

7

THE MASTERPLAN PROPOSAL



# 7.1 OVERVIEW

"The Masterplan is not a building, and it does not prescribe the design of buildings. Instead it defines a physical matrix into which individual buildings are to be inserted, the quality of the spaces they define, and the richness of the activities they are intended to support."



3D illustration of North Quay masterplan (section cut horizontally through at first floor level)

The North Quay Masterplan aims to create a high-quality urban environment where people will choose to live, work and visit, and which is able to accommodate future growth and change within a sustainable development framework.

Central to the Masterplan is its well-considered and generous public realm strategy which will underpin the transformation of a largely vacant site into a fully functioning part of London. This transformation will not only transform the Site but also the surrounding area. Specifically, the adjacent South Poplar neighbourhood will, for the first time in decades, become much better connected and integrated with the economic and cultural and social infrastructure of Canary Wharf.

Neighbours will be able to share in, and contribute to, the benefits brought by the ongoing investment in the area.

The Masterplan is embodied in the Parameter Plans (NQ.PA.03.A) which enshrine the fundamental principles required to guide development, while preserving an appropriate degree of flexibility for the design of individual buildings and the balance of uses they will house.

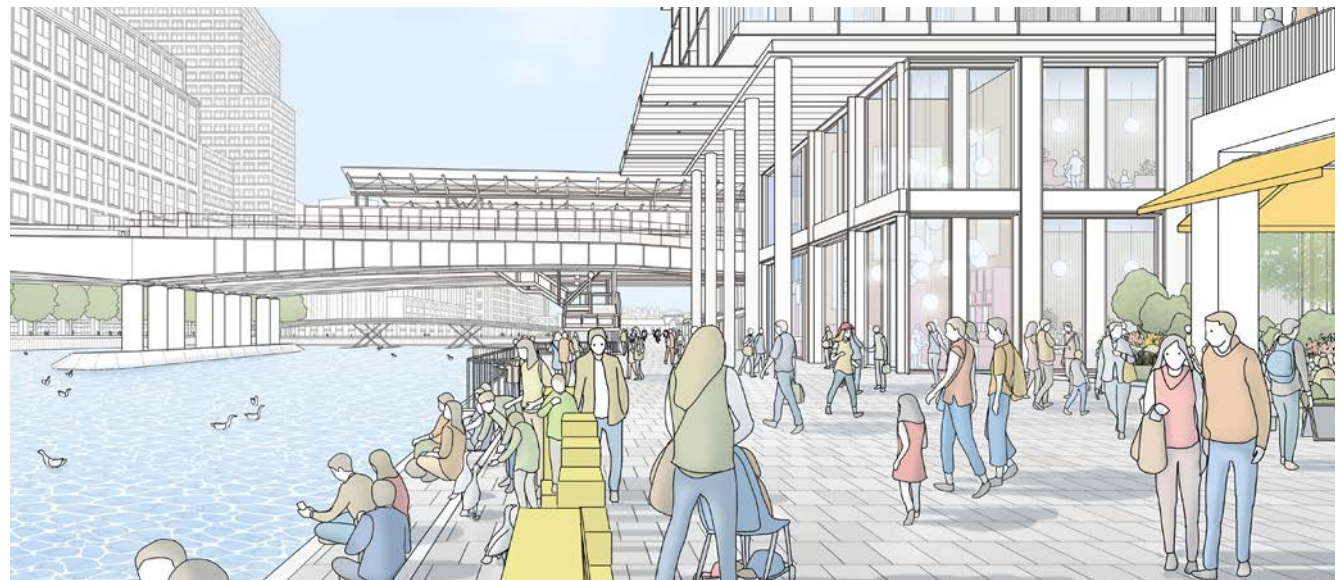
Similarly, the Design Guidelines (NQ.PA.04) provide information about how to work within the framework of the Masterplan, but are intentionally not overly prescriptive in order

to leave room for other teams of designers to respond to each Development Plot individually, and in response to the rest of the development as it stands at that particular time.

All of the decisions described in the previous chapters have gone towards the evolution of the Indicative Scheme, which has in turn informed the Parameter Plans, Development Specification and Design Guidelines. The Masterplan provides the parameters for a high-density, employment led mixed-use urban development.

The following chapters describe how the elements of the Masterplan work together to achieve the fundamental design principles which underpin the project framework and demonstrates one way this framework could be built out through the description of the Indicative Scheme.

Where to start? The first thought is to repair what's broken, the second is to maintain what's good and the third is to observe the guiding principles and contribute with the new.

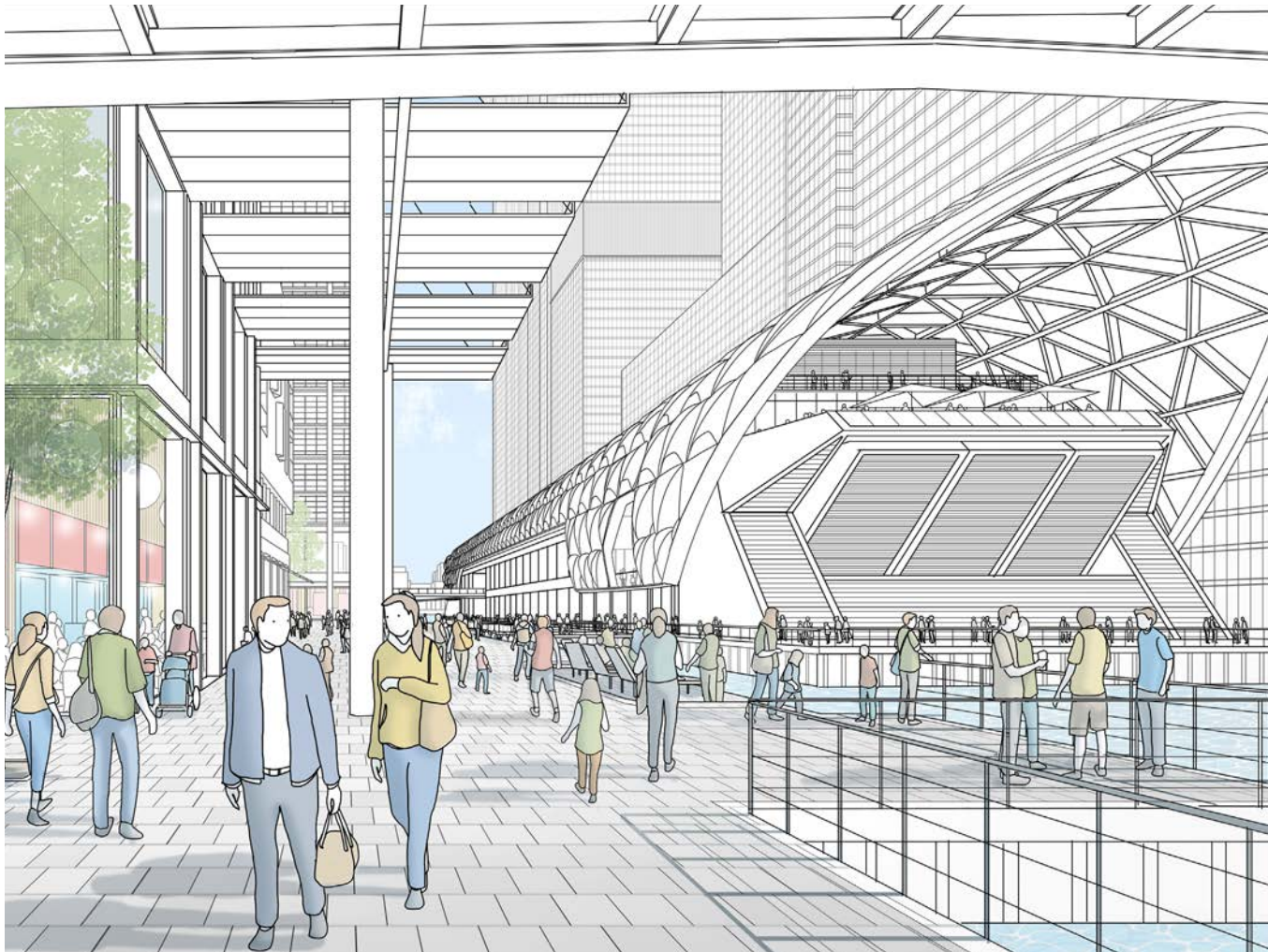


Indicative view along the Quayside towards West India Quay



## 7.2 LAYOUT

Where to start? The first thought is to repair what's broken, the second is to maintain what's good and the third is to observe the guiding principles and contribute with the new.



