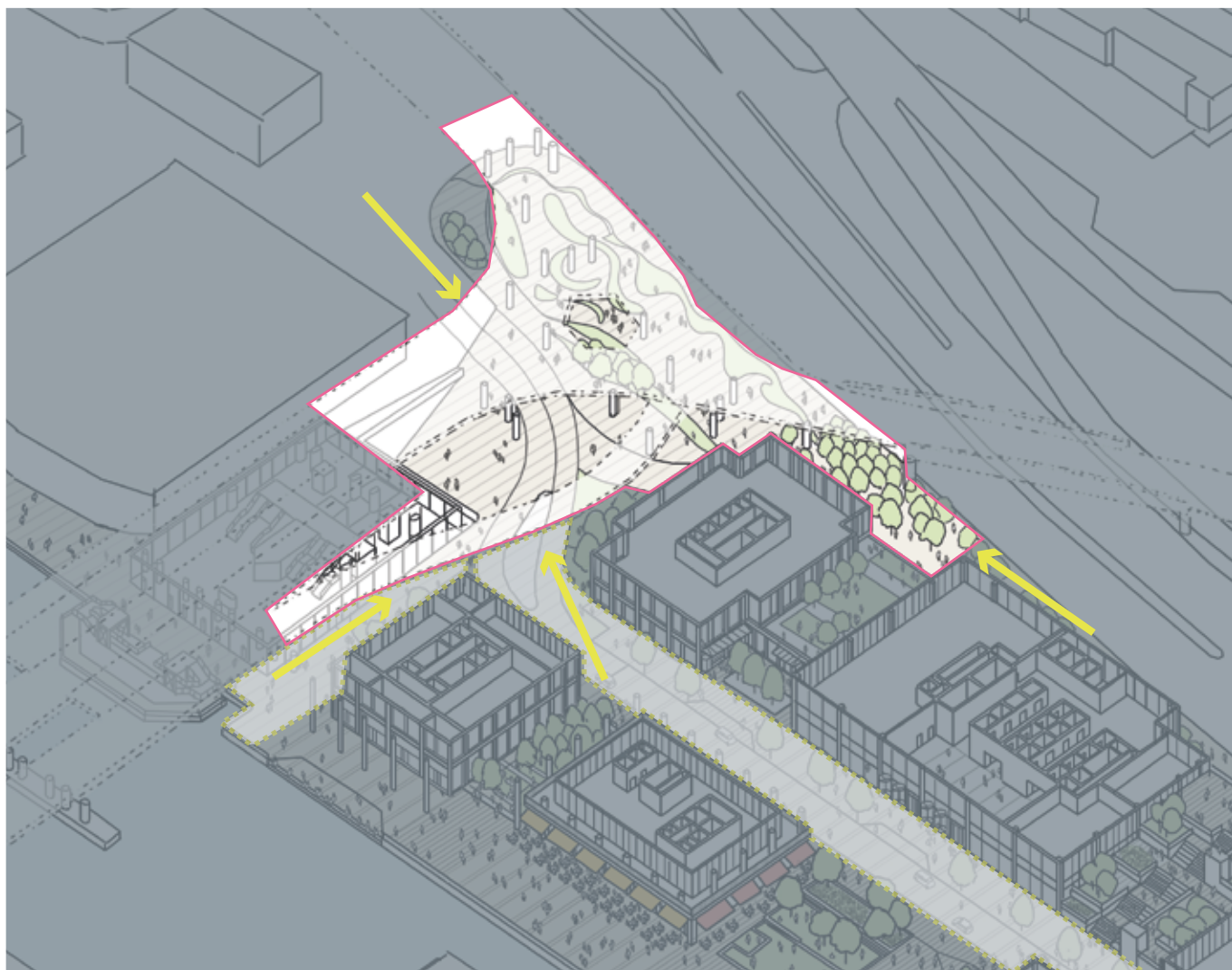


Indicative view of The Delta



## b) Access, Entries & Permeability

The Delta is currently under-used and left-over space dominated by the vehicular movement along Aspen Way. The creation of a new east-west cycle and pedestrian route will help to populate this and provide function to the space.

**The landscape of The Delta should incorporate an east-west two-way cycle route and pedestrian route connecting Aspen Way Gardens with Hertsmere Road.**



Entry to and permeability of The Delta

-  Space
-  Access to
-  Connecting space

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#### c) Views

**A planted or screen wall along the north side of The Delta should be incorporated to enclose the space from Aspen Way and reduce noise pollution and help to improve air quality.**

**The southern edge of The Delta should be designed to maximise views and continuity of movement.**

This would enhance the natural surveillance of the area and encourage its use.

**The Delta should be designed with attention given to its view from adjacent buildings.**

**The Delta should be appropriately lit at night to maximise its use and security, enhance visual amenity and prevent light pollution for any live/stay units overlooking it.**

Consideration should be given in the lighting design such that it may also contribute to creating a strong night-time character to The Delta by accenting the DLR columns and structures overhead as a contrast to soft landscaping.

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#### d) Landscape

The Delta is currently a hard landscaped space dominated by Aspen Way and the overhead DLR structures.

It is also however a dramatic urban space and an opportunity to create a large area of landscape linking Aspen Way Gardens and North Quay Way with Hertsmere Road in a positive way.

**The Delta should create an enhanced landscaped area within the Masterplan that incorporates elements of playable landscape to engage with older children.**

**The Delta should incorporate seating areas providing places to dwell.**

This would enhance the natural surveillance of the area and encourage its use.

**The Delta should contain planted areas with a diverse palette of trees and planting to encourage biodiversity and interactivity with nature.**

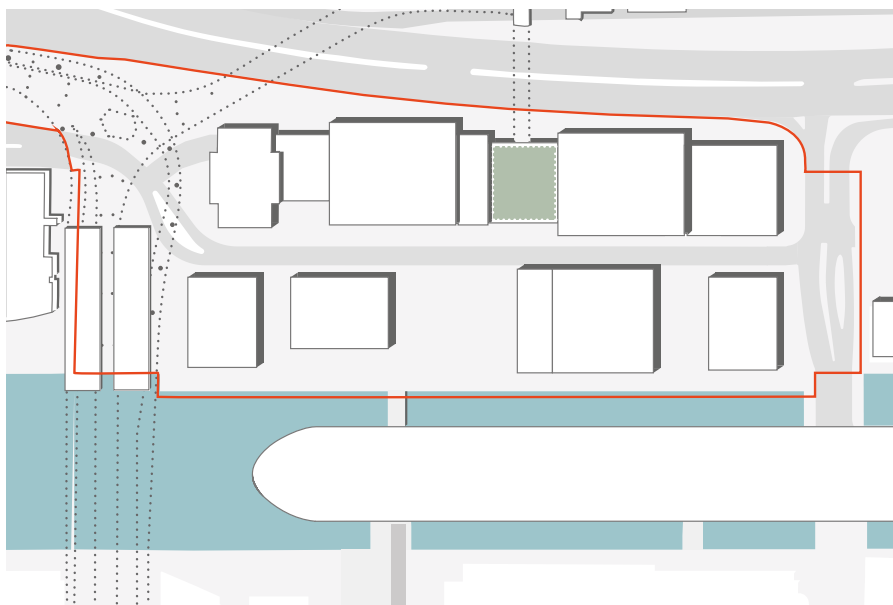
This should create a soft and moving contrast to the strong rigid urban forms of the DLR structures.

## 3.3

### POPLAR PLAZA

#### 3.3.1

#### CHARACTER



Location of Poplar Plaza in the Masterplan, shown here in the context of the Indicative Scheme

Poplar Plaza occupies Development Plot NQ.C1 and is an important formal open space linking the Site with Aspen Way Footbridge, Poplar DLR station and the community of South Poplar beyond.

It is edged by Development Zones NQ.B and NQ.D and integrates with North Quay Way along its south edge and meets Aspen Way and the Aspen Way footbridge to the north.

**Being elevated above Aspen Way, the Plaza should be designed to feel distinct from the busy character of the Aspen Way road below and instead connected with the spaces to the south in North Quay.**

**Poplar Plaza should be designed to connect positively to the Aspen Way Footbridge to provide a sense of continuity for pedestrians walking through.**

**The end of the existing bridge may be modified and/or extended with a transition structure/entrance portal. The existing south staircase and lift towers may be removed to provide a clear new connection into Poplar Plaza and North Quay.** This is to provide a sense of arrival from the bridge to Poplar Plaza and to integrate the bridge structure with the design of the Plaza.



### 3.3.2

## PUBLIC REALM

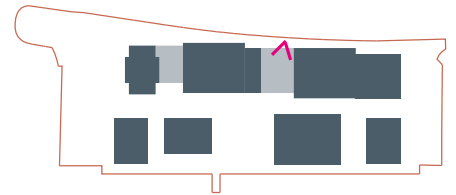
#### a) Levels

Poplar Plaza should be a tiered space managing the changes of level from the existing Aspen Way Footbridge down to North Quay Way for all users.

**The level change from Aspen Way Footbridge down to North Quay Way should be formed from a series of gentle steps and terraces that create a series of spaces to sit/dwell alongside a clear and legible staircase route.**

An inclusive and legible step free access route should also be provided to transition the level change from Aspen Way footbridge to North Quay Way in the form of a passenger lift or lifts suitable to also carry wheelchairs, buggies or bicycles. The lifts may be free standing or incorporated into adjacent building structures but should, in either event, include clear glazing for passive surveillance.

**The design of the public realm will need to be assessed in line with the BREEAM Communities Technical Manual 2012 to meet the required BREEAM rating.**



Indicative view of Poplar Plaza looking South

## b) Access, Entries & Permeability

**Poplar Plaza should be a fully permeable and open space that will form part of the network of routes and a relaxation space within North Quay.**

The Plaza may be animated by a mix of uses at ground and upper ground floors including cafes, restaurants, and shops. On the upper floors a mix of commercial and potentially live/stay uses may overlook the plaza.

**Active retail frontages should be included where possible to ensure that Poplar Plaza is established as a place which is dynamic and lively throughout the day and evening.**

**Retail units fronting this space should have entrances and outdoor seating areas located on the edges of Poplar Plaza where possible.**

There should ideally be no or limited frontage allocated for servicing.

**Where ventilation grilles, etc, are unavoidable they should be limited in extent and integrated into the architecture of the building.**

This would establish Poplar Plaza as an active and lively area and preclude dominant servicing and support spaces on the frontages.

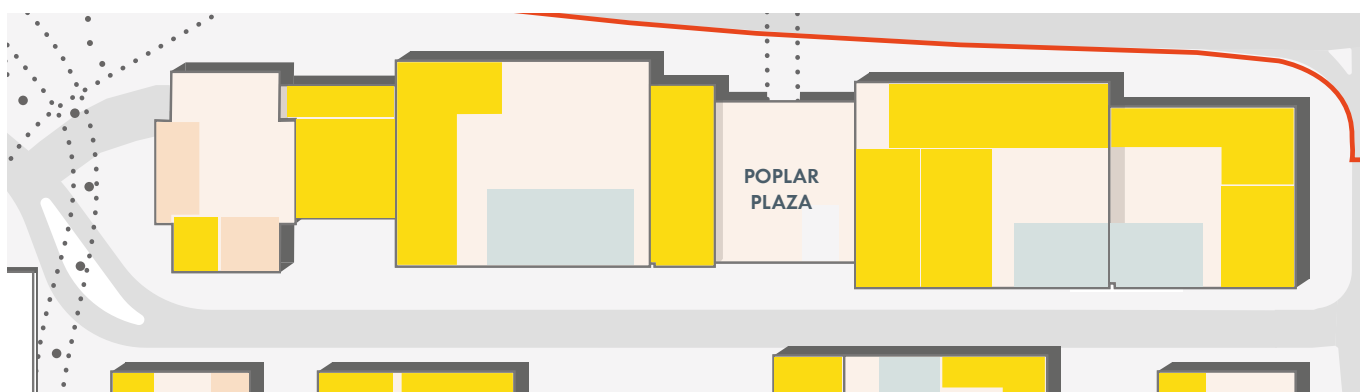
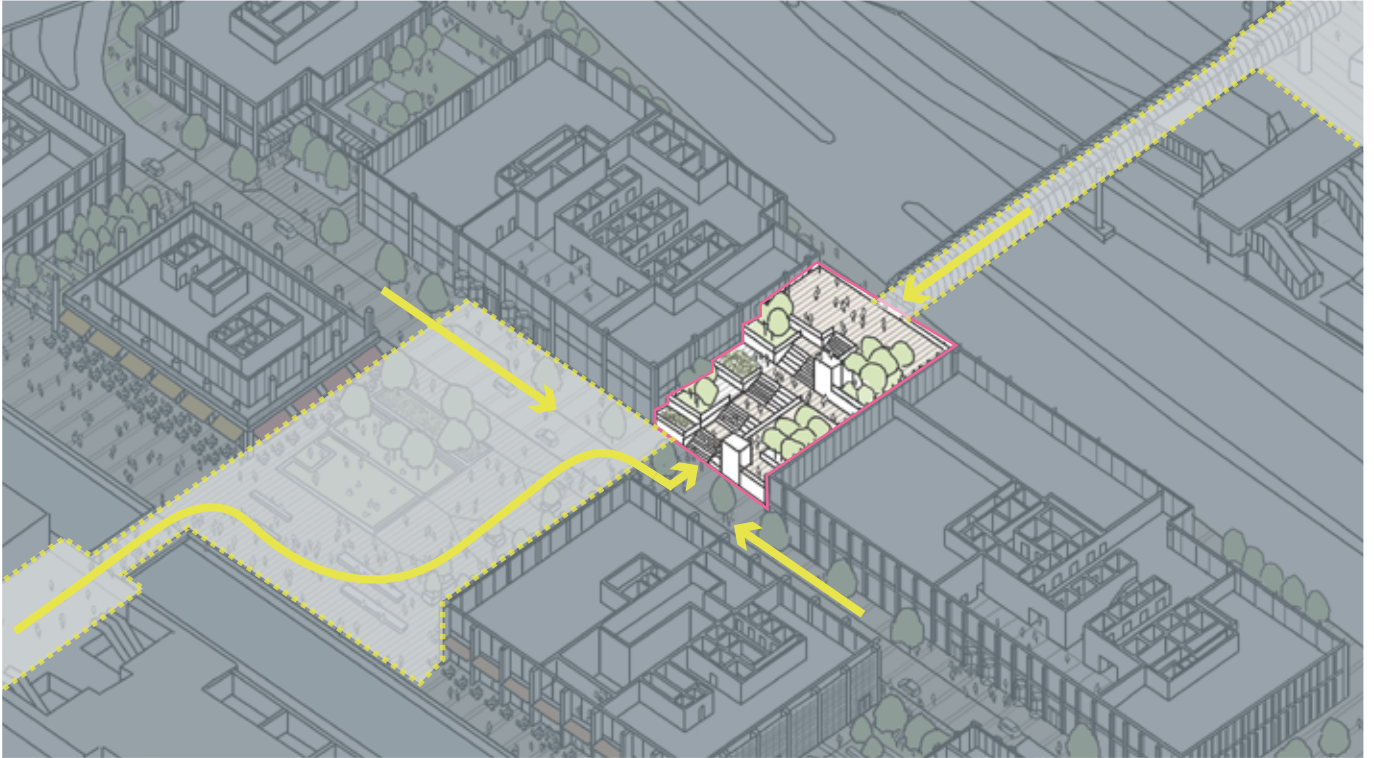


Diagram using the Indicative Scheme to illustrate the requirement for active retail frontages to be included where possible. See Parameter plan NQMP-PP-010

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage





Entry to and permeability of Poplar Plaza

- Space
- ➔ Access to
- Connecting space

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#### c) Views

On entering Poplar Plaza from the Aspen Way Footbridge, views through to Quay Square, Crossrail Place and Canary Wharf beyond will help provide legibility and orientation.

**The design and layout of Poplar Plaza should provide visibility through to Crossrail Place and Quay Square. The transition from the bridge level to grade should be clear, legible and accessible via steps and lifts.**

Poplar Plaza will be overlooked by a mix of office and possibly live/stay uses, and views into the space from these uses provide an important visual amenity.

**The Plaza should be designed with attention given to its view from above and adjacent buildings.**

**The space should be capable of being well lit at night to maximise its use and security, and to enhance the visual amenity provided to the uses overlooking it.**

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#### d) Landscape

Poplar Plaza will be designed as a south facing tiered place and will be akin to an urban piazza with people moving through but also with spaces to dwell for visitors to North Quay as well as residents and local workers.

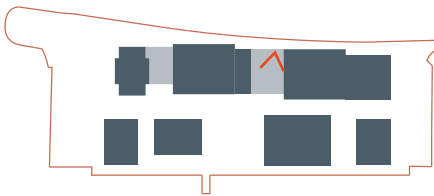
**The space should be designed to contribute positively to the chain of public realm spaces within the Masterplan. Landscaping of this area should be designed to clearly and positively define the route from Aspen Way footbridge through to Quay Square.**

**It should be formed with a combination of hard and soft landscape and may incorporate areas for child play space where appropriate.**

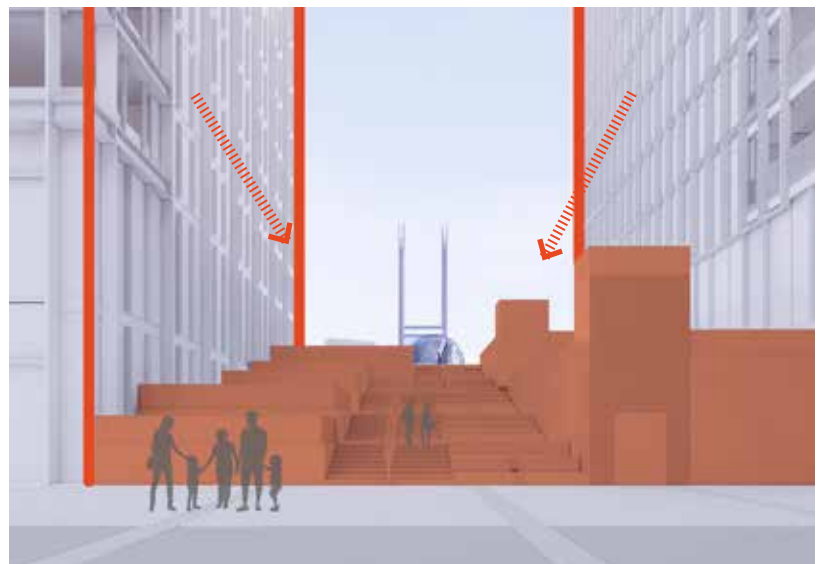
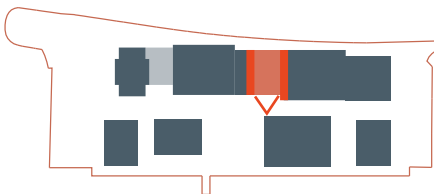
**Large vertical walls/surfaces without active frontages or planting should be avoided.**

**The landscape of Poplar Plaza should include soft planting and trees and seating areas to create the sense of a tiered garden square that positively contributes to improving sustainable drainage and air quality in this area of the Masterplan.**

Opportunities for public art and temporary installations should be encouraged.



Poplar Plaza provides direct views to Quay Square, Crossrail Place and Canary Wharf beyond



Poplar Plaza should be designed with attention given to its view from above

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#### e) Scale and Enclosure

Poplar Plaza will be a strong urban formed rectilinear space with large buildings framing its long sides. It will also however bring light, space and air into to the north side of North Quay and emphasise the connection through to South Poplar.

**The Plaza should not be any narrower than 20m from east building face to west building face (excluding passenger lifts, projecting cornices, balconies, entrance canopies, etc).**

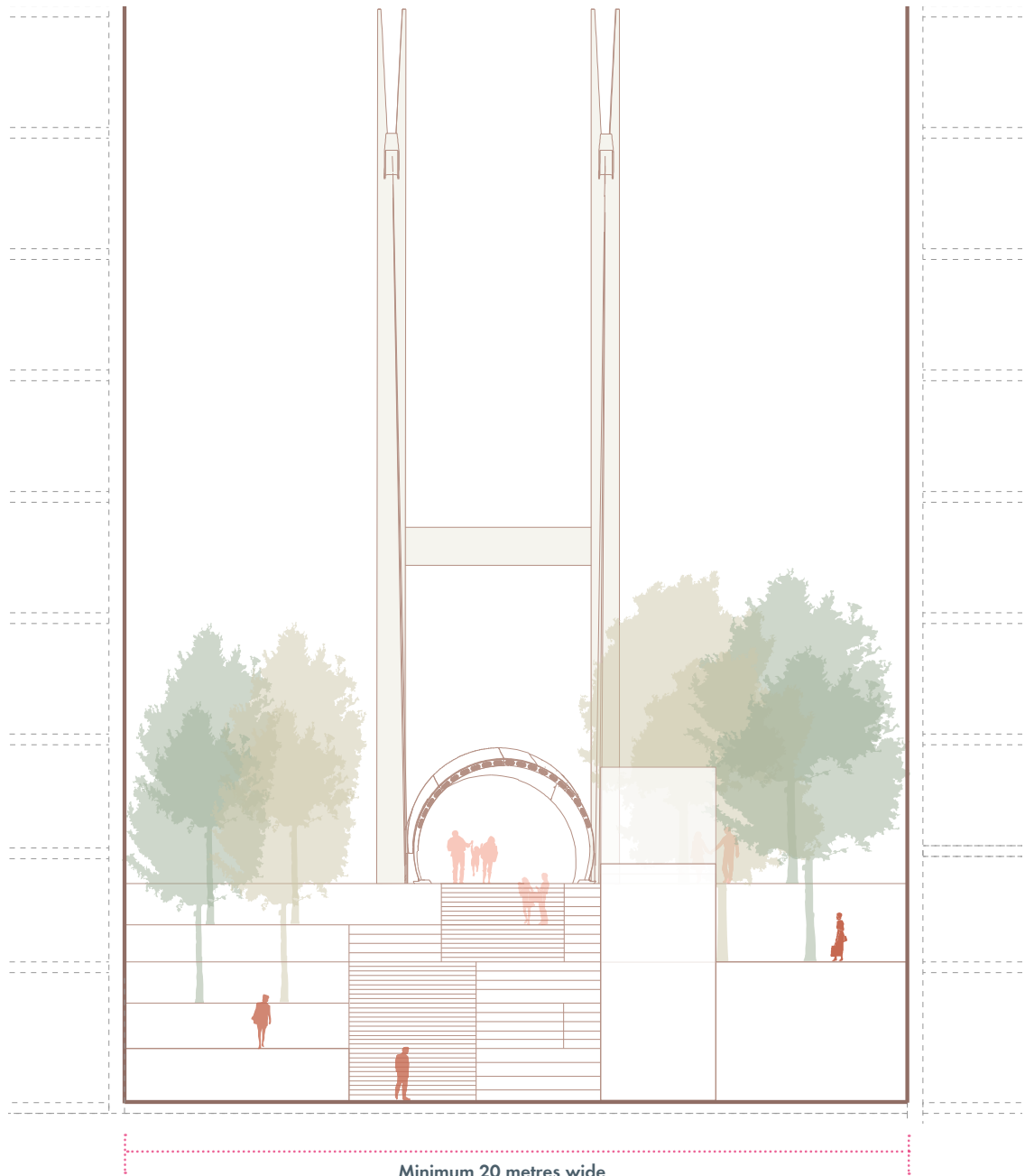
This minimum width will ensure a sense of generosity to this arrival space from South Poplar whilst still offering a sense of enclosure to the Plaza and establish a dialogue between the sides of the space.

The scale of the buildings in Development Zone NQ.B and NQ.D that frame the east and west sides of Poplar Plaza will vary but will form a consistent enclosure to the space. These frontages will act as the edges to the space.

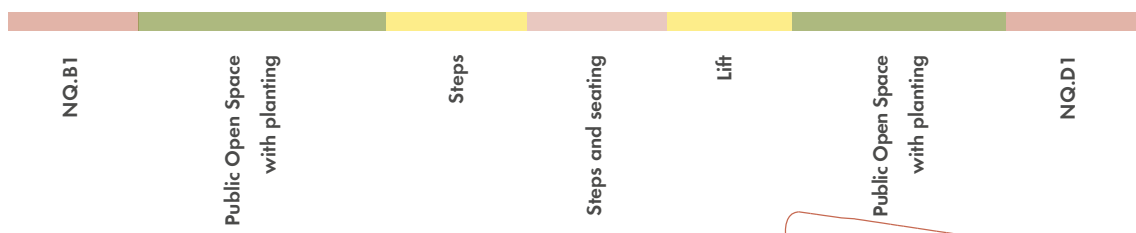
**Building frontages on the east and west edges of Poplar Plaza should be designed to maintain views through the space and to provide active frontages and passive surveillance.**

Views of the contrasting scales of South Poplar, North Quay, Crossrail Place and Canary Wharf would lend a richness and urban layering to this new civic space.

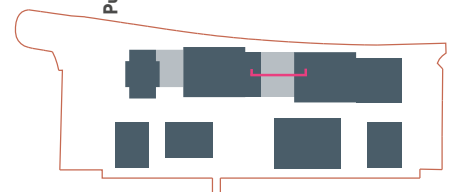
**Buildings surrounding Poplar Plaza and the landscaping within the space should be designed to ensure it has a pleasant microclimate that can be enjoyed while standing still or seated, increasing the success of any outdoor play areas and encouraging use of outdoor seating provisions.**



Minimum 20 metres wide



Section through Polar Plaza

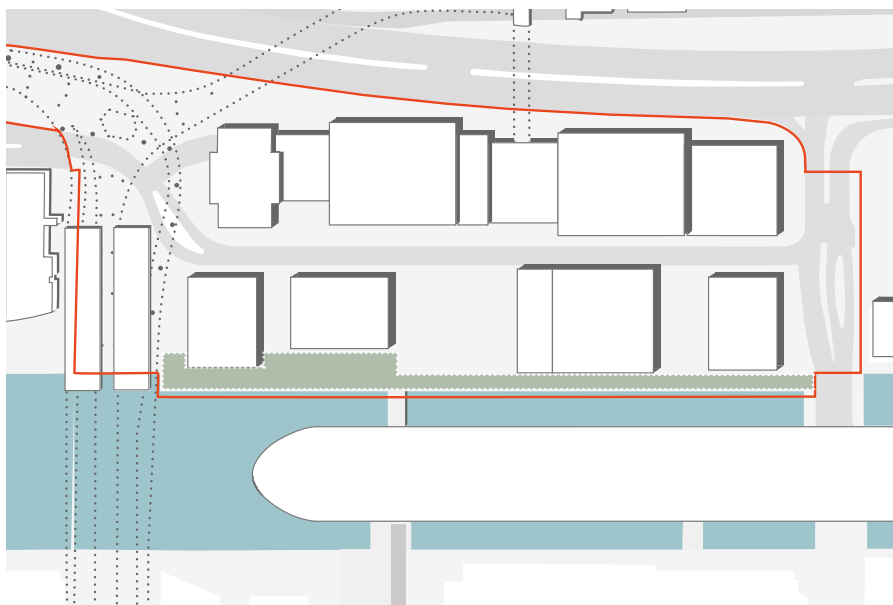


## 3.4

## QUAYSIDE

### 3.4.1

### CHARACTER



Location of Waterside in the Masterplan, shown here in the context of the Indicative Scheme

The Quayside is located along the southern edge of North Quay, located adjacent to the water's edge and across from the existing Crossrail Place building. It will be a predominantly hard landscaped open space with its western end benefitting from plenty of uninterrupted sunlight daily throughout the year. It will connect through to the existing West India Quay under the DLR Station to the west and enable future connection under Upper Bank Street to Billingsgate to the east.

The Quayside is built over part of the existing dock water using a marine deck with the water underneath, consistent with the existing areas adjacent, whilst maintaining the required navigable channel for water vehicles.

The Quayside should be a place of leisure within the Masterplan and will be a destination for those working in Canary Wharf to visit at lunchtime and after work. It will also offer amenity space to tenants/residents of adjacent buildings and to the wider population of the area and should incorporate elements of playable landscape and informal play for younger residents and visitors.

It should be formed by a collection of spaces that enable east-west connectivity but also creating opportunities for outdoor dining and informal dockside dwell spaces that engage with the water edge.

### 3.4.2

## PUBLIC REALM

#### a) Levels

The levels of the Quayside will be guided by the requirements to bridge over the existing listed banana dock wall and to connect to existing ground levels under West India Quay Station and to the Crossrail bridge.

The water edge may sit at a lower level than Quay Square, subtly grading the level difference between Quay Square and the water.

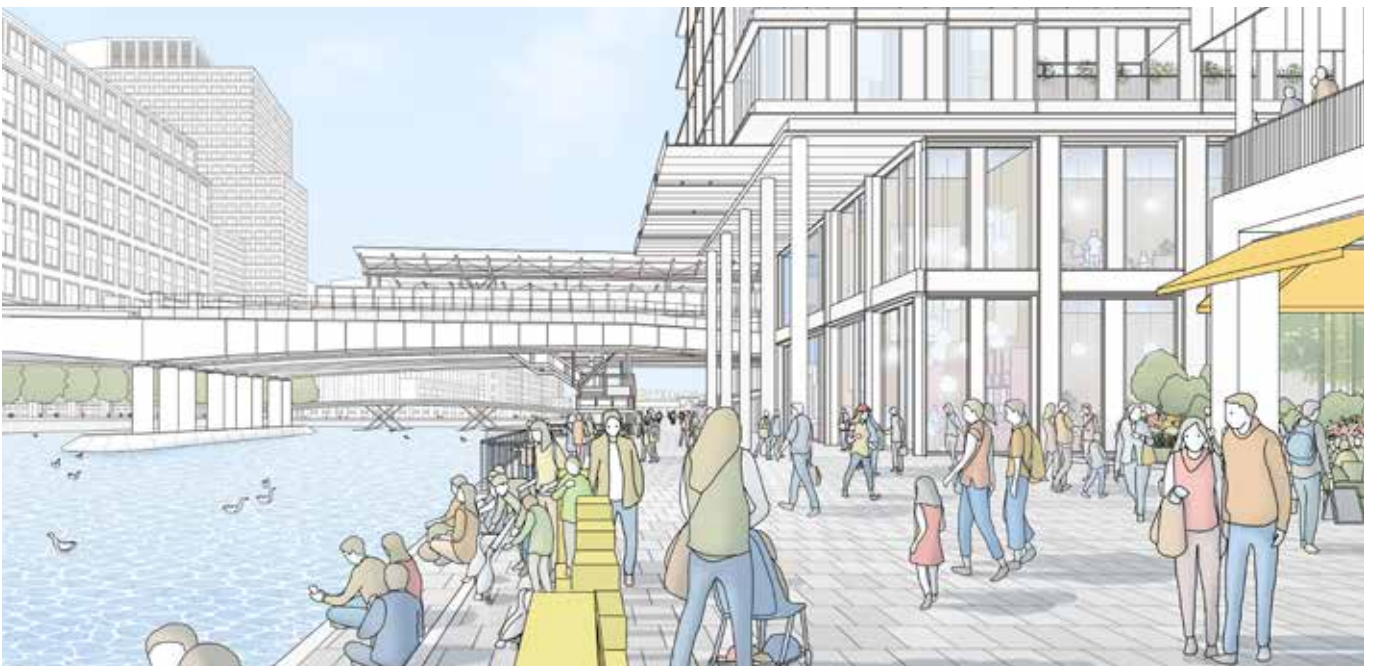
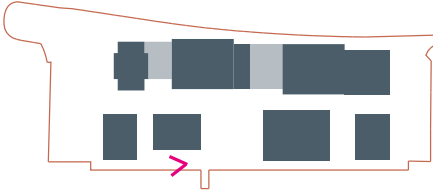
The existing dock water level lies at approx +4.23m AOD, with the 1 in 100 year flood level at approx +5.0m AOD.

**Generally, the main circulation areas of the Quayside should be no lower than approximately +5.5m AOD, and no higher than approximately +6.5m AOD.**

This will allow the Quayside to remain usable if water levels rise following a 1 in 100 year flood event whilst also ensuring a positive relationship with the water.

Stepping down to the water level on the Quay edge to create areas for informal seating steps is encouraged.

**Where provided, balustrades should be designed to be visually unobtrusive, elegant and allow views through to the water edge. They should have lighting and boat mooring connections integrated into their design where possible. General lighting along the boardwalk should not be susceptible to damage from boats.**



Indicative view of Quayside looking West



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## b) Access, Entries & Permeability

The Quayside is a pedestrian space and will be connected to the remainder of North Quay through Quay Square, Garden Square and Dock Square as well as at the edges of the Site. The Quayside is also an important east-west route connecting West India Quay under the DLR through to Upper Bank Street and Billingsgate.

A busy north-south route will link Canary Wharf and Crossrail Place with Poplar Plaza and through on to Poplar DLR and South Poplar beyond.

**The Quayside should be designed to facilitate these east-west and north-south pedestrian routes.**

**The Quayside should have a minimum width of 9 metres from building face to water edge. This may include external seating areas and street furniture.**

This will allow adequate space for usage by people travelling along the space with pushchairs and wheelchairs whilst still also allowing people to dwell and engage with the waterside.

With its proximity to the Elizabeth Line entrance at Crossrail Place, on certain occasions it may be required to act as a bulge space to accommodate travellers awaiting access into the station in the event of delays or short term station closure.

**The Quayside should be proportioned and have street furniture positioned such that it can act as an intermittent holding space for travellers delayed accessing the Elizabeth Line station from North Quay.**

The Quayside should be freely accessed from all entry points. It should be designed to be as permeable as possible and encourage numerous access points and routes through.

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## c) Views

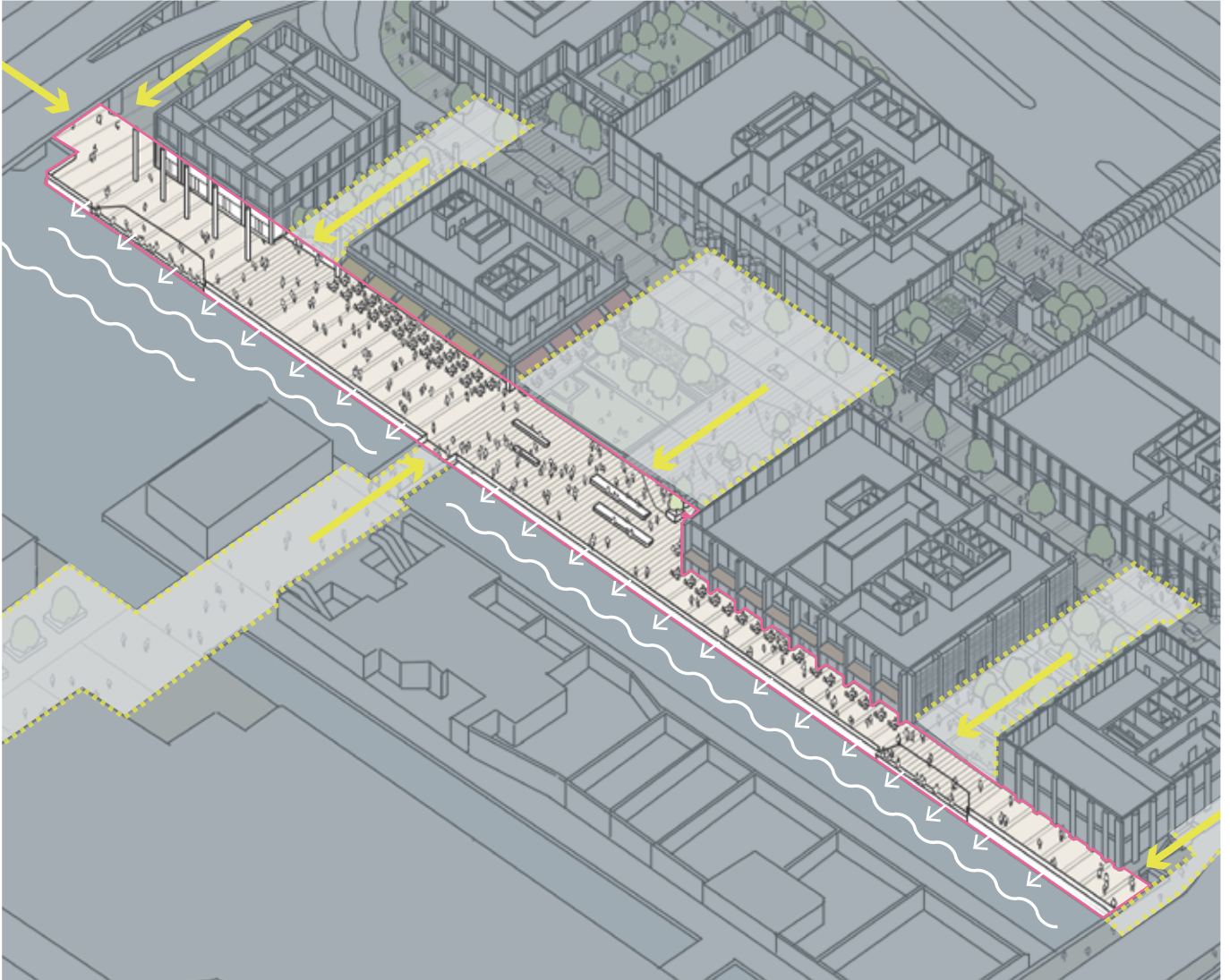
The view from the arrival through Crossrail Place across Quay Square to Poplar Plaza will be important in enticing visitors on arrival to North Quay from Canary Wharf and permeability across to South Poplar. Equally the legibility of the view from Poplar Plaza across to Crossrail Place is important.