View from West India Quay - repairing broken connections with the existing

This chapter describes the strategic decisions that were taken in order to reach the physical framework for the Masterplan. It shows how the layout of the Masterplan evolved from the early resolution of a site wide framework, to the detailed definition of Development Zones and the eventual indicative design of the Development Plots within each zone.

Pedestrian legibility and simple way finding are at the heart of the Masterplan layout with the clear objective of creating safe and accessible environments where the fear of crime does not undermine quality of life and community cohesion.

### 7.2.1 MASTERPLAN 'LAYOUT' CONTROLLED THROUGH PARAMETER PLANS

The Parameter Plans come together to form the development controls which influence the Masterplan. To do this, they work alongside the Development Specification and the Design Guidelines.

The controls in the Parameter Plans are concerned largely with the design headings of 'Layout', 'Scale' and 'Use', where the Development Specification is concerned with 'Amount' and the Design Guidelines with all aspect of design including 'Scale' and 'Appearance'.

The highlighted design development described previously in this document, led to the final layout of the Indicative Scheme. This indicative layout design is fed back into the Masterplan control documents in the form of the Parameter Plans.

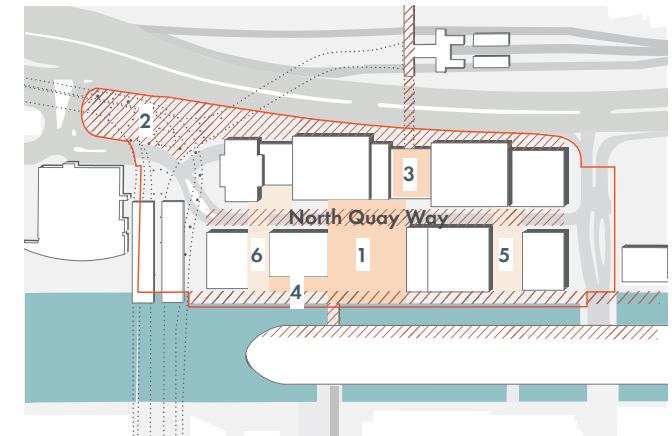
It is through these Parameter Plans that the street layout, public spaces and Zone locations are laid out and controlled. They work together to secure the creation of a legible urban grain.

### 7.2.2 MAKING CONNECTIONS - REPAIRING WHAT IS BROKEN

Having assessed movement patterns to and around the site, we propose a new network of routes that will connect West India Quay with Billingsgate west-east and South Poplar through to Canary Wharf north-south. A new street through the centre of the site will provide access for servicing and drop off and connect to the existing road network. These road connections are made at Upper Bank Street to the east and Hertsmere Road to the west. The east-west footpath along Aspen Way will be strengthened with a new cycle route.

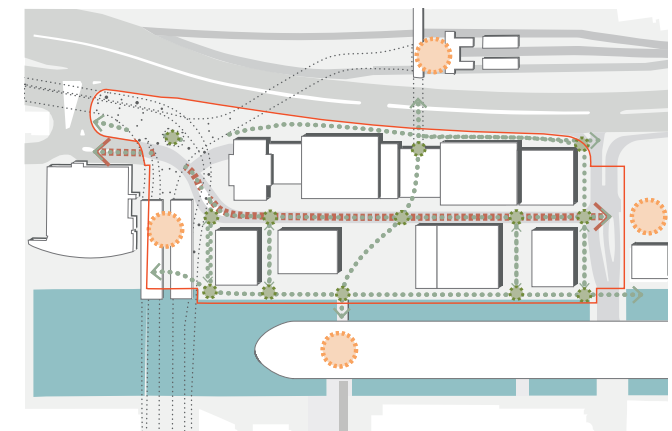
The route layout is intentionally orthogonal, allowing for a series of regular Development Zones and public spaces to emerge in between the Aspen Way and the Quayside.

The central street is located such that there are suitable sized areas for Development Zones both to the north, lining Aspen Way and to the south lining the Quayside, either side of the road. The Parameter Plan entitled 'NQMP-PP-006 Access and Circulation Routes' formalises the general arrangement of the route network between, around the edges, and in some cases through, Development Zones. The different routes are described in section 2 of the Design Guidelines.



New public spaces - a fundamental part of the new route network

- |               |                |                 |
|---------------|----------------|-----------------|
| 1 Quay Square | 3 Poplar Plaza | 5 Dock Square   |
| 2 The Delta   | 4 Quayside     | 6 Garden Square |



A new network of routes



### 7.2.3 NEW PUBLIC SPACES - CONTRIBUTING TO THE EXISTING

The Site is surrounded by a series of very different edge conditions. These include a busy road to the north, a dock to the south, an elevated railway to the west and a key access road to Canary Wharf estate to the east. Beyond these are the open spaces of West India Quay, Canary Wharf and South Poplar. These areas already exist but interconnection between them is currently limited and incongruous.

Exposing and enriching the dock side at North Quay is a key priority of the Masterplan and individual building plots have been laid out to achieve this opening up of the water's edge.

In addition to the waterfront or Quayside, we propose five further new public spaces within the Masterplan; these are Quay Square, Dock Square, Poplar Plaza, Garden Square and The Delta.

The Masterplan contains plots of high-density workspace and live/stay buildings and it is this high density that enhances the importance of the public realm between buildings.

Public space is at the heart of the Masterplan formation. The creation and protection of these spaces is the priority within the Specified

Parameters, everything else follows from this. This high density importance on public space has driven the decision to split the allocation of public realm across the site rather than providing one large open space in the centre of the Site.

This spreading of amenity brings value throughout the Masterplan and ensures everyone has access to open space close to their front door. It also ensures that there are different public spaces on offer to be enjoyed by visitors and occupiers across the Site and that these spaces benefit from surveillance and active building frontage.

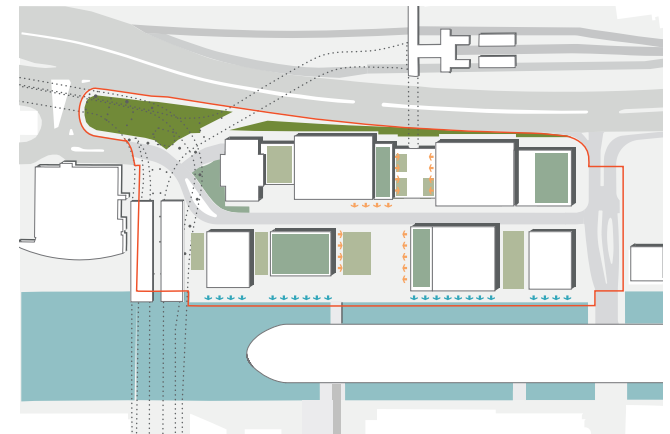
The Quayside provides south facing water-side amenity that maximises exposure to the sun all year round. The space is located along the south of the Site, adjacent to Canary Wharf and Crossrail Place.

Quay Square and Poplar Plaza constitute the heart of the Masterplan, they are intended to be places which are full of activity, attracting new and existing local residents as well as people working in the area. These spaces are located in the centre of the plan connecting Canary Wharf with Aspen Way Footbridge, making them as accessible and active as possible.

Dock Square and Garden Square are the greenest spaces in the Masterplan, located either side of Quay Square and provide important calm, quieter spaces to serve new inhabitants as well as contributing to the amenity provision for the surrounding community.

The Delta is an area of open space located under the existing elevated DLR tracks at the western end of the site. It provides connectivity between the edge of Aspen Way and Hertsmere Road and will form part of a key east-west cycle and pedestrian route.

The landscape design of these open spaces within the Indicative Scheme is explained in more detail in Chapters 8 of this document.