**The landscaping of the Quayside and any artwork/ street furniture installed in the space should maintain this legibility to/from Poplar Plaza.**

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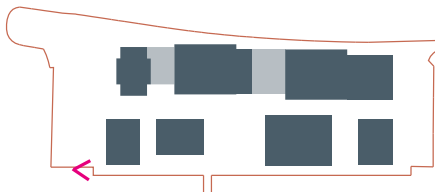
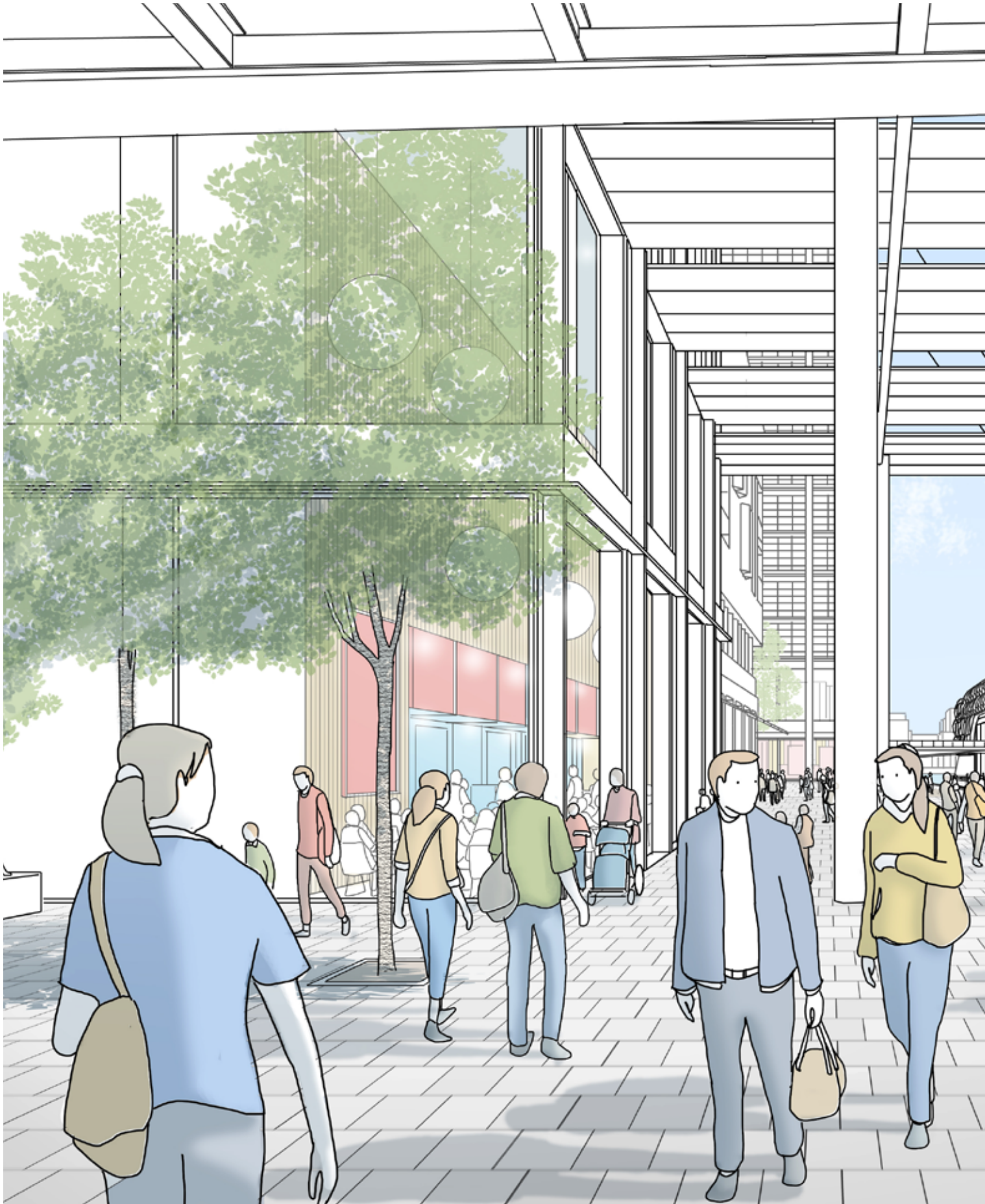
## d) Landscape

**The landscape design and materiality of the Quayside should reflect the site history as part of the working import/export docks of London and the rugged nature of docks/quaysides.**

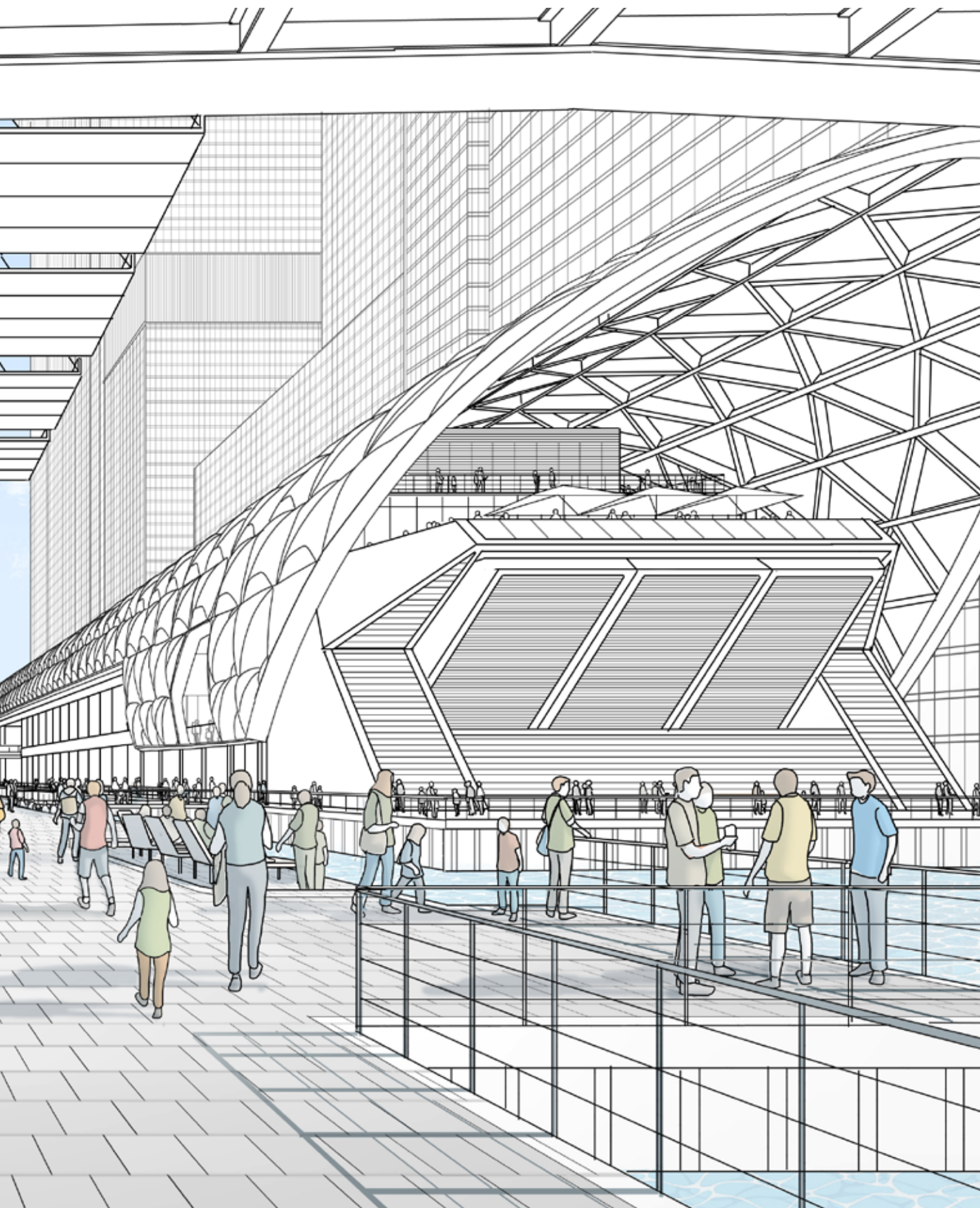


Entry to and permeability of Quayside

- Space
- ➔ Access to
- Connecting space
- ➔
}
 Engage with waterfront







This is to ensure it fits with the adjacent character of West India Quay and other docksides within Canary Wharf.

The landscape may naturally create incidental play opportunities through street furniture or functional art pieces.

**The design of the Quayside should be flexible in providing different options for seating, walking and connecting to adjacent spaces.**

#### e) Scale and Enclosure

The northern edge of the Quayside is formed by a series of buildings in the South of the Masterplan. Through the control of the Parameter Plans, the space will have a varied width.

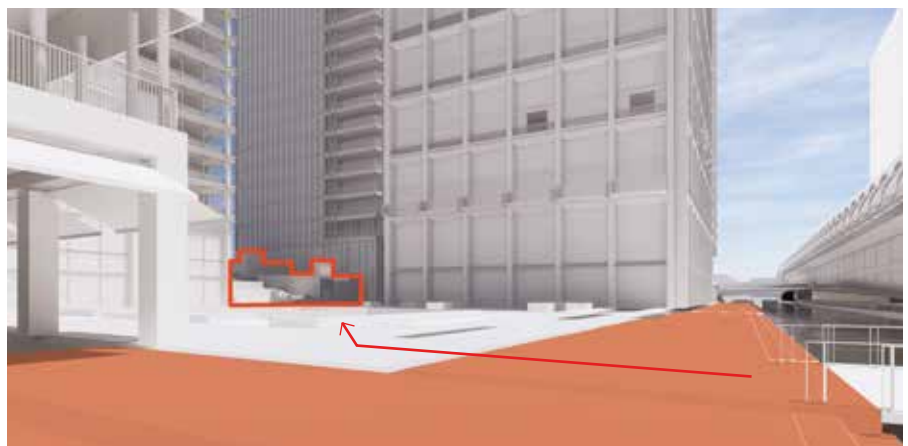
The southern edge of The Quayside as an urban space is not just defined by the edge of the water but instead by the built form of Crossrail Place and the Canary Wharf buildings to the South.

The Quayside and water space of the dock combine to create a sense of space between Crossrail Place and the buildings of the Masterplan.

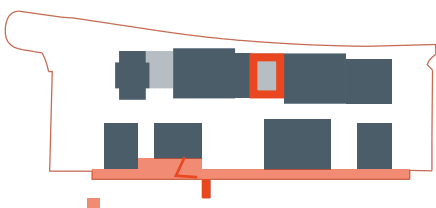
It is expected that the existing retail units within Crossrail Place will in the future open out to face towards North Quay and create an active edge along the north side of Crossrail Place.

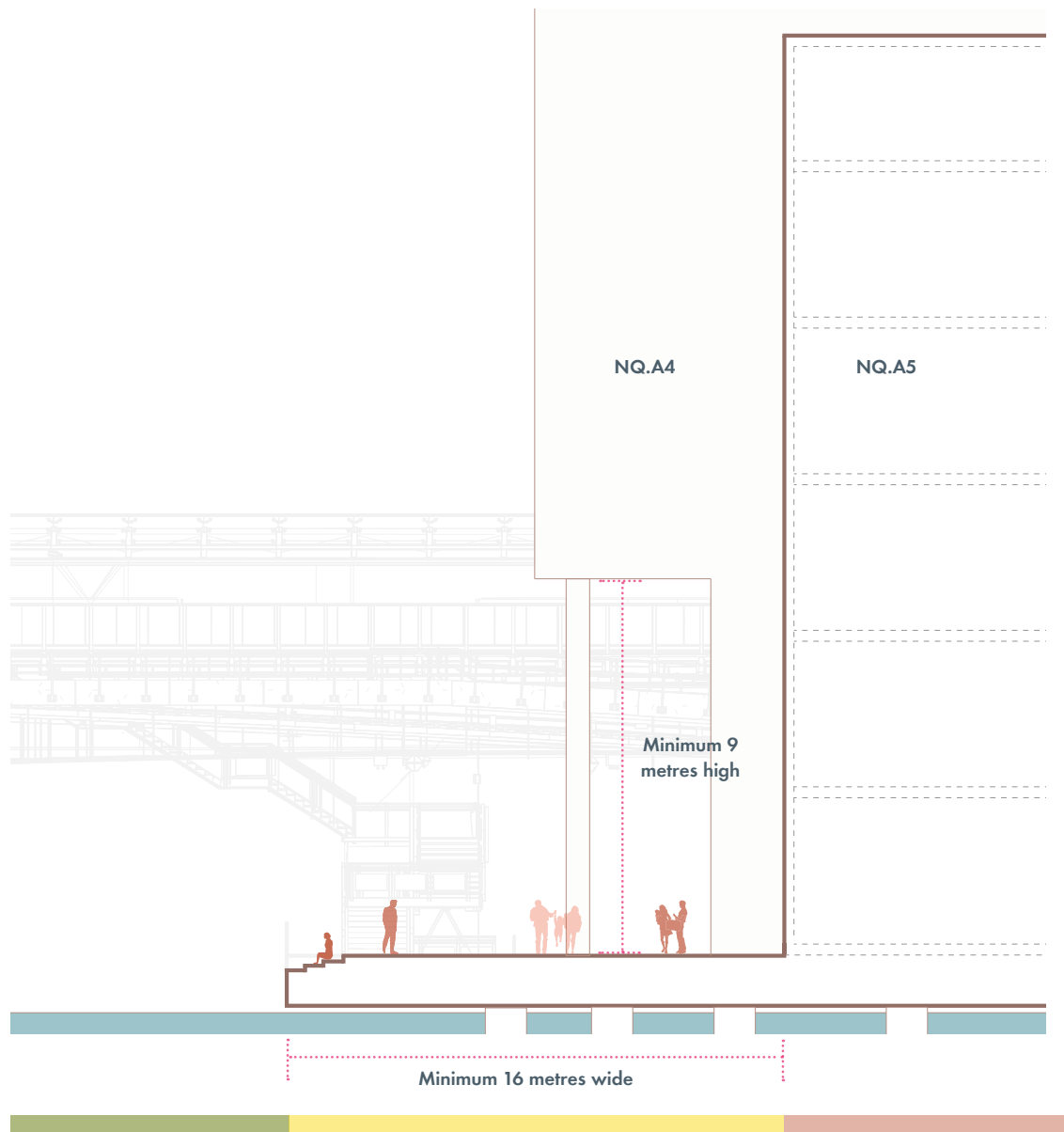
**The southern edge of the Quayside should be straight and offer workers, residents and visitors within North Quay a direct experience of the dock and Crossrail Place/Canary Wharf beyond.**

**A minimum width of 16 metres should be provided between the facade of NQ.A5 and interior face of dock edge guarding, and the setback face of NQ.A4 should align with the existing permanent DLR structures to maximise the visibility and permeability along this route. Where Development Plots NQ.A4 and NQ.A5 may build over the Quayside, a minimum clear height of 9 metres should be provided from the ground plane to the soffit of the space under the buildings in these areas.**



Quayside provides a clear visual connection from Crossrail Place to Poplar Plaza

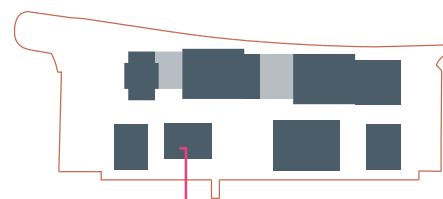




North Quay Dock

Quayside

NQ.A5

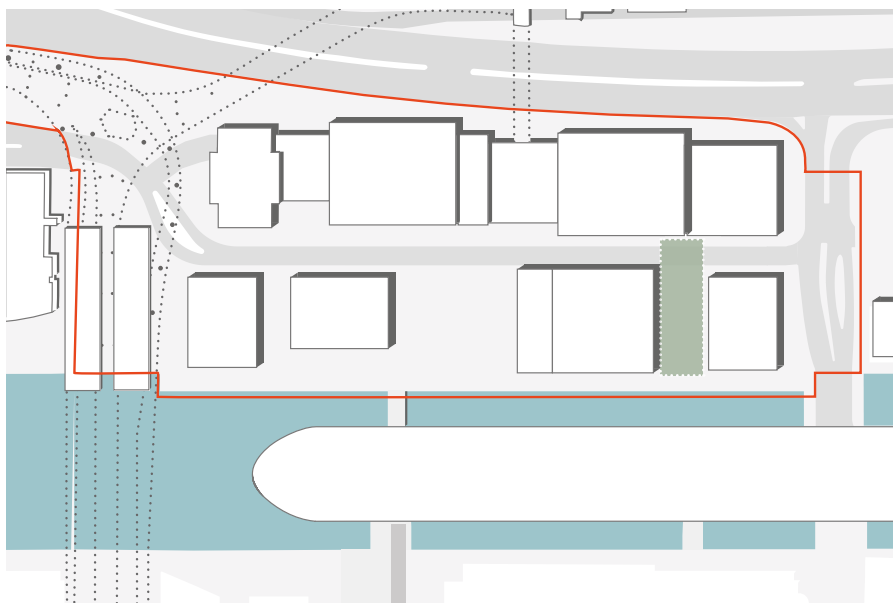


## 3.5

## DOCK SQUARE

### 3.5.1

### CHARACTER



Location of Dock Square in the Masterplan, shown here in the context of the Indicative Scheme

Dock Square will be a garden space located in the south-east of North Quay, between plots NQ.D3 and NQ.D4 and adjacent to The Quayside. In certain development scenarios where Plots NQ.D3 and NQ.D4 might combine as a single non-residential building plot, this open space would not be provided.

Where provided, it will be a soft and more intimate landscaped open space as a place of relaxation in the Masterplan. It will be bordered on both sides by active building frontages and on its south side by The Quayside forming the dock edge. On its northern edge it connects through to North Quay Way.

Dock Square should be a place of quiet leisure within the Masterplan. It will offer amenity space to tenants/residents of adjacent buildings and to the wider population of North Quay. It should be formed by a collection of planted spaces which vary in character, together offering a unified garden space while individually providing a series of landscape rooms.



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### 3.5.2

## PUBLIC REALM

#### a) Levels

Dock Square will sit at approx +6.2m AOD but its ground plane may be sculpted to create landscape structure to the square. This should contrast to the strong architectural forms of the surrounding buildings and lend an organic feel to the space.

**Level differences should be managed in such a way so as to not have a detrimental effect to the functionality of the garden landscape.**

This is to ensure the utility of the space would not be compromised and adequate free space would be allocated for public use.

## b) Access, Entries & Permeability

North Quay will offer a series of interesting public spaces which will connect to each other to provide attractive routes for pedestrians through and around the Site. Dock Square will sit as one of the secondary spaces in hierarchy, connecting The Quayside with North Quay Way. **Dock Square should be a fully permeable and open space that will form part of the network of routes and a relaxation space within North Quay.**

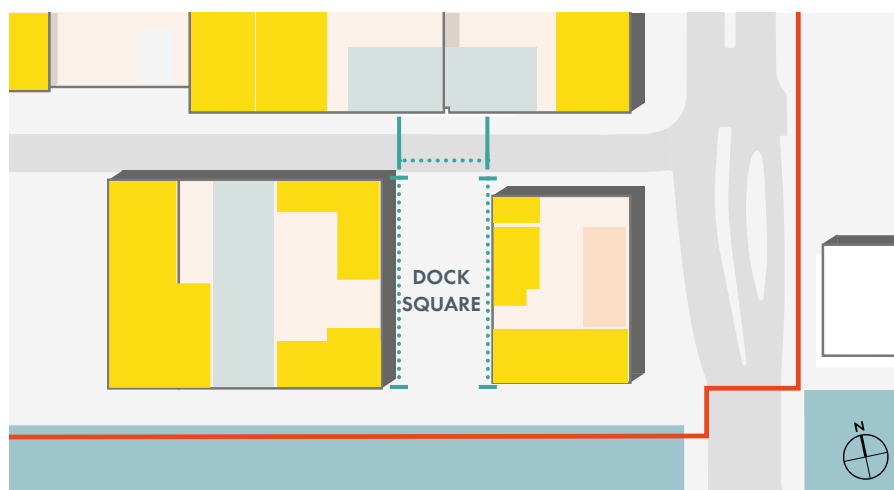
**Ground floor retail units fronting this space should have entrances and outdoor seating areas located on the edges of Dock Square where possible.** See Parameter Plan NQMP-PP-010 for active frontage requirements.

There should ideally be no or limited frontage allocated for servicing.

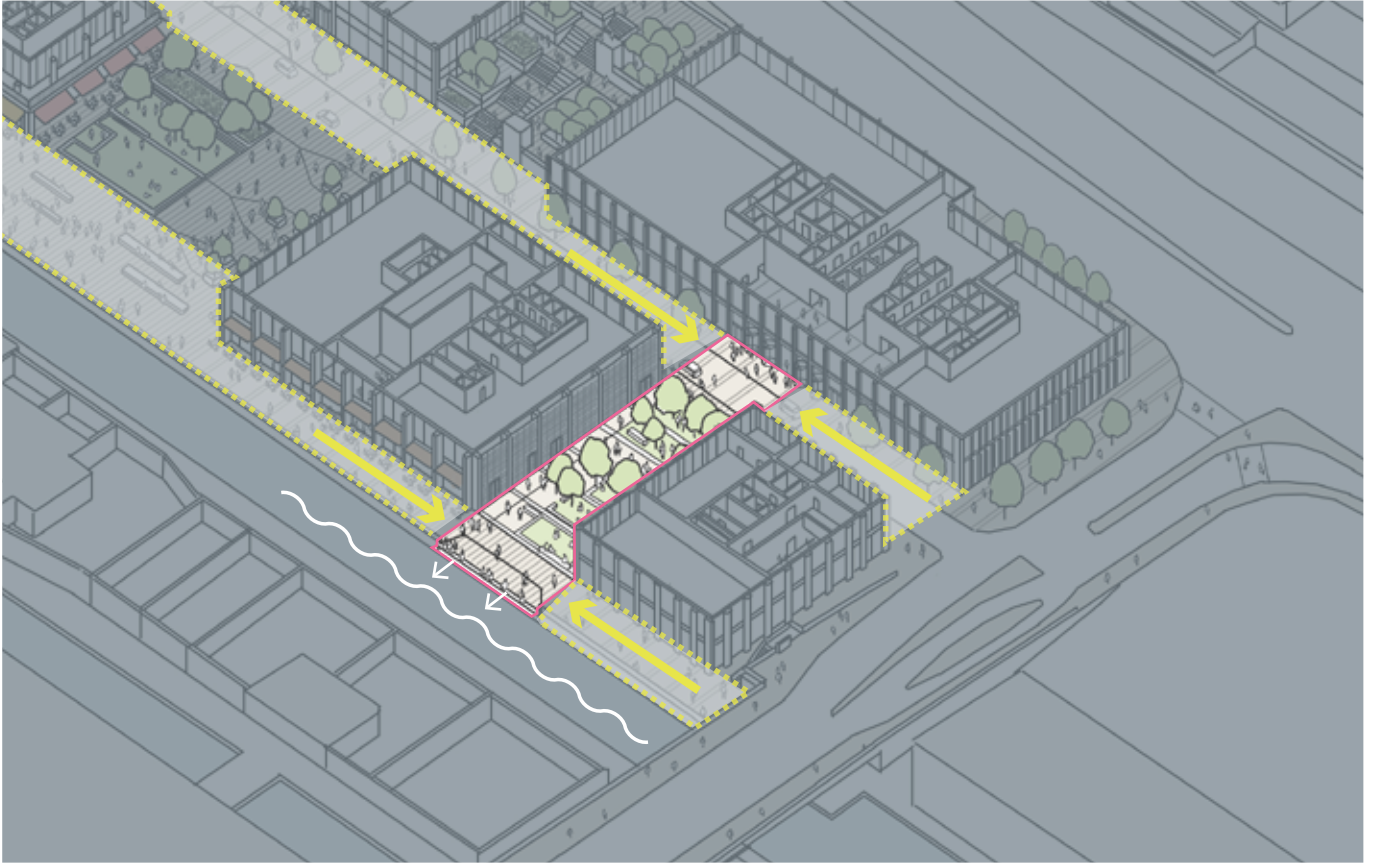
**Where ventilation grilles, etc, are unavoidable they should be limited in extent and integrated into the architecture of the building where possible.**

This would establish Dock Square as an active and lively area and preclude dominant servicing and support spaces on the garden frontages.

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage
- Built active frontage. For requirements see Parameter Plan NQMP-PP-010



Example active frontage diagram using the Indicative Scheme. See Parameter plan NQMP-PP-010 for requirements.



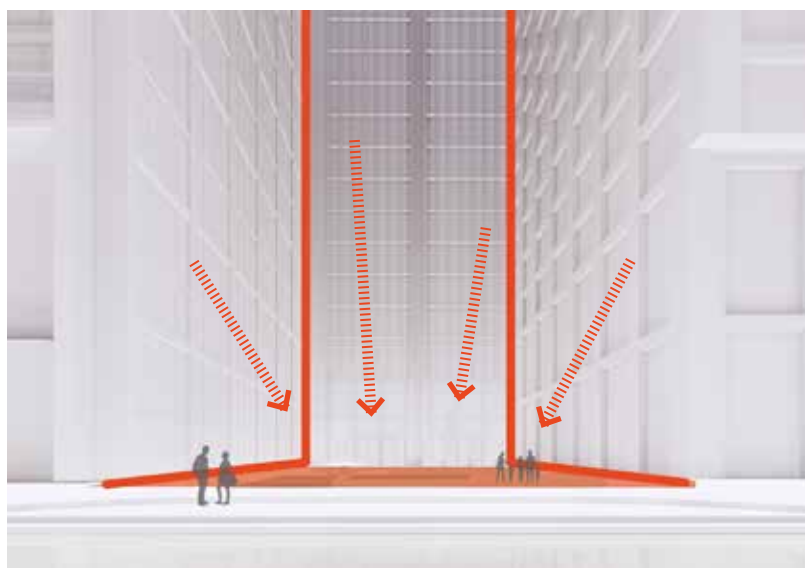
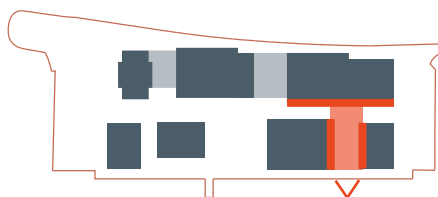
Entry to and permeability of Dock Square

- Space
- ➔ Access to
- Connecting space
- ➔ } Engage with waterfront

c) Views

Dock Square will be overlooked by a mix of office and live/stay buildings, and views into and through the square from these uses provide an important visual amenity.

**The square should be designed with attention given to its view from above. The space should be capable of being well lit at night to maximise its use and security, and to enhance the visual amenity provided to the residential units overlooking it.**



Dock Square should be designed with attention given to its view from above

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#### d) Landscape

Parameter Plan NQMP-PP-008 defines the upper floor usage of the Development Zones which will surround Dock Square. The square therefore sits in an area where there is flexibility within the Masterplan in terms of the activities that will surround it.

**The landscaping of Dock Square should be informed by the predominant usage and activities in the upper floors of the surrounding Development Zones and Plots as they unfold.**

For example, should the surrounding buildings be allocated for an urban style mix of commercial and CAZ type uses, the square may be a harder landscaped space with some strong green elements. However if it were to be surrounded by predominantly live/stay activities, the square might provide more soft landscaped area with more of a park garden character.

**Regardless of upper floor uses, the square should be lined with an active retail edge and landscaping should therefore account for external terraces, awnings etc.**

Dock Square will be a quieter space within North Quay but should still support a variety of functions including intimate performance events, art or light installations or exhibitions, etc.

The relationship between Dock Square and the uses adjacent to and overlooking it is important in establishing a unique identity and sense of place.

**Landscaping should not overly screen the square from the lower level units around it.**

**Any surface car parking/loading bays/cycle parking should not create a visual and physical barrier between the square and North Quay Way.**

This is to ensure natural people flow through the space from the buildings on the north side of North Quay Way.

**Planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.**

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#### e) Scale and Enclosure

Dock Square will be a strong urban formed space with large buildings framing its sides. It will also however bring light, space and air into to the east side of North Quay.

The scale of the buildings in Development Zone NQ.D that frame the east and west sides of Dock Square will vary but will form a consistent enclosure to the space. These frontages will act as the edges to the square and behind them views to the building(s) on the north side of North Quay Way will be offered.

**Building frontages on the east and west edges of Dock Square should be shaped to maintain views of the buildings looking north and through to Crossrail Place looking south.**

Views of the contrasting scales of North Quay, Crossrail Place and Canary Wharf would lend a richness and urban layering to the square.

**Buildings surrounding Dock Square should be designed to ensure it has a pleasant microclimate that can be enjoyed while standing still or seated, increasing the success of any outdoor play areas and encouraging use of outdoor seating provisions.**

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#### f) Building Lines & Frontages

It is important that the proportions of the square are sized to create a sense of intimacy but also resonance between retailers in order to promote a civic garden space in keeping with the character of the North Quay neighbourhood.

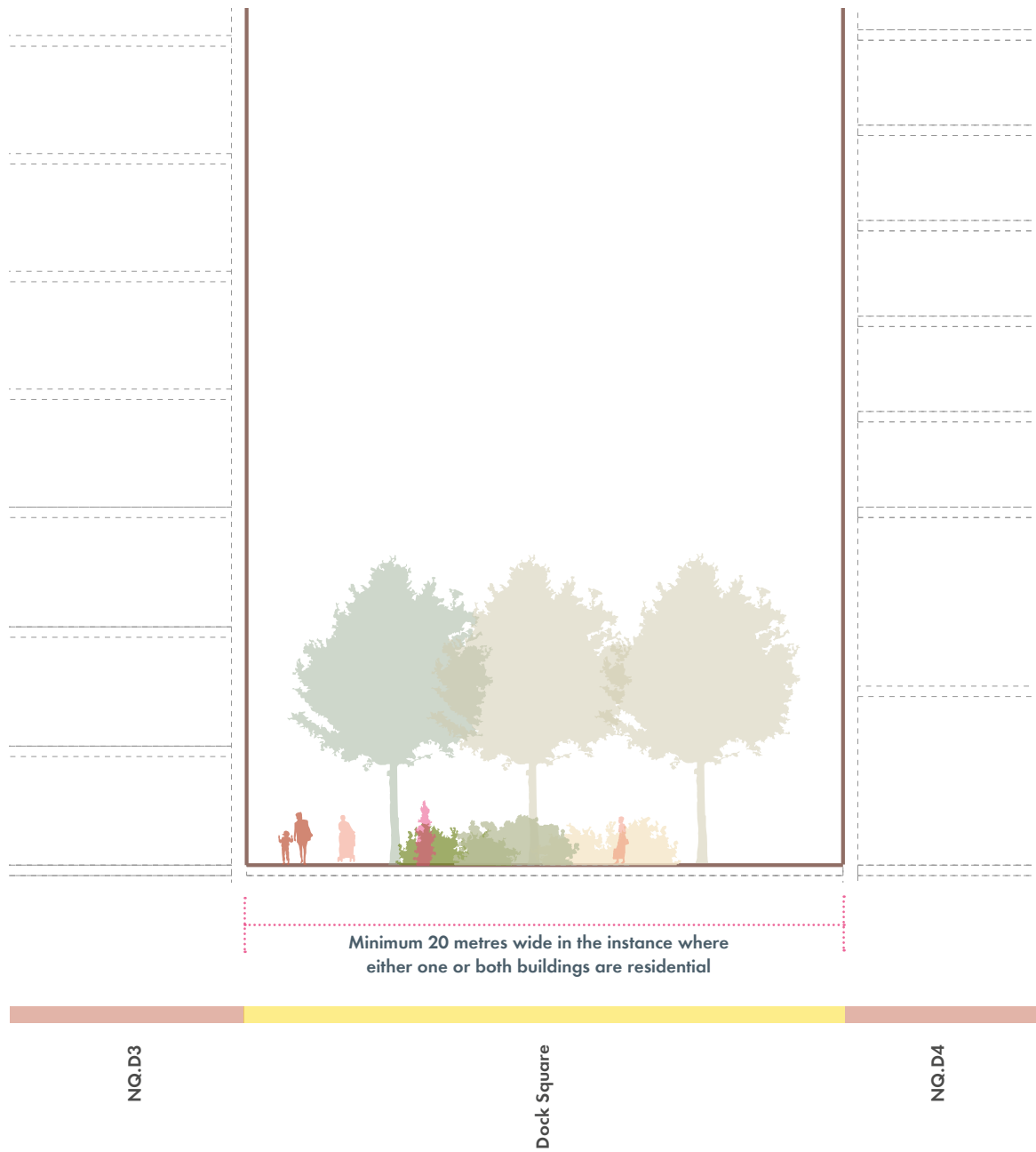
**Dock Square should not be any narrower than 20m from east building face to west building face if one of the buildings are residential buildings (excluding projecting cornices, balconies, entrance canopies, etc).**

**Building lines along the east and west edges of Dock Square should be parallel to each other.**

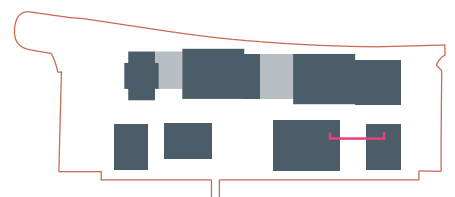
This is to define Dock Square as a formal rectangular space, establishing connections to adjacent routes/spaces to the north and south.

**Facades should give priority to street enclosure over the object-like quality of individual buildings.**

**Built frontages and planting should be designed to maximise natural light in Dock Square.**



Section through Dock Square



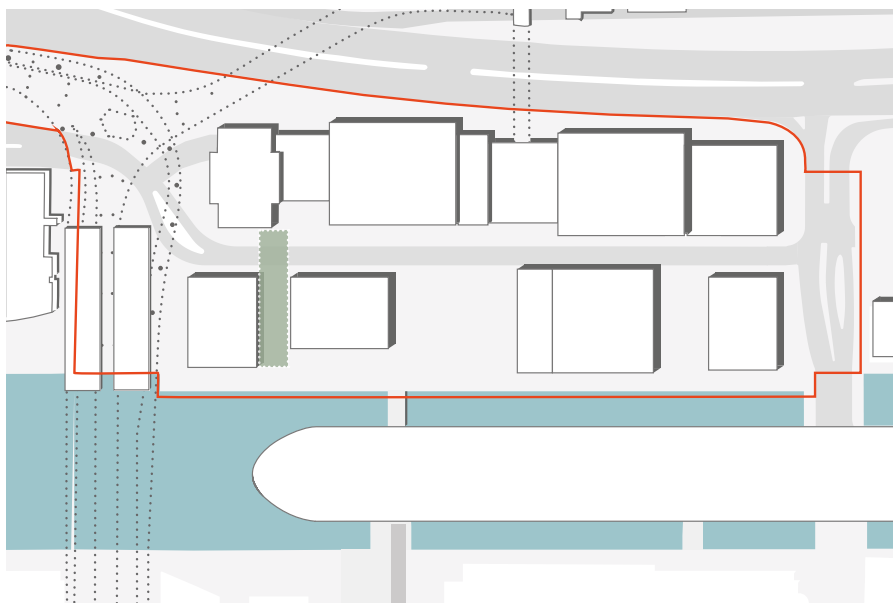


## 3.6

## GARDEN SQUARE

### 3.6.1

### CHARACTER



Location of Garden Square in the Masterplan, shown here in the context of the Indicative Scheme

Garden Square will be a small garden space located in the south-west of North Quay, between Development Plots NQ.A4 and NQ.A5 and adjacent to The Quayside. In certain development scenarios where Plots NQ.A4 and NQ.A5 might combine as a single non residential building plot, this open space would not be provided.