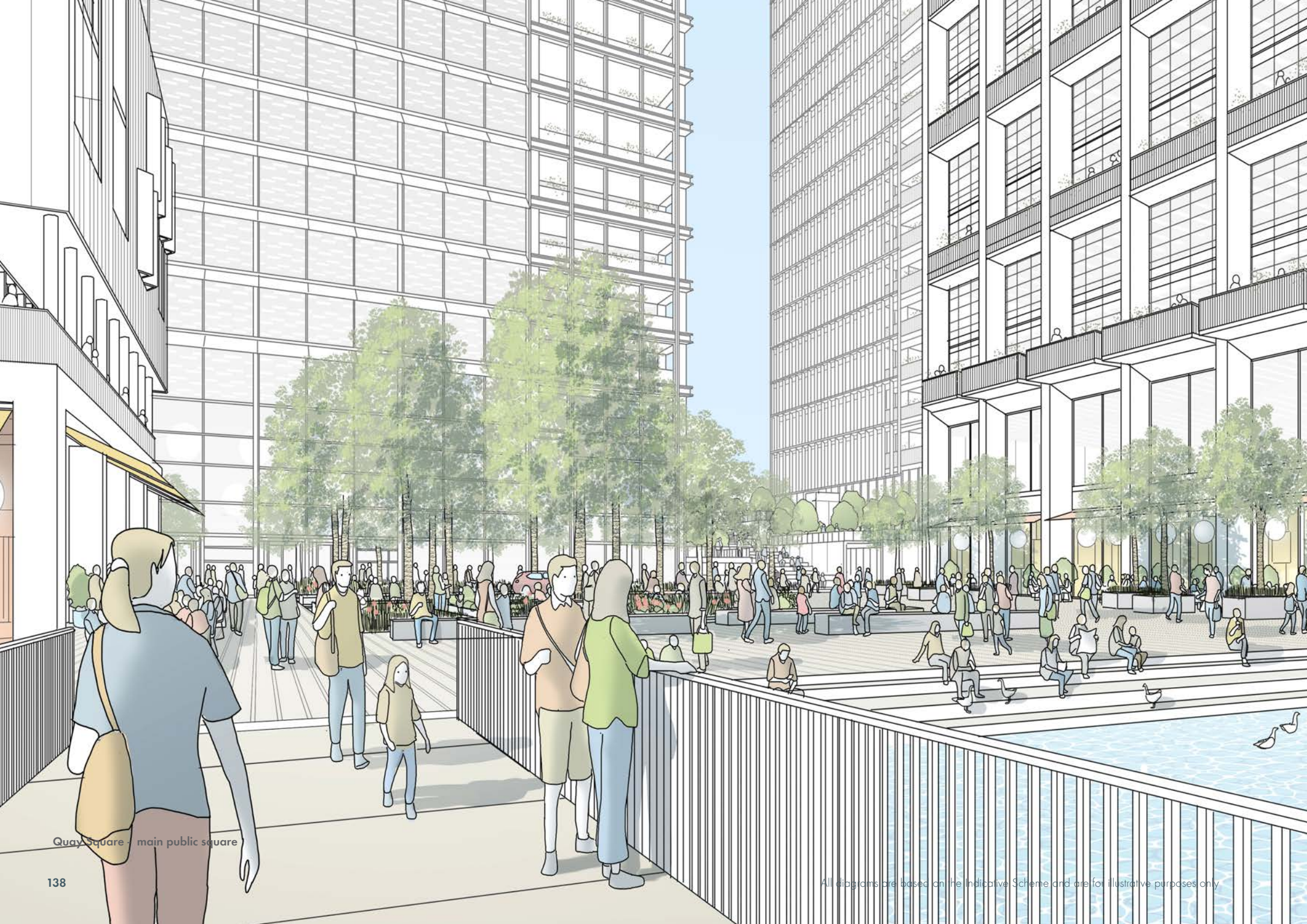
Green spaces and water amenity in the Masterplan





Landscaping - contributing to the existing DLR undercroft - The Delta





Quay Square - main public square

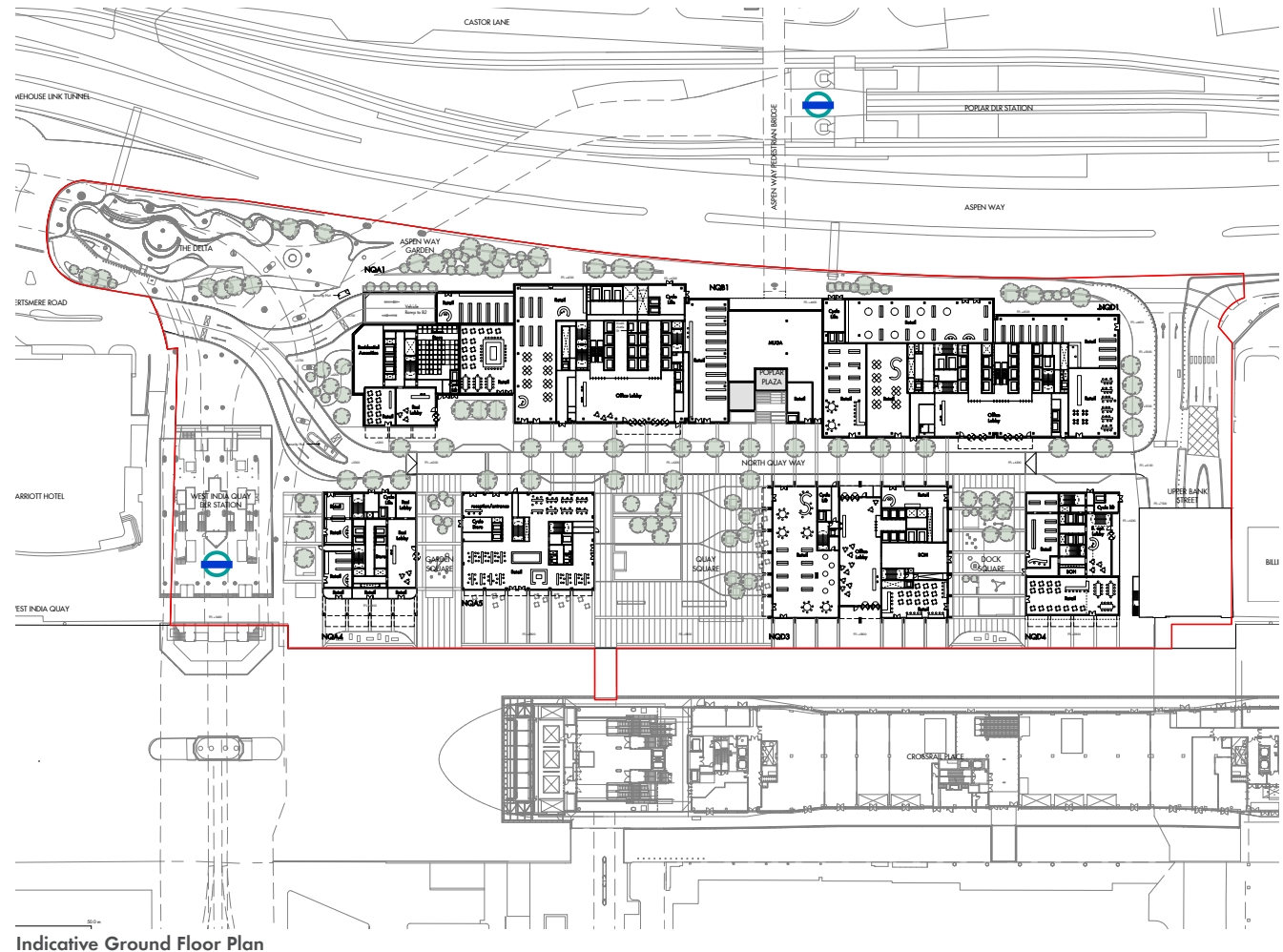


## 7.2.4 THE MASTERPLAN FRAMEWORK

The description of the layout so far has been specified to the level of detail to which the parameters constrain the Masterplan. Beyond this the eventual individual Development Plots should be allowed to flexibly vary within the framework as individual architects are appointed to build out the Masterplan.

To test and inform specified parameters, we have prepared an Indicative Scheme which complies with all of the "rules" set out in these control documents.

The structuring of the Masterplan, using a series of routes and open spaces, result in the formation of 8 Development Zones labelled NQ.A-NQ.H. These Zones are controlled by the Parameter Plan entitled 'NQMP-PP-004 Development Zones Ground Level and above'. Development Zones are not just building plots, they also cover open spaces such as Quay Square and the area under the DLR tracks.