Where provided, in a residential setting, it will be a soft and more intimate landscaped open space as a place of relaxation in the Masterplan. It will be bordered on both sides by active building frontages and on its south side by The Quayside forming the dock edge. On its northern edge it connects through to North Quay Way.

Garden Square should be a place of quiet leisure within the Masterplan. It will offer amenity space to tenants/residents of adjacent buildings and to the wider population of North Quay.

It should be formed as a unified garden space providing a landscape with its own character.

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## 3.6.2

## PUBLIC REALM

### a) Levels

Garden Square will sit at approx +6.2m AOD but its ground plane may be sculpted to create a defined landscape and structure to the garden. This should contrast to the strong architectural forms of the adjacent buildings and lend an organic feel to the space.

**Level differences should be managed in such a way so as to not have a detrimental effect to the functionality and permeability of the garden landscape.**

This is to ensure the utility of the space would not be compromised and adequate free space would be allocated for public use.

## b) Access, Entries & Permeability

North Quay will offer a series of interesting public spaces which will connect to each other to provide attractive routes for pedestrians through and around the Site. Garden Square will sit as one of the secondary spaces in hierarchy, connecting the western end of The Quayside with North Quay Way.

**Garden Square should be a fully permeable and open space that will form part of the network of routes and a relaxation space within North Quay.**

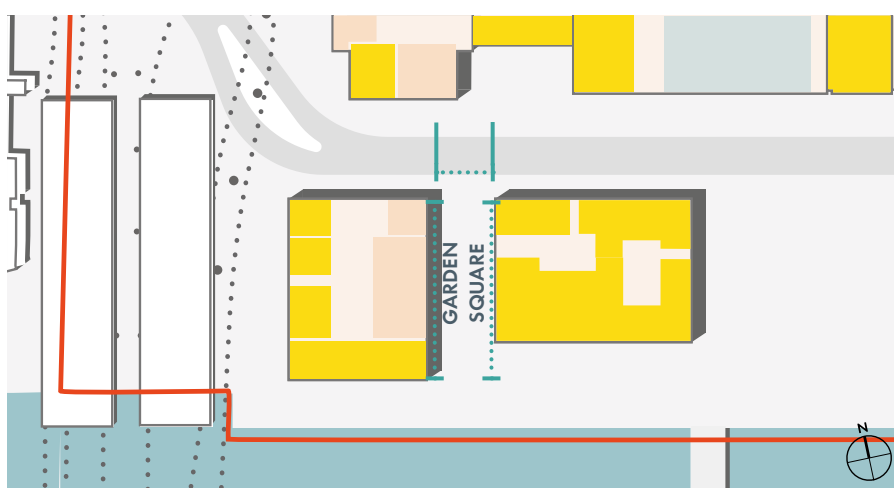
**Ground floor retail units fronting this space should have entrances and outdoor seating areas located on the edges of Garden Square where possible.** See Parameter Plan NQMP-PP-010 for active frontage requirements.

There should ideally be limited frontage allocated for servicing.

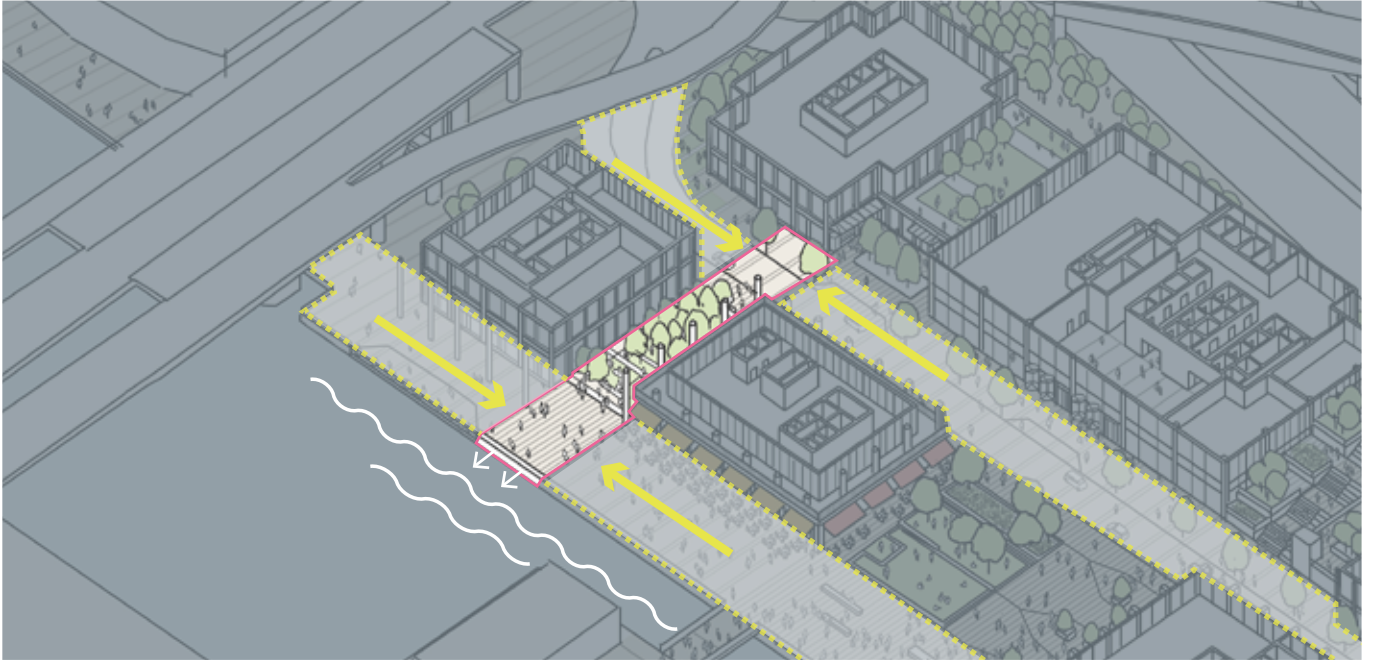
**Where ventilation grilles, etc, are unavoidable they should be limited in extent and integrated into the architecture of the building where possible.**

This would establish Garden Square as an active and lively area and preclude dominant servicing and support spaces on the garden frontages.

- Retail active frontage
- Office lobby active frontage
- Live/stay active frontage
- Built active frontage. For requirements see Parameter Plan NQMP-PP-010



Example active frontage diagram using the Indicative Scheme. See Parameter plan NQMP-PP-010 for requirements.



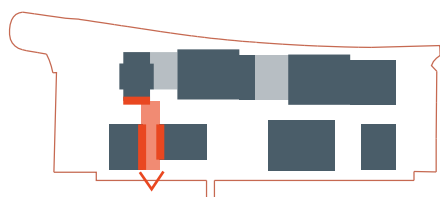
Entry to and permeability of Garden Square

- Space
- ➔ Access to
- Connecting space
- ➔ } Engage with waterfront

### c) Views

Garden Square could be overlooked by a mix of office or live/stay buildings and retail uses, and views down into the garden from these uses provide an important visual amenity.

**The square should be designed with attention given to its view from above. The space should be capable of being well lit at night to maximise its use and security, and to enhance the visual amenity provided to the uses overlooking it.**



Garden Square should be designed with attention given to its view from above

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#### d) Landscape

Parameter Plan NQMP-PP-008 defines the upper floor usage of the Development Zones which will surround Garden Square. The square sits in an area where there is flexibility within the Masterplan in terms of the activities that will surround it.

**The landscaping of the square should be informed by the predominant usage and activities in the upper floors of the surrounding Development Zones and Plots as they unfold. This may include the provision and integration of child play space within the landscape where applicable.**

For example, should the surrounding buildings be edged for an urban style mix of commercial and CAZ type uses, the square may be a harder landscaped space with some strong green elements. However if it were to be adjacent to predominantly live/stay activities, the square might provide more soft landscaped area with more of a park garden character.

**Regardless of upper floor uses, the square should be lined with an active retail edge and landscaping should therefore account for external terraces, awnings etc.**

Garden Square will be a quieter space within North Quay but could still support a variety of functions such as art or light installations or exhibitions, etc.

The relationship between Garden Square and the uses adjacent to and overlooking it is important in establishing an appropriate identity and sense of place.

**Landscaping should not overly screen the square from the lower level units around it.**

This is to ensure natural people flow through the space from the buildings on the north side of North Quay Way.

**All planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.**

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#### e) Scale and Enclosure

Garden Square will be an urban linear square with very different scales of buildings framing its long sides. It will bring sunlight, space and air into to the west side of North Quay.

The scale of the buildings in Development Zone NQ.A that frame the east and west sides of Dock Square will vary significantly but will form a consistent rectilinear enclosure to the space. These frontages will act as the edges to the Square and behind them views to the building(s) on the north side of North Quay Way will be offered. Due to the lower height of Development Zone NQ.A5 there will also be a vertical zone of sky beyond.

**Building frontages on the east and west edges of Garden Square should be shaped to maintain views of the buildings looking north and through to the Quayside, dock and Canary Wharf beyond looking south.**

Views of the contrasting scales of North Quay, Crossrail Place and Canary Wharf would lend a civic richness and urban layering to the square.

**Buildings surrounding Garden Square should be designed to ensure it has a pleasant microclimate that can be enjoyed while standing still or seated, increasing the success of any outdoor play areas and encouraging use of outdoor seating provisions.**

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#### f) Building Lines & Frontages

It is important that the proportions of this small square are sized to maintain a sense of intimacy.

**Consistent with guideline 2.2.2.e, and where provided, the square should not be any narrower than 12m from the east building face if one of the buildings are residential buildings (excluding projecting cornices, balconies, entrance canopies, etc).**

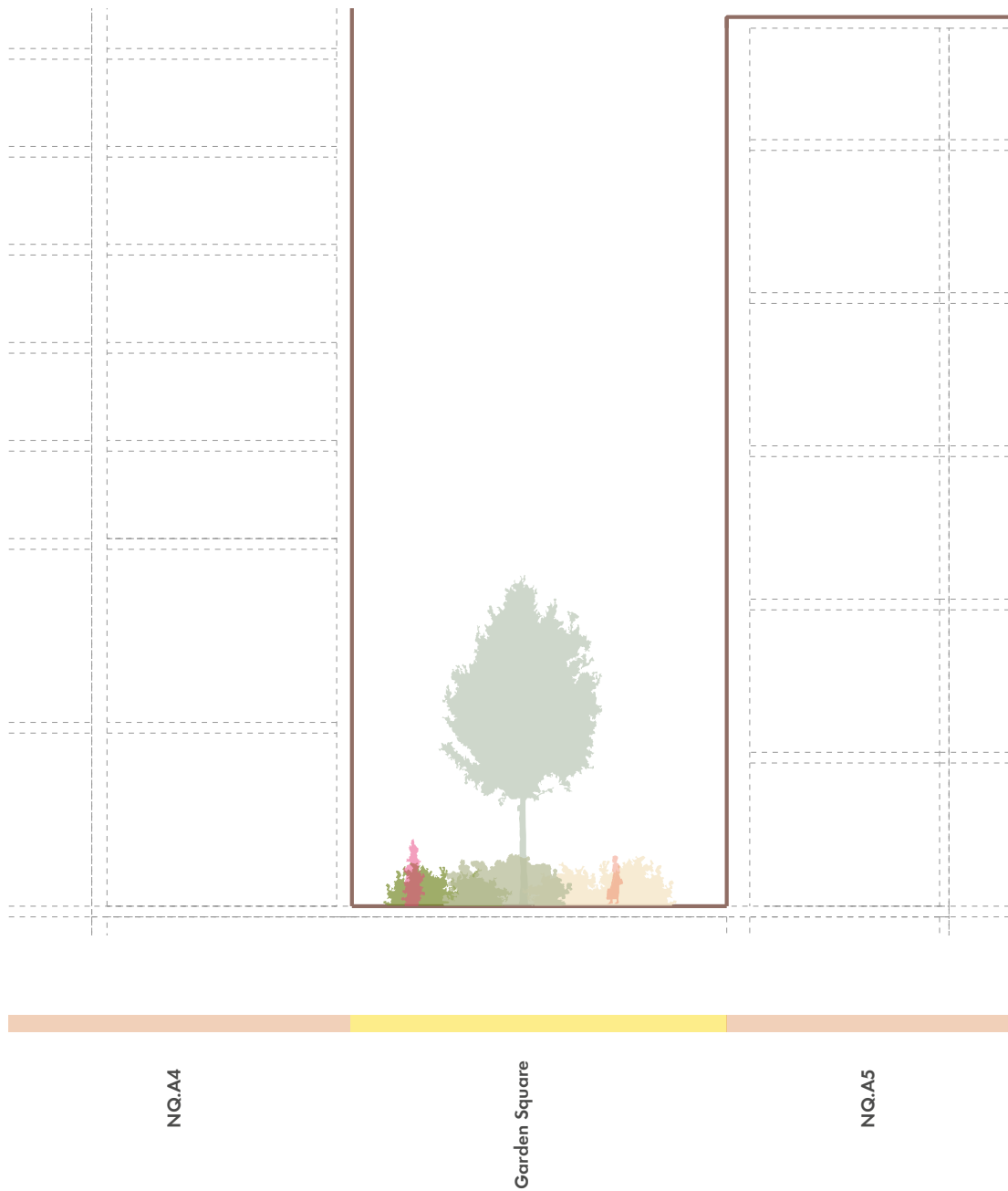
**Building lines along the east and west edges of Dock Square should be parallel to each other.**

This is to define Garden Square as a formal rectangular space, establishing connections to adjacent routes/spaces to the north and south.

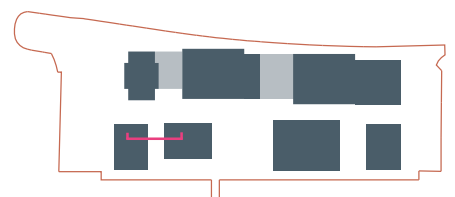
**Facades should give priority to street enclosure over the object-like quality of individual buildings.**

**Built frontages and planting should be designed to maximise natural light in Garden Square.**





Section through Garden Square



## 3.7.1

## ENVIRONMENTAL STANDARDS

a)

Soft and hard landscaping should be designed to support building forms/ canopies in providing an appropriately mitigated wind climate within each open space and route within the Masterplan.

Landscaping measures should assist to provide localised shelter and space to move through and dwell as appropriate to each space/route.

b)

Where possible (and not limited by existing ground levels interfaces), the ground levels of open spaces and routes should be raised above known 1:100 year flood levels.

This is to ensure that in the event of a flood the Site remains as accessible, permeable and resilient as possible.

c)

Where possible, surface water runoff is to be discharged into the dock to reduce reliance on the public sewer. SUDS is to be utilised where possible to improve water quality and reduce flows to the public sewer.

This is to reduce additional load on the public sewers and to reduce surface water flood risk off site. It will also help improve water quality.

d)

All planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.

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### 3.7.2

## TRANSPORT DESIGN STANDARDS

- a) **The Masterplan should accommodate visitor cycle stands which are distributed across the Site so that they are suitably accessible for building occupants and visitors as required.** To encourage cycling as a sustainable transport mode.
- 
- b) **There should be provision for a 32-point London Cycle Hire docking station at a location to be agreed with Transport for London.** To encourage the continued use of the cycle hire scheme as a city-wide sustainable transport system.
- 
- c) **Cycle routes should be designed to facilitate access to the proposed cycleway along Aspen Way and beyond to existing dedicated routes in the area, e.g. Cycle Superhighway 3 (CS3).** To encourage active travel and meet the Isle of Dogs and South Poplar OAPF aspirations to improve connectivity along and across Aspen Way.
- 
- d) **A north-south pedestrian route connecting South Poplar with Crossrail Place via the Aspen Way footbridge and the North Quay development will be provided. The clear route through the Masterplan should be a minimum of 3m wide (except where intermediate handrails are required on stair access).** This is to encourage active travel, connections to key public transport nodes and meet the Isle of Dogs and South Poplar OAPF aspirations to improve connectivity along and across Aspen Way.
- 
- e) **An east-west pedestrian route should be provided along the Quayside between the building face and dock edge as noted in Section 3.4. The clear route through the Masterplan should be a minimum of 4m wide, which allows for zones for roll out seating and informal play.** This is to encourage active travel, connections between West India Quay, Crossrail Place and Billingsgate and to meet the Isle of Dogs and South Poplar OAPF.
- 
- f) **The Masterplan should accommodate secure cycle parking for all live/ stay and retail uses, which are distributed across the Site so that they are suitably accessible for building occupants. Showers/lockers should be provided for employment uses.** To actively encourage cycling as a sustainable transport mode.
- 
- g) **The Masterplan should minimise car parking provision.** To actively encourage public transport and cycling as alternative sustainable transport modes.
- 
- h) **The Masterplan should be developed cognisant of the Docklands Light Railway (DLR) Site constraints and operational requirements.** To ensure that the DLR is not compromised by the Development.



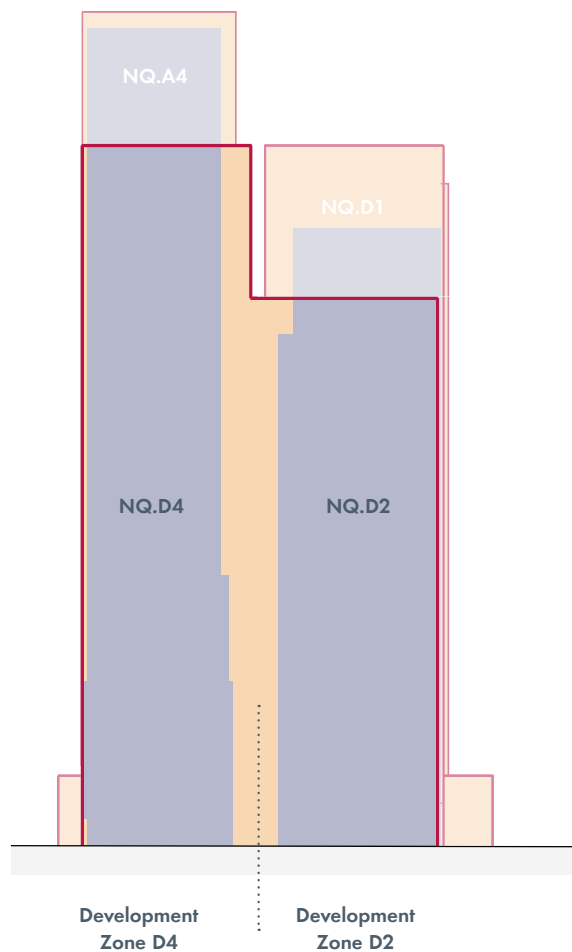
4

BUILDING  
TYPOLOGIES

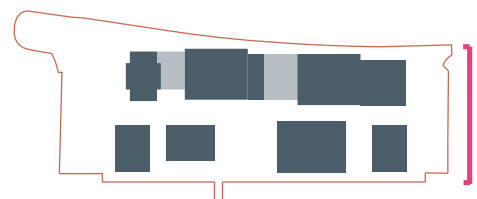
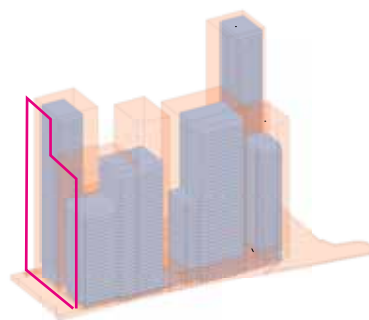
# 4.1

## SITE WIDE MASSING

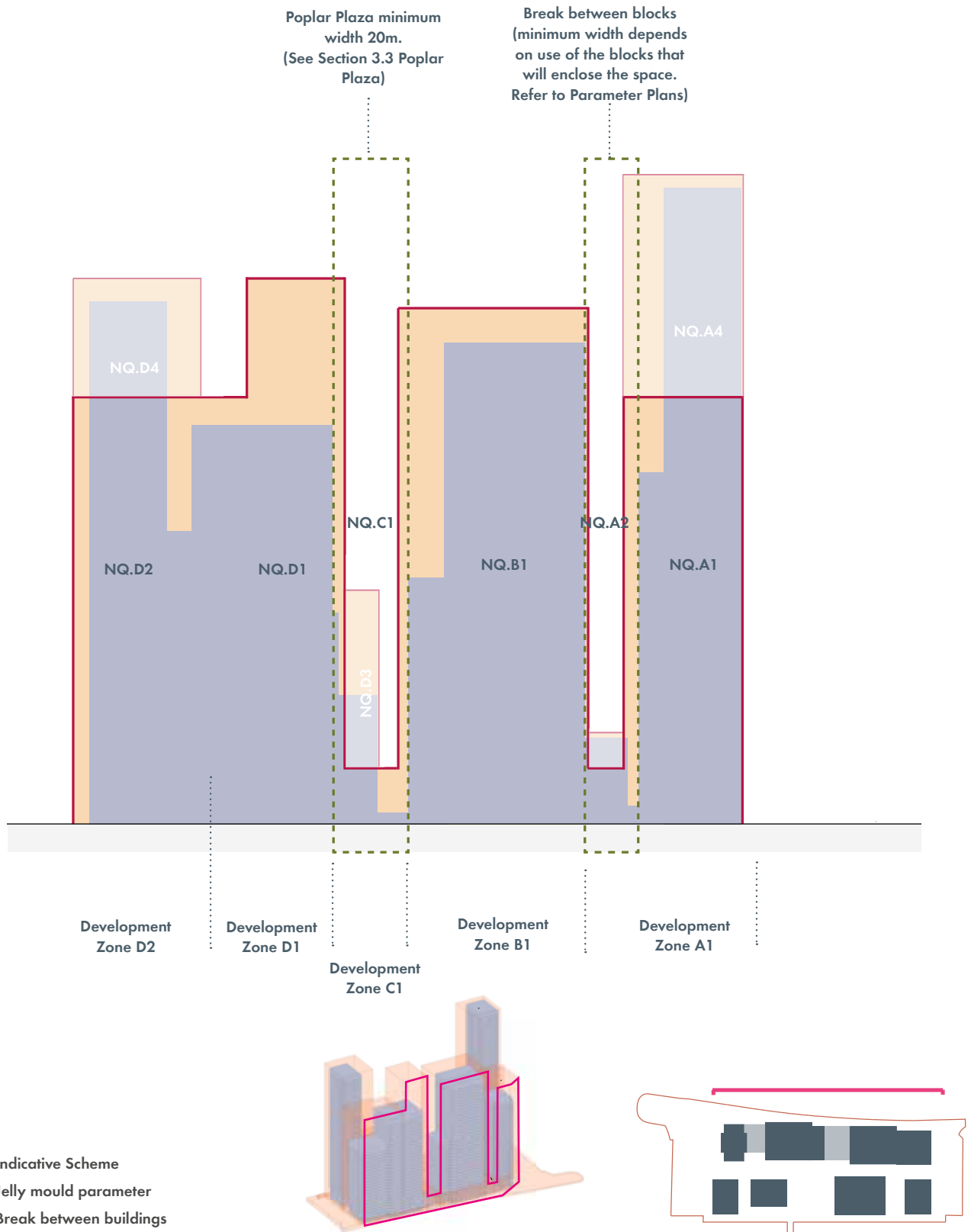
The elevations illustrate how the rules governing the distance between building faces and relationships of adjacent building heights will create variety, visual interest and prevent a wall of development onto Upper Bank Street and Aspen Way.



- Indicative Scheme
- Jelly mould parameter
- Break between buildings



East elevation from Upper Bank Street





## 4.2

## FLEXIBLE LIVE/STAY BUILDINGS

The Masterplan allows for flexible Live/Stay towers up to +225 metres AOD. These could include residential use, co-living and student accommodation. The Masterplan also allows for serviced apartments and hotel use.

Live/Stay buildings are defined as towers if they are over 20 storeys tall. These towers will contribute to the density and urban grain of North Quay, offering a wide variety of units ranging from studios up to 4-bedroom apartments which would offer a range of choice to potential residents. As a result of their presence on the wider Masterplan, the detailed design of the tower buildings should address the following:

### 4.2.1

### ACCESS

a)

Access from the street will be from a lobby entrance. **Lobbies should be designed as active frontages with a direct relationship to the adjacent public realm. Appropriate materials should be used to maximise transparency at the base of the towers.** This creates vibrant frontages to lobbies, retail and any other uses that form the remaining perimeter of towers at street level. It will also help animate the public realm and enhance natural surveillance. **There should be a level access from the street into the building.**

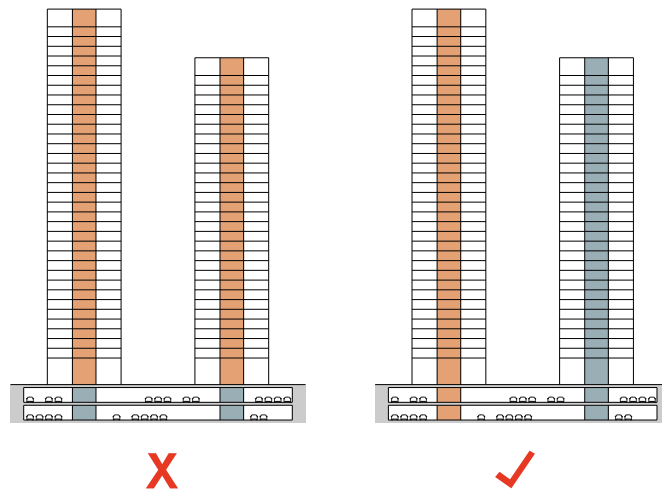
Entry lobbies with transparent frontages activate the public realm



b)

Access provided from any basement servicing/car park/cycle parking shared by various buildings, will be via a circulation core. **Tower cores should be easily accessible from the basement car park and be designed so that the individual tower buildings above ground are immediately identifiable.** This is so that tenants and visitors easily identify which building they are entering from the shared basement through the design of the circulation core.

Tower cores should be designed so that the individual tower buildings above ground are immediately identifiable

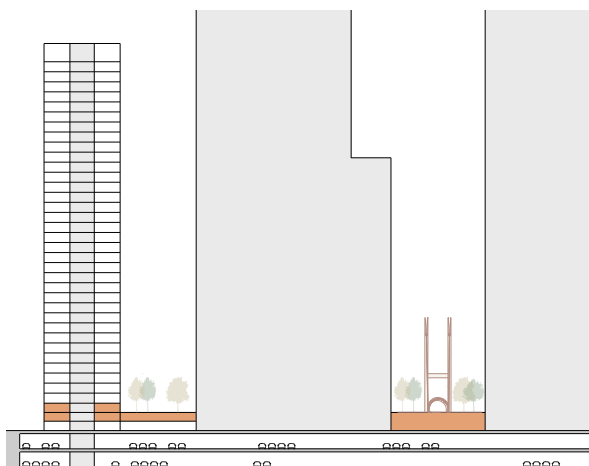


## 4.2.2

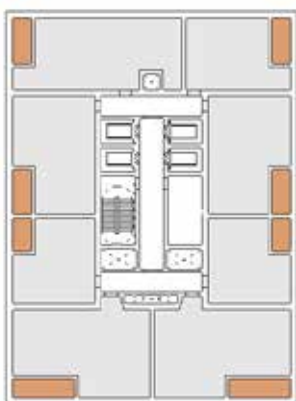
## AMENITY AND PLAY SPACE

a)

**Communal amenity space may be provided at ground, mezzanine and/or podium levels. Where appropriate, the opportunity may be taken to provide shared amenity space at roof level or within upper floor levels.** This will provide a choice of amenity spaces for shared use by residents, offering an enjoyable outdoor experience and forging connections between neighbours. Provision of external amenity space should be maximised where possible.



b)



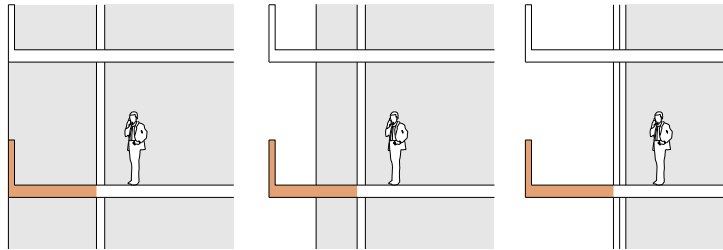
**Private amenity space for C3 residential units may be provided by balconies or winter gardens. Building setbacks and terraces at upper levels may provide private outdoor space for larger units at high level.** Residential units will therefore be provided with private outdoor space, utilising the built form of the tower to provide different private types of amenity space to the range of residential units. **Where it is demonstrated that it is not possible to provide private open space due to site constraints, dwellings may instead be provided with additional internal living space equivalent to the area of the private open space requirement. This area must be added to the minimum GIA.**

c)

Balconies, where provided, may be designed as projecting, recessed or semi-recessed, but should be fully integrated within the formal composition of the building and the architectural detail of the facades. Balcony soffits should be solid or opaque and should not be transparent or contain openings. Individual projecting balconies are allowed to project up to 2m beyond the Development Zone lines as defined in the Parameter Plans. This will ensure balconies have a positive impact on the architecture of buildings and provide useful, pleasant private outdoor space for residents.

Where balconies are inset they should stack vertically to avoid complexity in the thermal line and minimise thermal bridging.

Balconies may be recessed, semi recessed or projecting with a solid or opaque base



d)

Where residential units (C3 use class) are provided in the scheme, appropriate play space and playable landscaping should be provided to cater to age groups in line with the guidance in GLA SPG Shaping Neighbourhoods: Play and Informal Recreation (2012). This may be provided as a combination of indoor and outdoor spaces.

This will ensure adequate space is provided for children of all ages in the correct locations. Play spaces should be accessible by all residents on North Quay.

Avoid under 5 and 5 to 11 year olds play space immediately adjacent to Aspen Way, incl Delta Junction area.

### 4.2.3

## MASSING

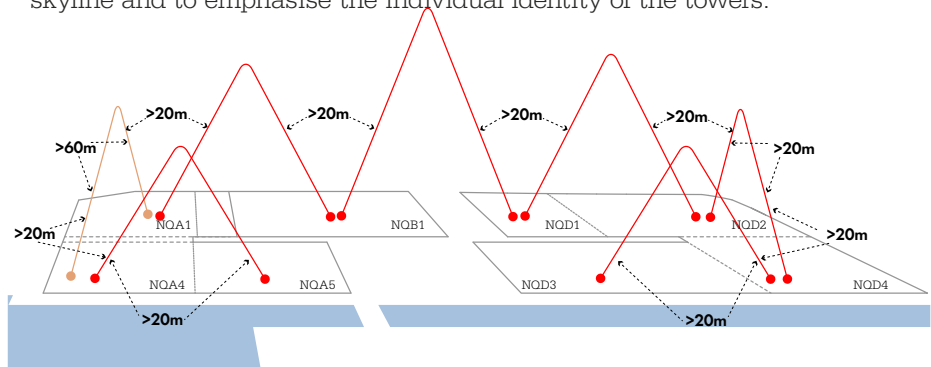
a)

There will be a minimum of 60 metres difference between the two live/stay buildings in Development Zone NQ.A1 and NQ.A4. The height difference of adjacent of buildings in NQ.B1 and NQ.A5 will be a minimum of 20m.

b)

All Development Zones other than the two live/stay buildings in Development Zone NQ.A1 and NQ.A4 , will have a minimum height difference of 20 metres. If the building uses in Development Zone NQ.A are not live/stay (e.g. office), then the height difference should be a minimum of 20m. This is to create a varied and informal skyline and to emphasise the individual identity of the towers.

Minimum Height Differences - Adjacencies Between Buildings Within Development Sub Zones



c)

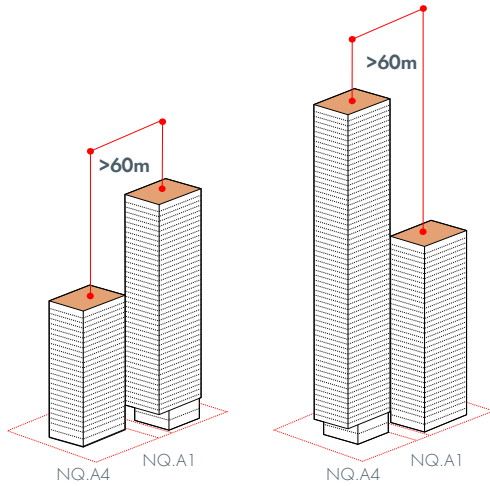
The maximum permissible floor plate of any live/stay tower is 1000m<sup>2</sup> Gross External Area (GEA). This ensures the relative slenderness of the towers to achieve views out of the Masterplan and to let light in.

d)

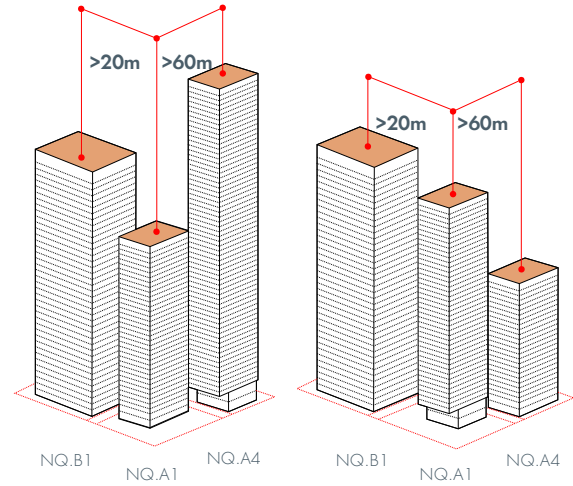
Live/stay towers should be detailed with a differentiated top and ground by employing one or more of the following strategies: setbacks, different cladding material, different facade system, different window proportion or increased percentage of glazing, etc. The built mass of tower buildings is thus broken down so that they have a positive impact on the public realm.

e)

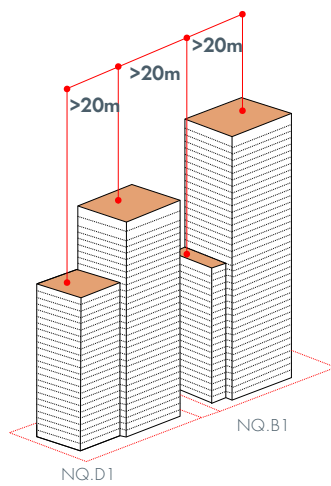
The minimum horizontal separation of residential buildings should be 18m, excluding any projecting balconies. Overlooking of habitable rooms in neighbouring buildings should be kept to a minimum and the privacy for the individual unit maintained. This also aids provision of panoramic views from the residential units and long range views between buildings from the public realm.