Indicative Ground Floor Plan



## 7.2.5 LIMITS OF DEVIATION

The Masterplan is a framework to create a surrogate context for future development and not a fixed graphic pattern. There is flexibility within the layout, where routes, spaces and building boundaries are not completely fixed but can be subject to levels of deviation.

In reference to the Parameter Plans provided in Chapter 6, each Development Zone boundary is marked on the Parameter Plan entitled 'NQMP-PP-004 Development Zones Ground Level and Above'. This represents a maximum extent allowing inward deviation for the building edges from this boundary.

When read alongside the Parameter Plan entitled 'NQMP-PP-009 Development Plots Maximum Heights, Widths and Lengths' this establishes the maximum developable envelope for the Masterplan.

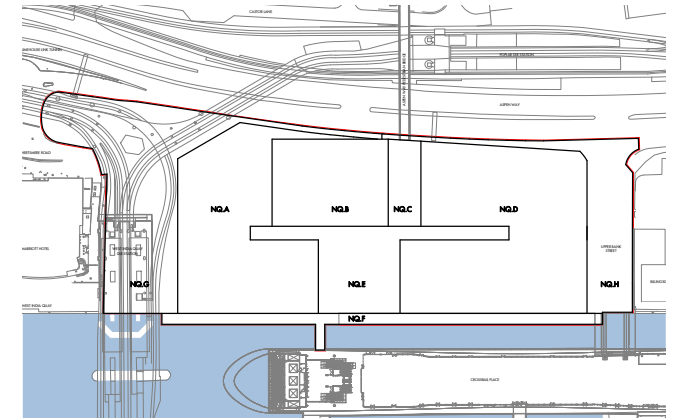
Parameter Plan 'NQMP-PP-006 Access and Circulation routes' establishes limits of deviation for the positioning of routes between building structures and therefore allows flexibility in the positioning of building edges and the size of adjacent plots, limited by the overall maximum parameter envelope.

In addition to the allowable deviation in the Parameter Plans, the Design Guidelines stipulate minimum distances between buildings, for example, two residential buildings must be a minimum of 18m apart from each other.

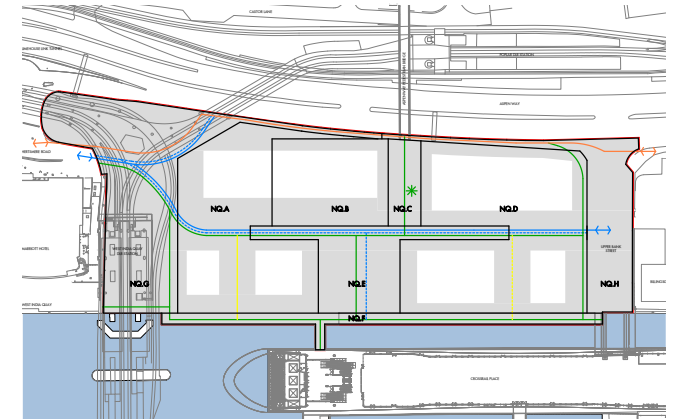
The Parameter Plans indicate the desired location for every route in the Masterplan and the Design Guidelines set key minimum dimensions, and only those that are critical to safeguard the fundamental relationships that exist between Development Zones in the creation of a good quality public realm.

The limits of deviation on the routes avoid an over prescriptive framework and instead encourage creativity. They allow the Masterplan to accommodate the reality of the present and, with a deviation in building footprint, on all sides in some cases, cater for a variety of building sizes as and when they are required.

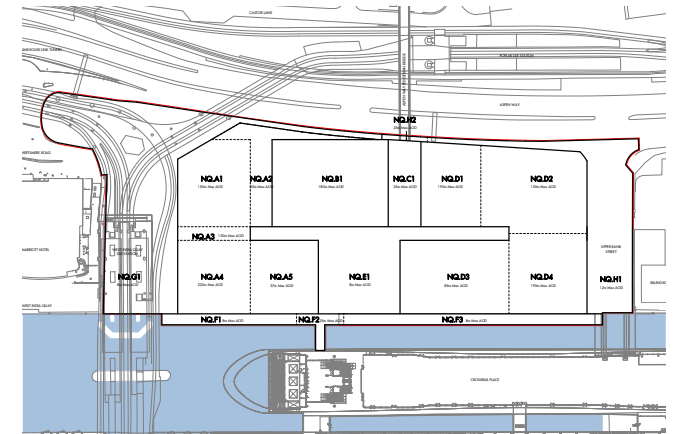
The Parameter Plans and Design Guidelines determine that routes and views are set out in a linear fashion. Each of these plots front onto a public space and as such the future buildings have a responsibility to the creation and enclosure of that space or route.



**NQMP-PP-004 - Development Zones Ground Level and Above**



**NQMP-PP-006 - Access and Circulation Routes**



**NQMP-PP-009 - Development Plots Maximum Heights, Widths and Lengths**







## 7.2.6 REGULAR ZONES MAKE GOOD PLOTS

Development Zone NQ.A is split into five Development Plots, NQ.A1 through to NQ.A5. These plots hold the edges of the Site on the west side, adjacent to West India Quay DLR station and The Delta. North Quay Way runs through NQ.A3, which may be built over by a building structure. NQ.A4 and NQ.A5 provide important frontage on to the Quayside.

Development Zone NQ.B has a single Development Plot NQ.B1 which may be one or more buildings. This provides a key stretch of frontage on to North Quay Way and forms much of the north side of Quay Square. It has a further long frontage onto Aspen Way.

Development Zone C has a single Development Plot NQ.C1. This is to provide the podium, open space, lifts and stairs that constitute Poplar Plaza and the upper level connection of Aspen Way Footbridge into the Masterplan. It also provides space for an indoor Multi-Use Games Area (MUGA) in the Indicative Scheme located within the podium below Poplar Plaza, accessed from North Quay Way with space for a small retail outlet.

Development Zone D is split into four Development Plots NQ.D1 through to NQ.D4 which are located on the east side of the Masterplan. North Quay Way runs through NQ.D4, which may be built over by a building structure. NQ.D3 and NQ.D4 provide important frontage onto the Quayside.