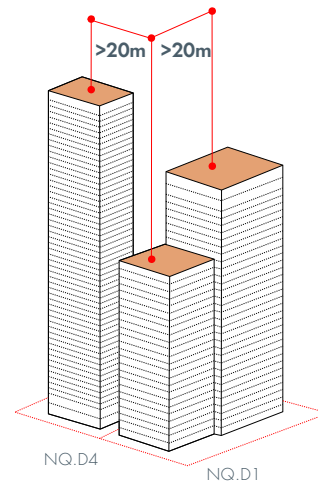
Height Difference in Development Zone NQ.A If Live/Stay Towers



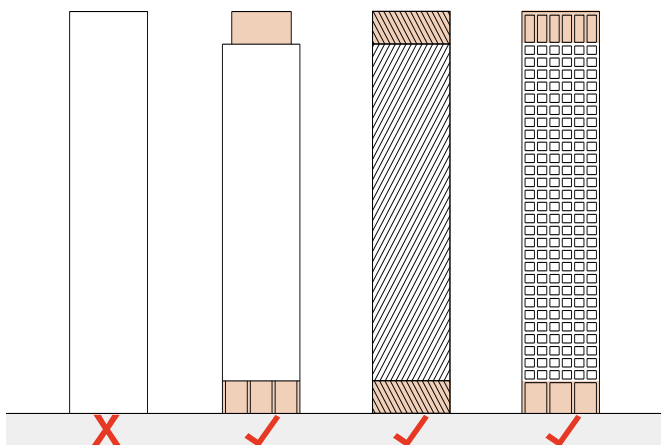
Height Difference in Development Zone NQ.A1 and NQ.B1



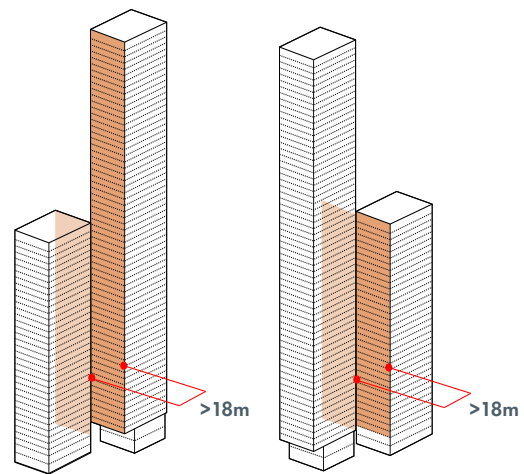
Height Difference in Development Zone NQ.B1 and D1



Height Difference in Development Zone NQ.D



Variation And Detail At The Building Top And Ground



Minimum Horizontal Separation For Residential Buildings

## 4.3

## RESIDENTIAL STANDARDS

The Mayor of London Housing Supplementary Planning Guidance (SPG) (2016) establishes design guidelines and space standards for new housing in London. It is recognised by all London boroughs and the Greater London Authority as one of the key documents to inform the design quality of new housing in the capital.

As across London, there will be demand for a wide range of residential unit types and sizes at North Quay to meet the differing needs of individuals, couples and family groups. A fundamental aim of the SPG is to ensure that London's housing is flexible and accessible in use and adaptable over the life of each building. Housing should support family life, both in the flexibility and adaptability of homes and in the provision, in greater quantities, of larger homes. The minimum space standards in the guidance aim to ensure that homes have the long-term adaptability to suit growing families or new owners and tenants. Within the SPG, the standards proposed are identified as either 'Baseline' or 'Good Practice'.

**Any residential homes (C3 use class) should aim to, where possible, meet the full range of standards set out in the GLA Housing Supplementary Planning Guidance (SPG) (2016).**

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### 4.3.1

### ENVIRONMENTAL PERFORMANCE

- a) **Internal noise levels within all new residential homes (C3 use class) should seek to comply with recommendations in BS 8233:2014 where reasonably achievable.**

This is to ensure that all new residential homes are built to appropriate acoustics liveable standards where possible.

- 
- b) **Noise levels within any shared residential amenity spaces should comply with the upper noise limit recommendations in BS 8233:2014 where reasonably achievable.**

This is to ensure that all new residential amenity space is built to appropriate acoustics standards where reasonably possible.

- 
- c) **Balcony and rooftop/podium terrace balustrades should be designed to provide a suitable environment with protection from wind.**

This is to ensure that residential amenity spaces are suitable for use. Solid, or limited perforation of materials may be considered. For balconies,



recessing is also recommended with placement away from exposed corners.

d)

**All residential dwellings and live/stay rooms are to be located above the first floor level with no habitable rooms located at ground/first floor level.**

**As far as possible, entrance lobby levels should also be raised above 1:100 year flood levels.**

This is to ensure that all new occupied live/stay spaces are located above the extreme tidal flood level.

This is also to protect future residents from higher traffic emissions close to Aspen Way and overlooking from the DLR.

**All residential dwellings should be designed in accordance with the principles of and tested against CIBSE TM59 to manage the risk of overheating.**

This is to ensure that all new residential dwellings consider the impacts of summertime comfort beyond today's climate.

## 4.3.2

## ENVIRONMENTAL CERTIFICATION

a)

**All residential dwellings should be certified against the Home Quality Mark.**

# 4.4

## OFFICE BUILDINGS

There will be the potential within the Masterplan to allow for office buildings up to + 180m AOD. They will offer flexible floor plates to larger tenants or subdivided into areas for smaller tenants. There will be retail units at ground and mezzanine levels, with office activities located on floors above. Because of their presence on the wider Masterplan, the detailed design of these buildings should address the following:

### 4.4.1

#### ACCESS

a)

Access from street level to office space on upper floors will be through a communal lobby entrance, shared by tenants of each building. **Lobbies should be designed as active frontages with a direct relationship to the adjacent public realm. Appropriate materials should be used to maximise transparency at the base of the building.**

This creates vibrant frontages to lobbies, retail and any other uses that form the remaining perimeter of office buildings at street level. This will help animate the public realm and enhance natural surveillance. **There should be a level access from the street into the building lobbies.**

b)

Access from any basement servicing, car park or cycle parking, shared by various buildings, will be via a circulation core. **Access cores should be easily accessible from any accessible basement areas and be designed so that the individual building is identifiable from below ground.** This is so that staff and visitors easily identify which building they are entering from the shared basement through the design of the circulation core.

### 4.4.2

#### AMENITY SPACE

a)

**Where possible, opportunities should be taken to provide amenity space at upper floor levels by use of building setbacks and terraces/ roof gardens.** This provides unique outdoor space for shared use by building occupants.

b)

**Balconies may be incorporated into the language of office buildings. Balconies may provide private amenity space to upper floor offices. Balconies may be designed as projecting, recessed or semi-recessed, but should be fully integrated within the formal composition of the building and the architectural detail of the facades.**

**Individual projecting balconies are allowed to project up to 2m beyond the Development Zone boundary as defined in the Parameter Plans.** This ensures balconies have a positive impact on the architecture of building and provide useful, pleasant, private outdoor space for staff.

## 4.4.3

## MASSING

a)

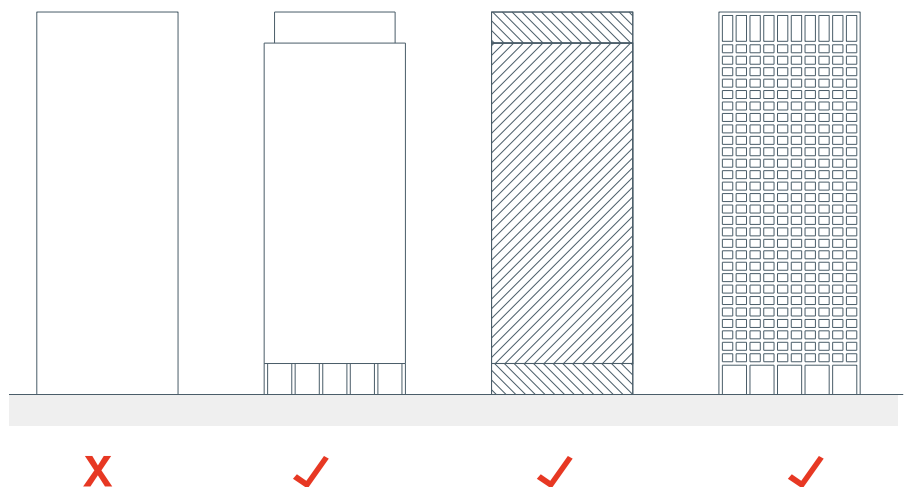
The height difference to adjacent buildings for any commercial building, including offices, will have a minimum height difference of 20 metres. This is to create a varied and informal skyline and to emphasise the individual identity of the building.

b)

Office buildings should be detailed with a differentiated top and ground by employing one or more of the following strategies: setbacks, different cladding material, different facade system, different window proportion, increased percentage of glazing, etc.

The built mass of office buildings is thus broken down so that they have a positive impact on the public realm.

Office buildings should be detailed with a differentiated top and ground. Adjacent buildings should differentiate from one another to create richness.



## 4.4.4

## ENVIRONMENTAL CERTIFICATION

a)

All offices should achieve a minimum BREEAM rating of Excellent.

# 4.5

## SMALL BUILDINGS

Development Plot NQ.A5 is a Small Building within the Masterplan. It has an important urban role in forming the sides of both Quay Square and Garden Square, whilst also forming edges to the Quayside and North Quay Way. In this respect, it is a building with four active frontages and should be designed to respond to each of these accordingly.

### 4.5.1

### ACCESS

- a) The Small Building may provide access to and from the basement levels beneath the square. Its circulation core should be designed so that the character of the building is appreciated from below ground.

**People should be encouraged to enter the building from ground level, which will activate any public areas around it.**

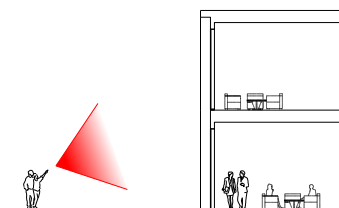
- b) **Access to the Small Building will be at ground floor level and entry points should be provided on North Quay Way with active spill out food beverage seating spaces on to Quay Square and The Quayside.**

This is to encourage people to enter the pavilion from the central street within the Masterplan and to create activity on a number of building faces along public spaces. Smaller active spill out food/beverage seating spaces on to Garden Square should also be considered.

- c) **The Small Building should be designed with predominantly active frontages along the upper and lower floors. Appropriate materials should be used to maximise transparency of the building at ground and first floor levels and create vibrant frontages.** This is to ensure the building acts as beacon of interest within the Site and as a landmark within its immediate context, activating the public realm.

**Where ventilation grilles are unavoidable, they should be limited in extent and integrated with the architecture of the building.**

Small buildings should be designed with predominantly active frontages along the upper and lower floors



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## 4.5.2

### AMENITY SPACE

a)

The Small Building should be designed with flexible units whose ground floor amenity space spills out onto Quay Square and The Quayside. Landscape design should provide amenity space for these pavilions, in-keeping with adjacent public spaces.

**Sufficient provisions should be made for spill out space and for patrons to feel like they are part of the wider civic space when using the Small Building amenity space.**

b)

**Balconies/roof terraces may provide additional outdoor space for the Small building at upper levels. Balconies may be designed as projecting, recessed or semi-recessed, but should be fully integrated within the formal composition of the building and the architectural detail of the facades.**

Building setbacks at upper levels and roof levels may provide amenity space where possible. This is to ensure balconies have a positive impact on the architecture of the pavilion and provide pleasant and unique outdoor space.

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## 4.5.3

### MASSING

a)

**The massing of the Small Building should be designed to relate to human scale and contrast to the larger buildings in adjacent Development Zones. The design, detailing and massing should distinguish the Small Building as a more distinct 'pavilion' object within the larger context and scale of North Quay.**

# 4.6

## ROOF TOPS

The Masterplan will have buildings varying dramatically in height; from pavilion Development Plots through to tower buildings. This contrast provides a diverse and interesting skyline which helps to establish the unique character of North Quay. The rooftops of buildings become their fifth elevation as they are overlooked from all directions and they therefore contribute significantly to the nature of the area. The design of all rooftops requires detailed consideration and should adhere to the following guidelines:

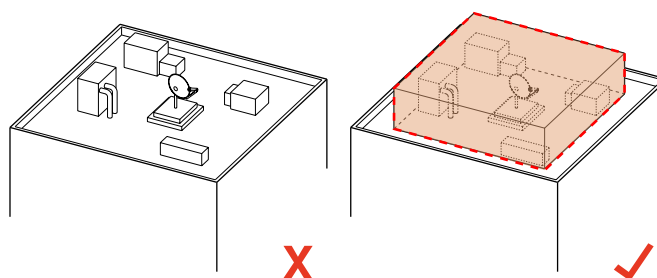
### 4.6.1

### ROOF TOP GUIDELINES

a)

**Buildings should have an uncluttered, simple roof profile with all elements such as plant enclosures, maintenance gantries, lift overruns, safety balustrades etc. forming an integral part of the overall building form. Where the correct functioning of the equipment permits, plant should be housed within solid or perforated roof enclosures that are designed to screen equipment from the public realm or when overlooked by neighbouring buildings or exposed in long views.** North Quay should be provided with a tidy, considered roofscape which will enhance the public realm and views from tall buildings.

Overlooked plant should be contained to screen it



- b) **Where plant is not overlooked (taking into account existing, consented or pipeline schemes) it may be concealed behind a parapet so as to be invisible from the public realm when not in use. Any mass projections rising more than 1m above the parapet line should aim to be set back by at least 3m.** This is to retain visually consistent parapet treatments and avoid visual clutter seen from the street level and adjacent buildings.
- c) **The Masterplan should incorporate areas for biodiverse roofs (substrate based and planted with wildflowers and added invertebrate habitat). Consideration should also be given to incorporate some areas of vertical greening, rain gardens and bird/bat boxes.** This will provide areas for wildlife habitat and also enhance visual amenity.
- d) **Any combustion plant flues should be located to terminate above upper roof levels and be excluded from lower level facades/podium roofs within 20m of ground level.** This will ensure good dispersion of flue gases and prevent any air quality impacts from such emissions.
- e) **Where possible opportunity should be taken to provide shared/private amenity space at roof level in the form of roof terraces.** This will provide residents/tenants/visitors North Quay a unique collection of outdoor spaces with panoramic view of the neighbourhood.
- f) **All planting should comply with detailed ecologist recommendations and recommended species list provided in the masterplan Ecological Impact Assessment (NQ.PA.27) and meet requirements of Canary Wharf Biodiversity Action Plan 2018 – 2028.**



# 4.7

## ENTRANCES & STREET FRONTAGES

North Quay's public realm will be animated by the active frontages of its many different uses at ground level: shops, cafes, restaurants, bars, leisure uses, shared office lobbies and shared residential lobbies. Entrances to these should be celebrated and visitors should be welcomed inside to ensure a busy and lively community is established. Entrances should be evenly distributed throughout the routes and spaces of North Quay and should be located to provide a beneficial connection with the public realm. The design of each entrance requires detailed consideration and should adhere to the following guidelines:

### 4.7.1

### ACCESS

a)

**Entrances are to be fully integrated with the architecture of the building and have a clear hierarchy compared to the remainder of the street frontage. They should have an appropriate level of prominence and be well signed.** This is to maximise access and provide visitors with a legible understanding of public buildings.

Entrances should be clear whilst being integrated into the facade



b)

**Entrances should be expressive of the individual character of the unit to which they offer access, while retaining the continuity of the street frontage.** This allows the individuality of unit to become a positive feature of the street, while still delivering a coherent frontage to the public realm.

Entrances should express the character of the unit

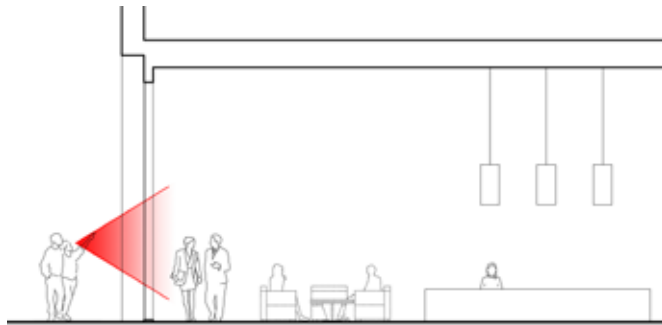


c)

**Entrance doors should be predominantly glazed to offer views into the retail unit or office lobby, whilst providing security as necessary.**

This is to encourage people into the unit and provide unobstructed views from the public realm into the unit.

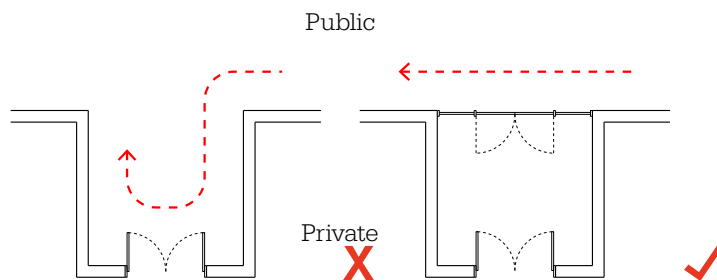
Entrances with glazing offer views



d)

**Where entry or escape doors are recessed by more than 1.5m from the predominant line of ground floor frontage and not located at the perimeter of the Development Zone, night-time closures could generate a cul-de-sac condition. Therefore, gates should be included at the perimeter of the building line.** This is to ensure security for pedestrians on affected routes and prevent any hidden alcoves.

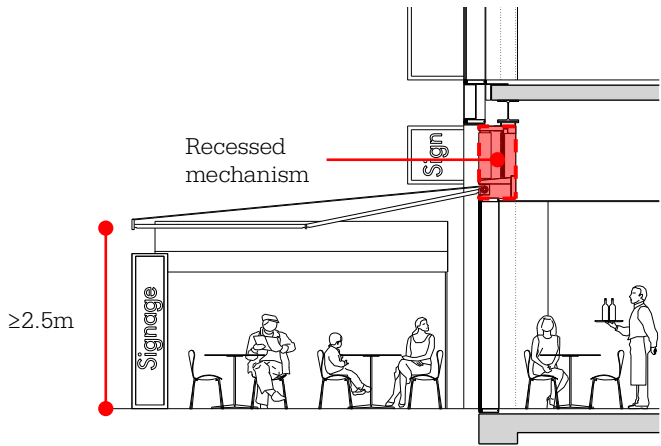
Where recessed, entrances should be gated to prevent cul-de-sac conditions at night



e)

Awnings may be provided at ground or mezzanine level but should be restricted to the same height along the street in any one Development Plot. When opened, the soffit should be a minimum of 2.5m above the street level beneath. Mechanisms required to control the awnings such as tracks, switches and head boxes are to be fully recessed and integrated into the design of the elevation and not project forward. Materials should be complimentary to the main facade and should provide diversity and animation to the public realm. They should typically be lightweight to minimise structural requirements, should be adjustable to provide shade from varying sun angles throughout the day and be easily retractable at night or in adverse weather conditions. This ensures that awnings become a positive addition to the public realm and complement each other and the built facade in a tidy fashion, while providing shelter to outdoor seating areas and enhancing the solar condition within the ground floor unit

Awnings should be carefully designed to enhance the public realm



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f)

**All shutters or grilles should be installed within the ground floor unit. They may not be fitted externally except when utilised in servicing entrances. Solid shutters will not be acceptable. Shutters are to be fully retractable and be aluminium or lightweight steel with a powder coated finish.** This should limit the detrimental effects of shuttering on the public realm.