Long stretches of active frontage along North Quay Way

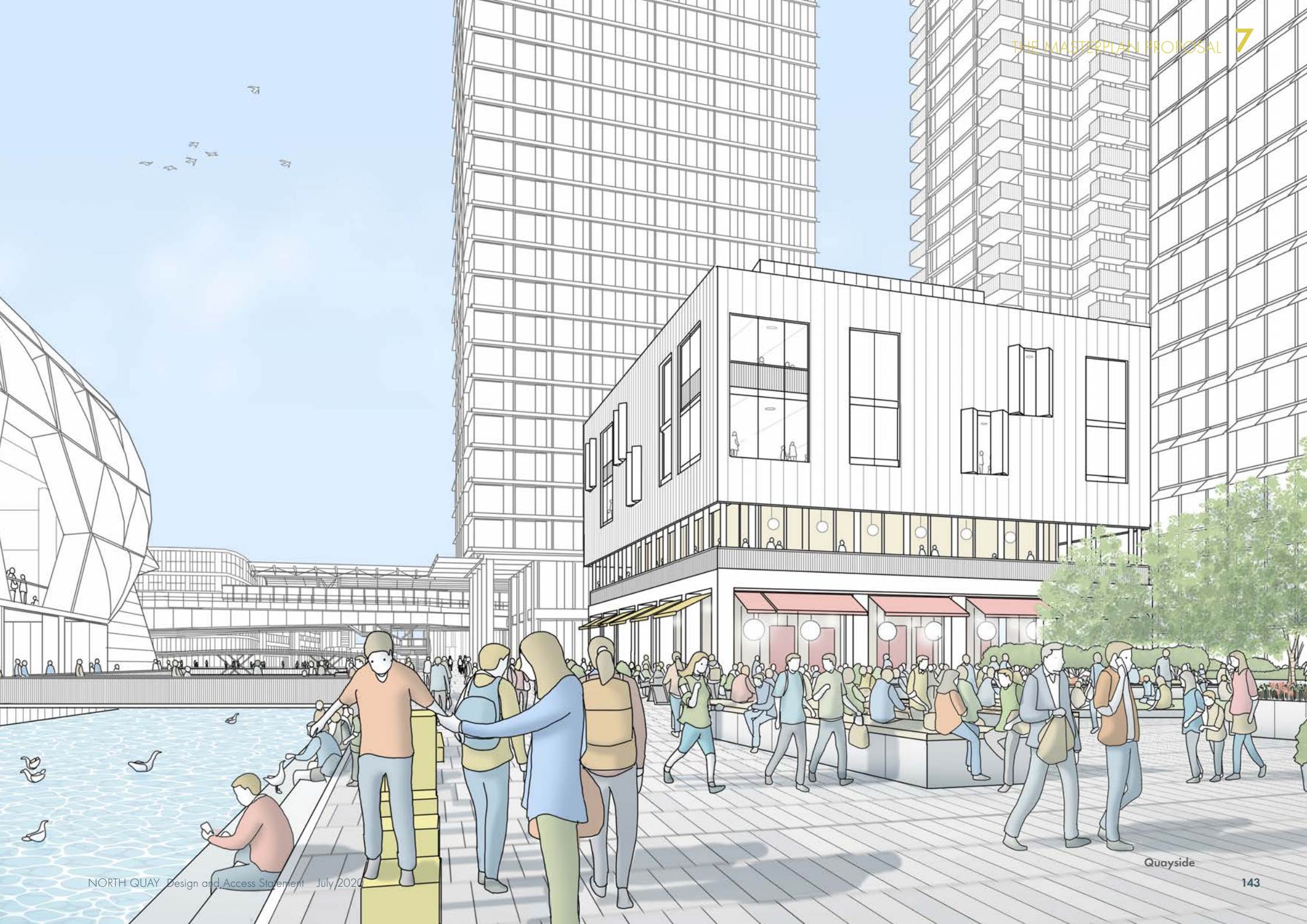


Active podium of open space on Poplar Plaza



Quay Square - one of the main vibrant public squares









Pedestrian route to North Quay from Poplar via Poplar Plaza





### 7.2.7 ACTIVE FRONTAGES

The ground floor use of the buildings around a space can make a big difference to the way that space will be used. Active frontage can be achieved by ensuring the ground floor use has a public rather than a private character. It is affected by the positioning of service access into buildings at ground floor and the transparency of the building facade as it fronts the space before it.

An active frontage can bring vibrancy to a place but also provide passive surveillance to help make places safer to the public and as such should be considered in the context of both the day and night. Active frontage in the Indicative Scheme follows the locations set out in the Parameter Plan by lining the key urban spaces and routes.

### 7.2.8 ACCESS TO BUILDINGS

Access to all buildings in the Indicative Scheme is off a street or well-defined pedestrian route. The Design Guidelines make suggestions for the locations of major entrances in the Masterplan.

The Indicative Scheme aims to keep main entrances lining main roads for ease of drop off, rather than pedestrianised spaces. Points of access also encourage people movement through different times of the day and night. By locating them with this in mind, the aim is to create a safe and animated public realm.

For further information on the proximity of entrances to drop off, set down and arrival points to ensure access and inclusivity for all visitors refer to the Access Statement in Appendix 2.



Animated and active public realm



Well defined, safe and inclusive routes



## 7.3 SCALE

Because no new buildings have been designed in detail yet, we have established a set of rules and guidelines which would guide issues of height and massing for future designs.

North Quay needs a density commensurate with its strategic importance which is at the same time sustainable and an attractive place to work and live.

In addition to overall density, an appropriate size and scale for the public spaces are vital to achieving a successful city.

North Quay provides a significant opportunity to create a new place with its own identity whilst at the same time complementing Canary Wharf and integrating further high-rise development into the Isle of Dogs skyline.

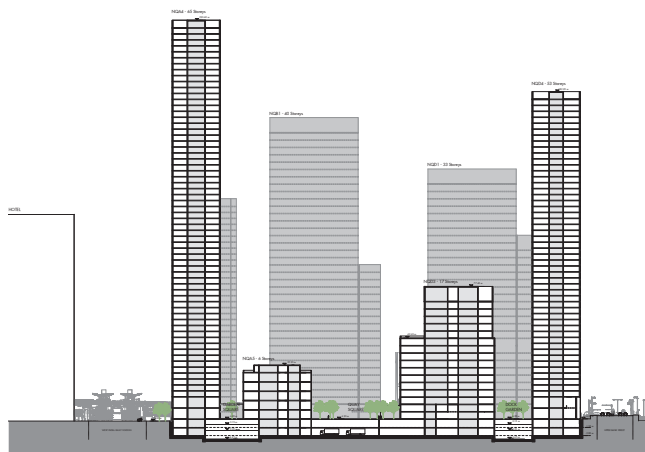
### 7.3.1 MAXIMUM BUILDING ZONES AND PUBLIC REALM

Maximum footprints and the maximum heights of buildings are defined within the Masterplan, however, there are no minimum heights stipulated.

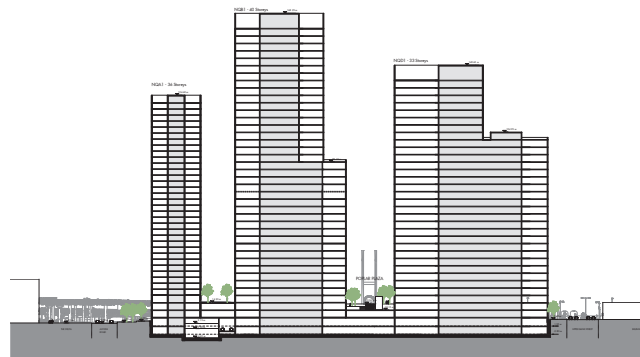
By defining the location and maximum extents of each Development Zone footprint, the overall urban composition and massing is assured and the minimum extent of open space will be defined as a requirement which guarantees space between buildings. This process of fixing maximum building Zones enshrines the larger open spaces of the public realm.

These maximum building Zones and heights have evolved in response to a number of factors, namely:

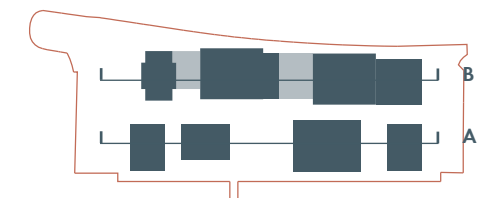
- Sympathetic response to surrounding context
- Belief that the Site is capable of sustaining high density workspace and live/stay space which can be achieved by the use of tall buildings
- Desire for a permeable urban grain at street level which allows easy wayfinding and views out to spaces beyond
- Aspiration to create a mix of public spaces between buildings of varied sizes from the London garden square through to an expansive Quayside
- The ability to deliver a good mix of buildings sizes and typologies that have in-built flexibility.
- Making the most of a well-connected Site with appreciable dockside amenity.



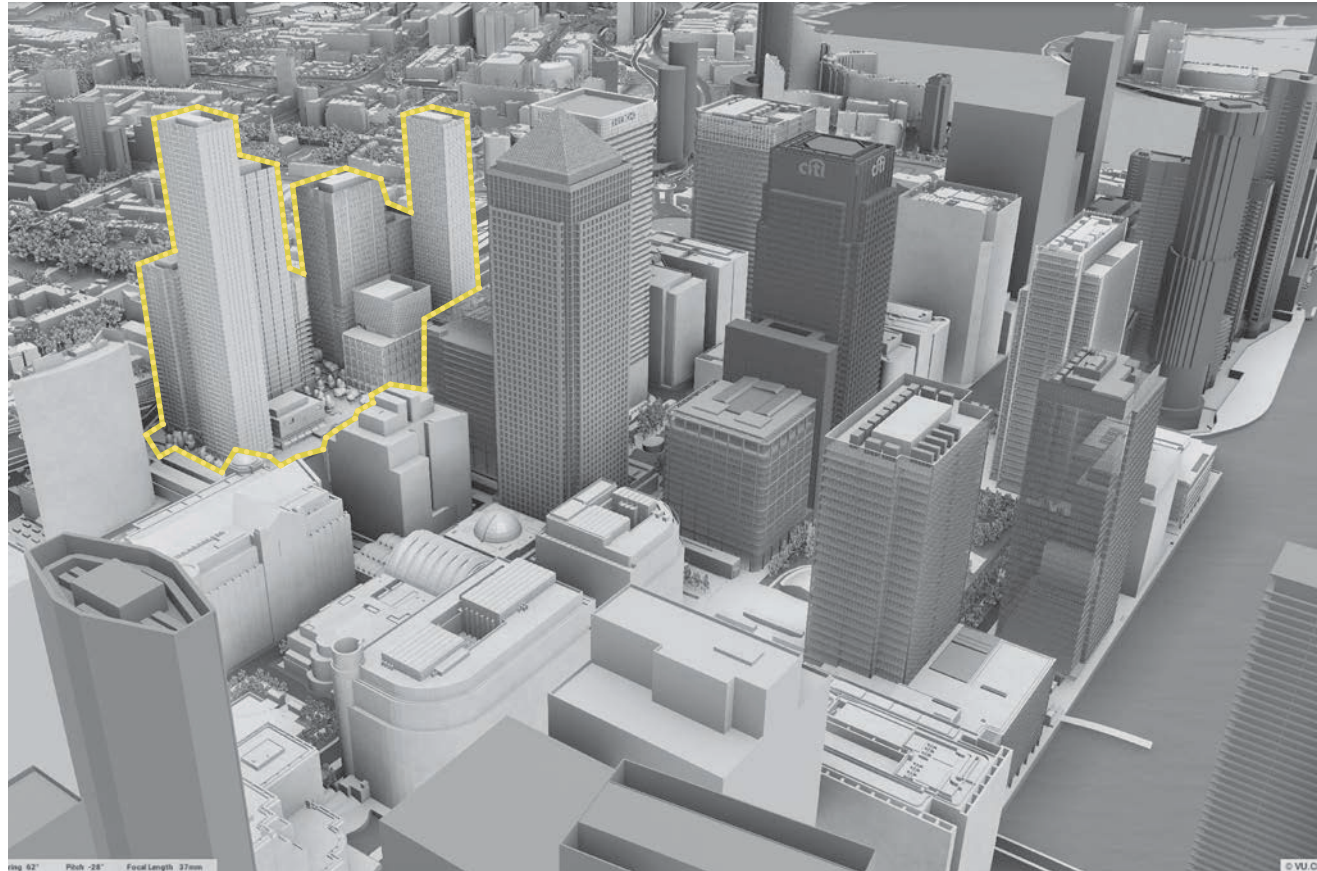
Section A through the southern part of the Site



Section B through the northern part of the Site



Section location plan

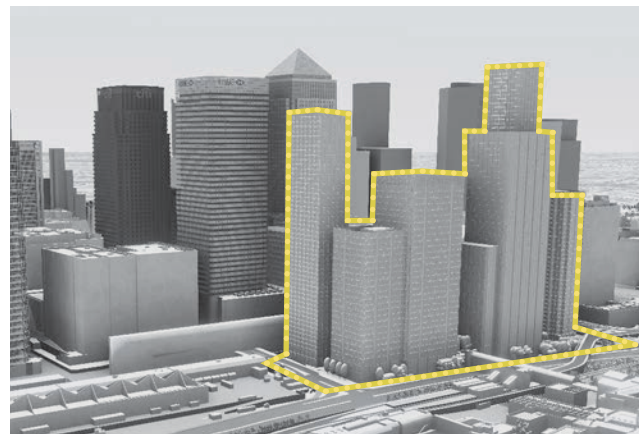
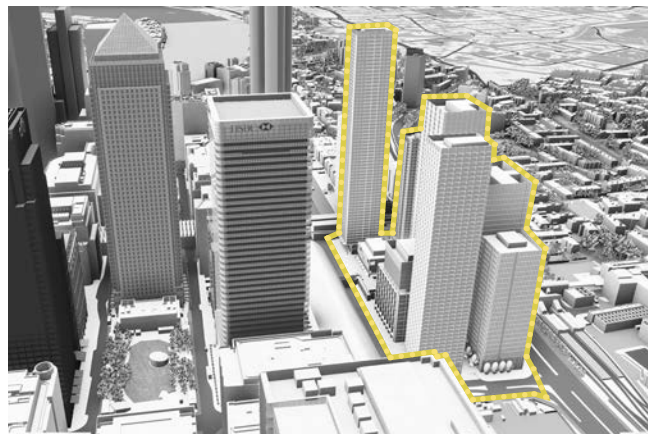


### 7.3.2 TALL BUILDINGS

In order to optimise the potential of the Site, tall buildings are the predominant urban form in the Masterplan. Current planning policy highlights North Quay as a suitable area for tall buildings and the adjacent Canary Wharf sets the precedent for tall buildings. The extant 2007 planning consent for North Quay also sets a precedent for tall buildings on the Site.

London's skyline and that of Canary Wharf and its local surroundings has changed dramatically since 2007 and policy on tall buildings can be found in the adopted LBTH Local Plan 2031 policy D.DH6.

Canary Wharf is allocated as a Central Activity Zone, Tall Building Zone, Opportunity Area and emerging Metropolitan Centre. The building heights and density proposed for North Quay reflect these designations and planning policies relevant to the Site.



- Existing buildings
- North Quay

Tall buildings of North Quay within the Canary Wharf and Wood Wharf Estate



## How are the tall buildings laid out and why?

The flight protection zone for London City Airport places limitation on the height of building structures on the Site to a maximum of 225 metres AOD on the west side, reducing to approximately 190 metres AOD adjacent to Upper Bank Street.

Though similar restrictions apply over Canary Wharf, a varied and articulated urban form is provided across the estate, established through the 1993 Canary Wharf masterplan by SOM.

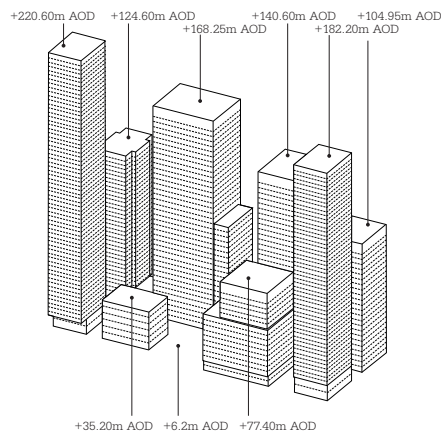
A similar varied and articulated skyline is proposed for North Quay through a mixture of building heights including tall, medium tall and lower rise. In combination with the Maximum Parameter heights, the Design Guidelines also set out a series of clear principles on how taller buildings should relate to each other, their spacing, composition and adjacent height relationships.

Plots NQ.A1 and NQ.A4 have a guideline to have a defined minimum height difference between them (Design Guidelines 4.2.3a). This is such that one of these plots is seen to be taller and the other subservient, yet it does

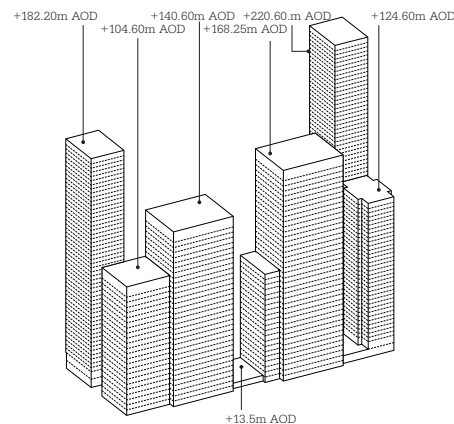
not determine which is the taller of the two buildings allowing flexibility.

Within the Indicative Scheme, a tall building is shown within Development Plot NQ.D4 with a lower commercial building shown across plots NQ.D1 and NQ.D2. There are however other possible scenarios that have been studied where this tall building could be relocated to Plot NQ.D1 and a commercial building or buildings occupy plots NQ.D2 and NQ.D4.

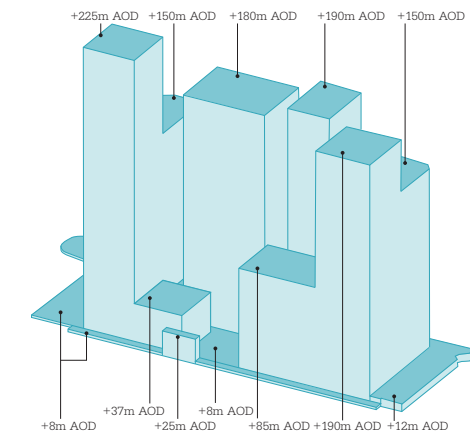
This flexibility across tall building plots NQ.A4, NQ.D1 and NQ.D4 has intentionally been built into the Masterplan to allow for a degree of controlled future change and is supported in



Indicative Scheme - view from the south



Indicative Scheme - view from the north



Jelly mould - view from the south

the Design Guidelines.