# 5

## GENERAL GUIDELINES

# 5.1

## GENERAL GUIDELINES

### 5.1.1

#### BUILDINGS

- a) Rainwater and sanitary waste pipes should not be visible on the facades, but where unavoidable should be recessed/fully integrated into the architecture of the building and facade composition.
- b) In areas where the building line is held back from the Development Zone boundary, the residual space should be designed to complement/be contiguous with the adjacent public realm.
- c) The provision of projecting canopies on buildings are allowed up to a distance of 2.5m. Canopies must be integrated into the architecture of the building and should not obstruct the width of the road or footpath, nor compromise the integrity of the public realm.
- d) The use and scale of signage should be sympathetic to the building on which it relates as well as the whole character of the public realm.  
**Signage can be used on any building typology within North Quay, therefore it should be bold, clear and legible.**  
  
Signage for retail units can vary, for example signage can be set behind or in front of glass frontages, be applied to awnings, or can be fixed to or integrated into the building facade. **Any integrated or fixed signage should be sympathetic with the architecture of the building on which it is located, and any projecting signs should not obstruct the road or footpath, nor compromise the integrity of the public realm.**  
  
Signage for residential buildings or offices located above retail on the ground floor, should be clearly legible and distinguishable from the ground floor public realm, as well as distinguishing it from any retail signage located nearby.
- e) Buildings should meet the ground with a level transition from outside to inside. Ground junctions should be carefully designed to mitigate detrimental effects of steps and slopes at threshold locations. All internal ground floor levels should correspond to external levels with a vertical tolerance of up to 500mm (except along Aspen Way Gardens where an elevated ground level is acceptable). This would minimise the effect of the slope on accessibility and connectivity between retail and the public realm.



f)

**All buildings providing employment i.e. commercial/retail uses should be designed to maximise access to all parts, providing appropriate facilities and services for people who are visitors and members of staff regardless of disability and as required by local, regional and national policy;**

To ensure that required standards for accessibility are met and as part of mainstream inclusive design wherever possible;

To design inclusively, which means designing beyond the minimum requirements of the Building Regulations Part M to ensure that all people, regardless of age, sex or ability can use and enjoy the built environment;

To address the anticipated, substantial increase of older people in proportion to the working-age population and their future needs;

To meet the aims of the Equality Act (2010) where applicable; and  
To follow design guidance given in relevant British Standards and other currently published good practice guidance about meeting the needs of disabled people and inclusive design.

g)

**All residential buildings should be designed to maximise access to all parts, providing appropriate facilities and services for people who are residents, visitors and members of staff regardless of disability and as required by local, regional and national policy:**

To ensure that required standards for accessibility are met and as part of mainstream inclusive design wherever possible;

To design inclusively, which means designing beyond the minimum requirements of the Building Regulations Part M to ensure that all people, regardless of age, sex or ability can use and enjoy the built environment;

To address the anticipated, substantial increase of older people in proportion to the working-age population and their future needs;

To meet the aims of the Equality Act (2010) where applicable;

To follow design guidance given in relevant British Standards and other currently published good practice guidance about meeting the needs of disabled people and inclusive design; and

To address the principles relating to GLA's guidance (Housing SPG) 2016 on wheelchair adaptable housing as required by London Borough of Tower Hamlets.

h)

Lighting design for buildings should balance a number of criteria such as character and ambience with legibility and way finding and safety and security, as well as sustainability and light pollution. **Glare and light pollution should be controlled to minimise stray light impacts on residential amenity.**

Residential buildings may be gently illuminated via concealed light sources. **At ground level, increased ambient lighting levels should be incorporated to aid the transition from exterior to interior.**

**Commercial and other non-residential buildings may be illuminated to enhance key architectural features.** Lighting may be used to express buildings at ground level, particularly at entrances and to enhance active frontages (including commercial parts of residential buildings e.g. ground floor retail or leisure). **Illuminated signage to retail units, leisure and commercial buildings should be carefully designed to integrate with the design approach and architecture of the building.**

**Light fittings should be concealed where possible, or otherwise designed and positioned to reflect the architecture and character of the building. External cabling/conduits should be avoided where possible.**

i)

**All live/stay buildings should consider maximising daylight to internal rooms and minimise single aspect north facing units where possible**

j)

**All buildings should be designed utilising high quality and appropriate external cladding materials and weathering detailing.**

This is to provide buildings with an appearance that will weather well and have longevity. The quality of the architecture should be commensurate with that evident in the existing Canary Wharf estate and in Wood Wharf.

Detailed design of the buildings will focus on maximizing the longevity of the structure and façade informed using whole life-cycle carbon analysis. Structural design will consider adaptability approach and opportunities for change of use of individual floor plates, to allow multiple functions and layouts. The façade system's service life, replacement, maintenance and end of life considerations will form part of the options appraisal for potential systems. Please refer to the sustainability statement for further detailed information

**All buildings should be designed in line with masterplan Net Zero Carbon strategy detailed in the Energy Statement (NQ.PA.17) and meet detailed requirements of designated environmental performance certification scheme (BREEAM, HQM). See Sustainability Statement (NQ.PA.16) for further details.**

## 5.1.2

## LANDSCAPE

- a) **Landscaping should be designed to allow access for emergency and servicing vehicles where necessary. These routes should be fully integrated with the landscape design and be allowed for by the careful placement of trees and street furniture, they should not compromise the integrity of the public realm.**
- b) **All areas of the public and private landscape should be designed to maximise access through and to all parts of the Development, its facilities and services for people who are residents, visitors and members of staff regardless of disability and as required by local, regional and national policy:**
- To ensure that required standards for accessibility are met and as part of mainstream inclusive design wherever possible;
- To design inclusively, which means designing beyond the minimum requirements of the Building Regulations Part M and Part K to ensure that all people, regardless of age, sex or ability can use and enjoy the built environment;
- To address the anticipated, substantial increase of older people in proportion to the working-age population and their future needs;
- To meet the aims of the Equality Act (2010) where applicable;
- To follow design guidance given in relevant British Standards and other currently published good practice guidance about meeting the needs of disabled people and inclusive design; and
- To address the principles relating to the GLA's guidance (Housing SPG) 2016 on wheelchair adaptable housing as required by LBTH.
- To maximise environmental net gain across the development and meet requirements of CWG Biodiversity Action Plan
- c) **Landscaping should be designed to use a wide range of plants and tree species, encouraging biodiversity and using local or indigenous species where possible. There should be a good balance between hard and soft landscaping in consideration of SUDS (Sustainable Urban Drainage Strategy).**

d)

Lighting within streets and landscaping should reflect the use and character of each space with the primary aim of creating a secure and pleasant environment. **Vehicular routes should provide sufficient illumination for vehicles, cyclists and pedestrians taking into account trees and street furniture, whilst limiting light spillage to a minimum.**

**The hierarchy of routes should be consistently reflected in the choice of lighting equipment and types and levels of lighting deployed.**

For example, pedestrian routes and landscaping will benefit from more localised lighting, resulting in a subtler, more gentle illumination.

Creative use of buildings and street furniture to house or conceal light fittings serving the adjacent public realm, thereby minimising potentially obtrusive poles and other elements, should be encouraged.

**External amenity and sports areas may require specialised lighting, enabling activities to continue during hours of darkness, as appropriate. Consideration will need to be given to ensure that adjacent buildings and spaces e.g. residential buildings, public highway, etc, are not adversely affected by glare or lighting spill.**

e)

In general, it is accepted that landscape features such as trees, arbours and planting, street furniture, lighting, bus shelters, signage, CCTV etc may protrude beyond the maximum heights stipulated on Parameter Plan NQMP-PP-009 Development Sub Zones and Maximum Heights'.

f)

**Where trees are planted, suitable soil depth and irrigation should be provided to enable establishment and encourage growth. This should be specific to the species of trees and the spacing of their planting, but typically the height of the growing medium will be between 1100 and 1000 mm and mounding when necessary between 200 and 300 mm.**

## 5.2

## WASTE & THE CIRCULAR ECONOMY

a)

Canary Wharf Group have a Zero Waste to Landfill Policy and Circular Economy Requirements. The development should make best endeavours to meet the objectives of Policy SI7: Reducing waste and supporting the circular economy within the Draft London Plan. This is to promote waste reduction, increasing material re-use and recycling, and reductions in waste going for disposal. A Circular Economy Statement defining the masterplan strategy has been submitted as part of the outline planning as part of the Sustainability Statement which developments should align to.

b)

### **A Circular Economy Statement should be submitted with each Reserved Matters Application.**

This should demonstrate:

1. How potential for waste arisings have been minimised through Designing Out Waste workshops with the extended design team and the Contractor.
2. How all materials arising from demolition and remediation works will be re-used and/or recycled
3. How design responds to circular economy principles and incorporates closed-loop products that can be taken back and re-purposed or re-manufactured
4. How the proposal's design and construction will enable building materials, components and products to be disassembled and re-used at the end of their useful life
5. Opportunities for managing as much waste as possible on site
6. Adequate and easily accessible storage space to support recycling and re-use
7. How much waste the proposal is expected to generate, and how and where the waste will be handled, including how Canary Wharf Group's Zero Waste to Landfill policy will be met.

c)

**Each building and separated uses within should be provided with adequate and easily accessible storage space(s) that supports the separate collection of dry recyclables (at least card, paper, mixed plastics, metals, glass) and food.**

This is to promote residents and occupiers to maximise the separation and recycling of their waste.



6

GLOSSARY

<b>Active Frontages</b>	<p>A building front that promotes activity and encourages movement between the building at public realm level and the adjacent public realm by the way the building front is designed or orientated.</p> <p>A building provides active frontage if the building at public realm level avoids blank walls or obscured frontages, includes windows and openings, and provides a variety of uses. It must contribute to natural surveillance and support the visual and physical relationship between the building and the external public realm.</p> <p>Residential and office entrance lobbies, retail units and other building parts, whether for public or private uses, can therefore be deemed to have an active frontage if designed in accordance with the above.</p>
<b>Amenity</b>	A positive element or elements that contribute to the overall character or enjoyment of an area. For example, open land, trees, historic buildings and the inter-relationship between them, or less tangible factors such as tranquillity.
<b>The Applicant</b>	Canary Wharf (North Quay) Ltd
<b>Building</b>	A Building is an occupiable built structure that may be built within development zones set by the Parameter Plans
<b>Building Line</b>	Where the elevation of a building should meet the ground.
<b>Building Structure</b>	Any building or part of building including a colonnade, portico, undercroft or passage.
<b>Business use</b>	This class is formed of three parts: B1(a) Offices - other than a use within Class A2; B1(b) Research and Development of products or processes; and B1(c) Industrial processes As defined by the Town and Country Planning (Use Classes) Order 1987 (as amended)
<b>Character</b>	A term relating to 'Conservation Areas' or 'Listed Building', but also to the appearance of any rural or urban location in terms of its landscape or the layout of streets and open spaces, often giving places their own distinct identity.
<b>Conservation Area</b>	Areas of special architectural or historic interest defined by the local authority, the character or appearance of which it is desirable to preserve or enhance.
<b>Control Documents</b>	The Specified Parameters are set out in the three Control Documents: the Parameter Plans, the Development Specification and the Design Guidelines.
<b>Cumulative Impact</b>	A number of developments in a locality or a continuous activity over time that together may have an increased impact on the environment, local community or economy.
<b>Density</b>	In the case of residential development, a measurement of either the number of habitable rooms per hectare or the number of dwellings per hectare



<b>Design Guidelines</b>	The Design Guidelines set out the guidelines by which any Reserved Matters Applications would need to follow if they are to be considered acceptable.
<b>Development Plot</b>	A building that can arrive within a Development Zone, which is defined by a maximum height and envelope. Development Zones may contain single or multiple Development Plots.
<b>Development Specification</b>	The Development Specification sets out a written account of the Parameter Plans and describes the OPA and the type and quantity of development that could be provided within each of the Development Zones across the OPA Site as a whole.
<b>Development Plan</b>	A Development Plan comprises a set of documents that set out the policies and proposals for the development and land use of an area. The relevant Development Plan for LBTH is the London Plan (2011) and the LBTH Local Plan which consists of the Core Strategy (2010) and Managing Development Document (MDD, 2013).
<b>Development Zone</b>	Areas within which buildings can arrive, which are defined by a maximum length, width and height.
<b>The Environmental Statement for the Outline Planning Application</b>	The 'Environmental Statement' for the 'Outline Planning Application' or the 'ES'.
<b>Environmental Impact Assessment (EIA)</b>	EIA is a process carried out which examines available environmental information to ensure that the likely significant environmental effects of certain projects are identified and assessed before a decision is taken on whether a project is granted planning permission.
<b>Equalisation Statement</b>	A document which is submitted with each 'Reserved Matters' application in order to ensure that the 'Proposed Development' is built out in accordance with the 'Specified Parameters' and ranges set out in the OPA. The 'Equalisation Statement' will confirm that the remaining balance of uses for which 'Outline Planning Permission' has been granted is capable of being delivered in a manner that satisfies the requirements of the 'Specified Parameters' and any relevant planning conditions or planning obligations.
<b>Frontage</b>	Portion of building envelope built out to the Building Line.
<b>Ground Level</b>	The floor of a building that is at or nearest to the level of the ground around the building.
<b>Hard/Soft Landscaping</b>	Hard landscaping is the provision of features such as paving, lighting, seating, etc. whilst soft landscaping is the provision of plants, shrubs and trees to improve the quality of the environment.
<b>Inclusive design</b>	Designing the built environment, including buildings and their surrounding spaces, to ensure that they can be accessed and used by everyone.
<b>Indicative Scheme</b>	The Indicative Scheme demonstrates one interpretation of the Specified Parameters.
<b>Jelly Mould</b>	The shape formed from the maximum development parameters.

<b>Limit of Deviation</b>	The extent to which defined boundaries and levels may deviate from those shown on plan.
<b>Listed Building</b>	A building or structure of special architectural or historic interest. 'Listed Buildings' are graded I, II* or II with grade I being the highest.
<b>Listed Building Consent Application</b>	Application to stabilise listed quay wall and any associated/necessary remedial works as well as demolition of the false quay
<b>The Masterplan</b>	The Proposed Development takes the form of a Masterplan and as such, these terms can be used interchangeably.
<b>Masterplan Framework</b>	The culmination of the Parameter Plans, Development Specification and the Design Guidelines.
<b>Mixed use (or mixed use development)</b>	Provision of a mix of complementary uses, such as business, residential, community and leisure uses, on a site or within a particular area.
<b>Public Open Space</b>	Public Open Space is defined as those areas where access for the public is permitted. In accordance with the definitions set out in the Local Plan 2031 (adopted 2020) it does not include areas of water or incidental areas, such as road verges, or streets (unless these form part of a link in the open space network)
<b>Outline Planning Application (OPA)</b>	The 'Outline Planning Application' for the 'Outline Planning Application Site' or the 'OPA Site'.
<b>Outline Planning Permission (OPP)</b>	The type of planning permission which is being sought for the 'Proposed Development' of the 'OPA Site'
<b>Overlooking</b>	A term used to describe the effect when a development or building affords an outlook over adjoining land or property, often causing loss of privacy.
<b>Parameter Plans</b>	The Parameter Plans define the extent of the proposed routes, open spaces and Development Zones across the OPA Site against a series of minimum or maximum dimensions. Each of these component parts is identified as a Development Zone which is identified by a letter (e.g. Development Zone NQ.A).
<b>Permitted Uses</b>	Those uses which are permitted on North Quay, defined in the Development Specification.
<b>Phasing or Phased Development</b>	The phased construction of the development into manageable parts.
<b>Planning Obligation</b>	A legally enforceable obligation entered into under Section 106 of the Town and Country Planning Act 1990 to mitigate the impacts of a development proposal. Sometimes called 'Section 106' agreement.
<b>Predominantly</b>	Equates to at least 51% of the specified use, length, area, frontage, etc.
<b>Predominant Use</b>	The use which dominates by occupying the largest amount of floor area within the building.

<b>The Proposed Development</b>	The applications ('Outline Planning Application' and 'Listed Building Consent') will be made for the 'Proposed Development'.
<b>Reserved Matters</b>	The 'Outline Planning Application' seeks approval for 'Specified Parameters' relating to the use and amount of the 'Proposed Development' and reserves details relating to matters of access, appearance, landscaping, layout and scale ('Reserved Matters'), for approval in 'Reserved Matters Applications'.
<b>Reserved Matters Application</b>	A 'Reserved Matters Application' seeks the approval of one or more Reserved Matters (See 'First Reserved Matters Application')
<b>Retail</b>	Use Classes A1, A2, A3, A4 and A5 as defined by the Town and Country Planning (Use Classes) Order 1987 (as amended).
<b>Secured by Design</b>	The national police scheme which aims to minimise crime and opportunities to commit crime through better design of buildings and places.
<b>Setbacks</b>	Where the Frontage of a building is not extended to the limits of the building envelope.
<b>Site</b>	The 'Outline Planning Application Site' or the 'OPA Site'. The site area enclosed by the 'Outline Planning Application' red-line boundary on application drawings.
<b>Specified Parameters</b>	The Proposed Development will be defined by Specified Parameters. The Specified Parameters are set out in the three Control Documents.
<b>Swing Block/Plot</b>	A 'Development Plot' which has no predominant use allocation and therefore can be allocated to any of the Permitted Uses in any combination.
<b>Upper Floor</b>	Any floor located above ground and upper ground (or mezzanine) floor.
<b>Upper Ground Level</b>	A floor halfway between the ground floor and the next higher floor, typically known as a mezzanine.
<b>1 in 100 yr Flood Level</b>	A 1 in 100 year flood level is an event that has the probability of occurring 0.1% per year.

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