The maximum parameters in the Masterplan establish a three-dimensional building envelope within which the detailed design of buildings will be developed. The 'jelly mould' is a term used to describe the shape that would emerge from a mould made to the outer most, maximum dimensions for every Development Zone on the Site.

We have seen how adjacent Development Zones within the Masterplan are related to one another parametrically and if one Zone were built out to its maximum dimensions, neighbouring Zones would be constrained

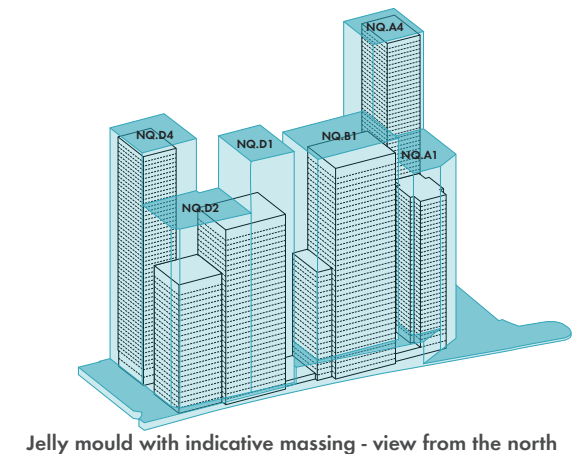
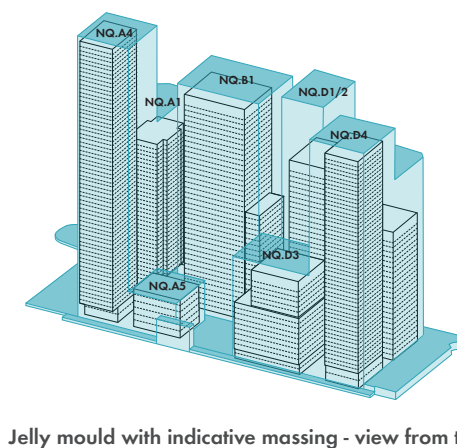
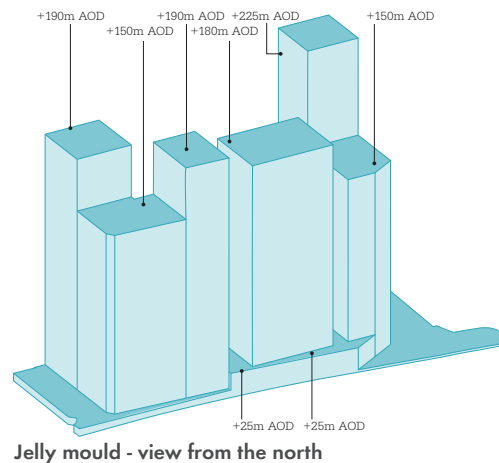
accordingly. All plots must have a minimum height difference to their adjacent plot, other than NQ.A1 and NQ.A4 if they are live/stay, as described earlier. Refer to the Design Guidelines for further clarification.

As a result of these constraints, the maximum jelly mould represents a shape that could never be built out in its full volume.

The construction phasing of the Masterplan cannot, and should not, be decided now. Instead, each phase should come forward organically, in response to real need as it emerges on the Site. As we do not know the order the Development Zones will be built in,

we cannot predict the constraints that will be placed on each Development Plot, as such, each building could eventually be forced to sit anywhere within its jelly mould.

As such, it is this moulded shape, along with the Indicative Scheme that has been taken as the worst-case scenario for testing sunlight and daylight, views, overshadowing, townscape impact and all other issues that would be affected by the scale of this development. The results of the assessment on the North Quay jelly mould can be found in the Environmental Statement submitted in support of this OPA.

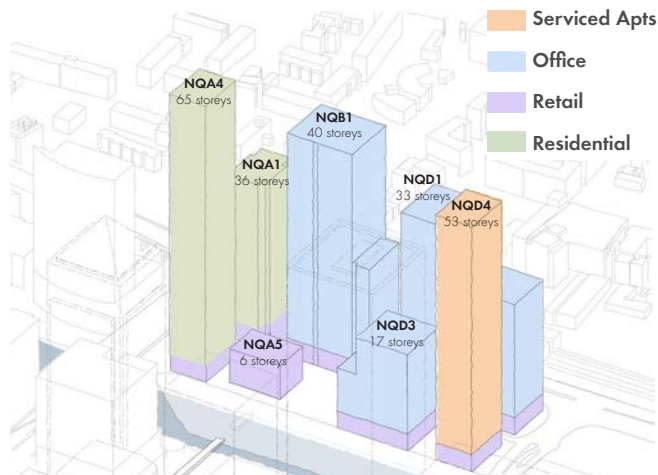




## 7.4 USE



A balance of residential and commercial uses



Indicative uses massing diagram

### 7.4.1 INDICATIVE USES

The type of uses proposed in the Masterplan respond to the Site's strategic location adjacent to the financial centre of Canary Wharf, with its excellent transport links to the rest of London, and to the growing demand to make this part of the Isle of Dogs a fully-functioning, 'liveable' part of London.

It also looks to reflect the fact that this site is seen as integral to the Central Activity Zone.

Permitted Land Uses are set out in the Development Specification with minimum and maximum floorspace figures where relevant. In order to create an active and vibrant place, Outline Planning Permission for a wider range of other uses that fall outside of the Use Classes Order is also sought in the Development Specification. The locations where those Permitted Land Uses may be placed on the Site are controlled by the Parameter Plans..

The Indicative Scheme shows a balance of uses and floor areas that have been deemed commercially viable in the context of today. The Masterplan framework, however, should 'future-proof' the area as it is built out over the next decade or more, anticipating market changes. At upper floor levels, the Indicative Scheme includes residential use within Plots NQ.A1 and NQ.A4, serviced apartments

within Plot NQ.D4 and offices within Plots NQ.B1, NQ.D1 and NQ.D3. Retail uses are shown across the ground and upper ground levels of all Plots and also within Plot NQ.A5 at upper levels.

The wide spectrum of uses applied for in the OPA – including a range of office specifications, dwelling types and tenures, student housing, hotel rooms, serviced apartments, shops, restaurants, cafes and community spaces represent a rich palette of future activity in an area which can respond effectively to inevitable changes in future demand.

The Masterplan is structured with features to provide flexibility in the way uses are distributed during the realisation of each phase. These features are discussed in the following pages and applied through the Parameter Plans and Development Specification which will ultimately control the development.





Poplar DLR

Aspen Way

NQA1

NQB1

NQD1

Marriot

West India  
Quay DLR

Upper Bank  
Street

Billingsgate  
(simplified impression of a  
future development)

NQA4

NQA5

NQD3

NQD4

Crossrail  
Place

- Served Apts
- Office
- Retail
- Residential

Indicative Uses - Ground Floor Plan





North Quay Way - main east - west through the Site



### Adaptability to avoid obsolescence

The use of 'predominant uses', 'swing blocks' and 'frontage' concepts allow flexibility for the Masterplan to respond to its context as it grows. It ensures that the Masterplan is not restrictive, or closed to potential opportunities in the future, whilst preserving an aspiration for a varied mix of character areas, necessary for a healthy piece of city. Thus helping to ensure that the development does not stall due to changing external economic conditions.

### Development Zones with 'Predominant Uses'

One of the ways the Masterplan will define a set of specific urban characters while at the same time preserving flexibility for the future is through the concept of 'predominant uses'.

In cities, blocks and individual buildings frequently support more than one use. Encouraging a mix of uses in blocks and buildings allows the right type of use to occupy the right type of space and can bring a greater number of uses into proximity of one another.

In this type of Development Zone at least 51% of the floorspace created within the Zone must be given over to the predominant use (or uses). The other 49% of the floorspace can be assigned to any of the other 'Permitted Uses' (found listed in the Development Specification).

### Swing blocks for flexibility

Development Zone NQ.A and plots NQ.D1 and D4, within the Masterplan act as 'swing blocks' with 'any permitted uses'. It means that (apart from any constraints imposed at ground, mezzanine and first floor levels) the block is not defined as being dominated by any one particular use. Instead it can be developed to house any use or mix of uses from the list of permitted uses given in the Parameter Plans. This allows for creative growth and enables unforeseen opportunities, across large blocks at the heart of the scheme, without closing them down.

### Defining uses for building frontages to make active and safe places

In addition to prescribing the predominant use of the upper floors of a Development Zone, Parameter Plan NPMP-PP-010 also aims to prescribe the ground floor, mezzanine and first floor 'frontages' in key locations. This is to ensure a healthy, focused, active frontage along the important routes and open spaces within the scheme. The successful animation of these places relies on interactive and engaging frontage from surrounding buildings.

### The mix of uses in the Indicative Scheme

With the Quayside water setting to the south and London city views to the west, the sub-zone Plots NQ.A1 and NQ.A4 within Development Zone NQ.A are shown as residential use above mezzanine level in the Indicative Scheme. Plot NQ.A5 is predominantly retail at all levels and acts as a key lower height building animating the public realm.

Development Zones NQ.B and NQ.D form most of the northern and the eastern side of the Masterplan with predominant office uses allocated at upper floor levels. Sub-zone Plot NQ.D4 complements this with Serviced Apartments use at upper levels and book ending the eastern end of the Quayside.

The Indicative Scheme has substantially more ground floor retail space than the minimum amount stipulated on the Development Specification. It is likely that the types of retail units will vary across the Masterplan and include a mix of A-class uses alongside some leisure D-class uses. There is also provision for a MUGA/internal sports hall illustrated in the stepped volume under Poplar Plaza.



## Basement Strategy

A shared basement approach has been adopted for the proposed Masterplan, accessed via a shared ramp from under the Delta near Hertsmere Road. The scale and extent of the Indicative Scheme has been informed by the operational requirements of the proposed buildings, the strategy for the management and servicing of the Site and the desire to minimise the impact of these requirements on the public realm.

The Indicative Scheme shared basement is typically 9m deep and is divided into 2 primary levels with some limited additional mezzanine space for cycle storage. In some areas, the B2 level is double height to accommodate the space required for refuse vehicles to lift/operate compactors.

The basement perimeter is defined by a continuous piled wall which is required to limit the ingress of ground water into the Site both during and after construction. A portion of the basement wall will be constructed behind the Grade I listed dock wall (banana wall). No modifications are proposed to the banana wall.

The following functions are supported by the shared basement:

- Large deliveries and collection
- Cycle storage
- Car parking
- Waste storage and removal
- Building and infrastructure plant
- Estate management and storage facilities.
- Retail storage/accommodation

### Car Parking

A limited number of car drop off spaces will be provided at street level across the Site. Accessible parking spaces will be provided within the shared basement at B2 level. The shared vehicular ramp provides access from The Delta near Hertsmere Road.

### Truck Access

Truck access will be provided within the basement however their movements will be limited to a specific loading and goods handling area. The loading area enables functions such as waste collection, plant replacement and large deliveries to be managed discreetly below ground thus maximising open space and amenity at street level as well as reducing visual clutter. The loading bay will be

supervised and deliveries managed to avoid congestion. Further information is available in the Transport Assessment which supports this OPA.

## Cycle Storage

Provision is made within the basement for secure cycle storage facilities. Storage facilities for commercial and residential buildings will be within their demise (or directly adjacent) with access and layouts determined at detailed design stage. For retail staff, storage has been identified in common basement areas as these facilities are shared between buildings. Where possible cycle storage has been located on the B1 level and will use a combination of single and double storey stacking systems. Access from street level is anticipated to be via secure ramped access routes, cycle lifts or stairs with gutter rails located within adjacent buildings.

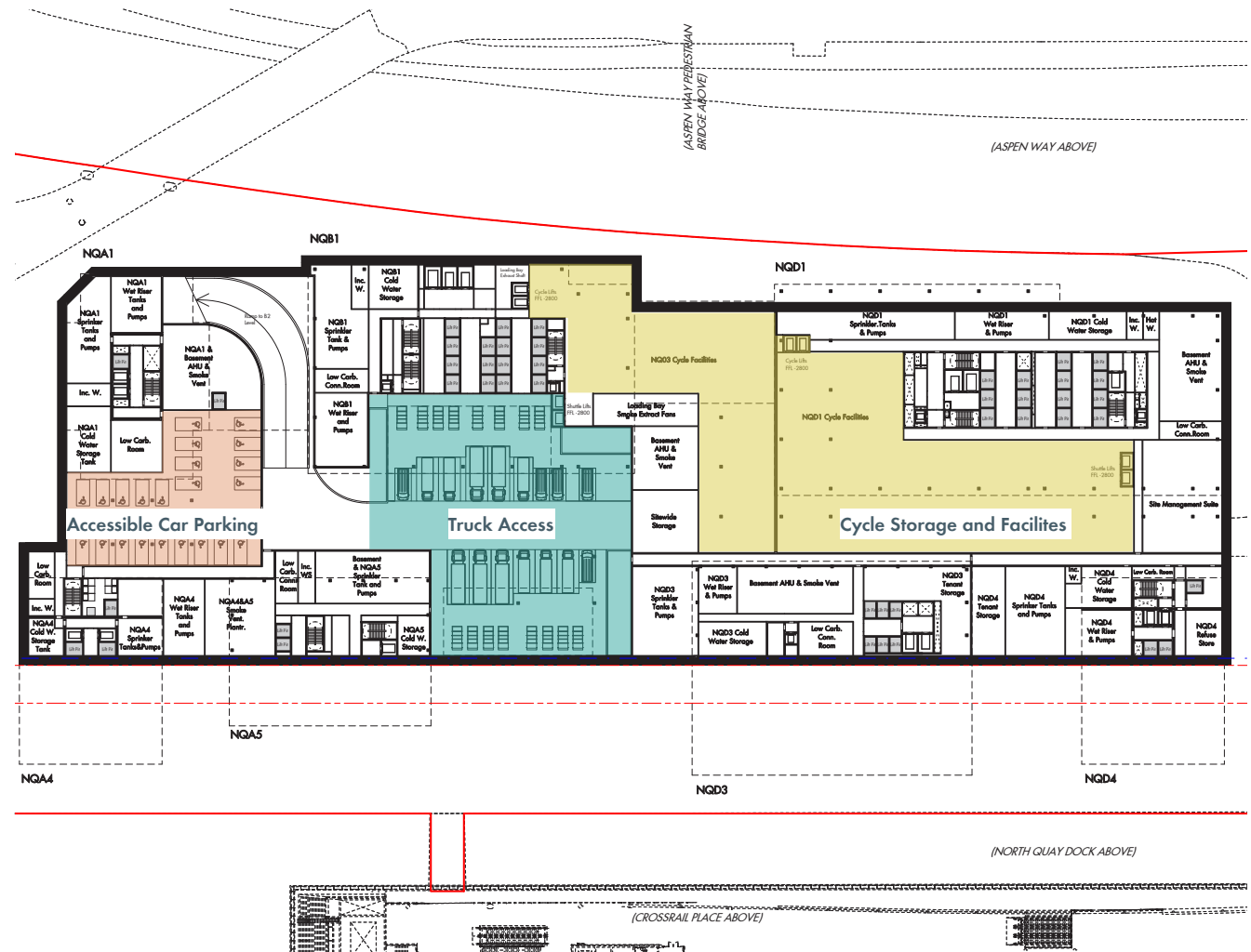


Accessible cycle storage facilities

## Basement Plant Accommodation

It is anticipated that a large proportion of the plant equipment required by both the commercial and live/stay buildings will be accommodated within the basement. This strategy enables a greater proportion of the building ground floors to be given over to active uses and more roof areas to be given over to amenity space or green roofs.

Building specific plant in the Indicative Scheme is located where possible immediately below the building footprint with infrastructure plant serving the basement located within common areas. This will include air ventilation plant for the car park, loading and plant areas. Air intake and exhaust points serving this plant will be integrated into the landscape or incorporated into the building facade designs.



### Indicative Scheme Basement Plan



## 7.5 AMOUNT

We have described the mix of uses proposed for the Masterplan and how these could change over time. There is a lot of in built flexibility but it is not endless...The Development Specification puts a cap on use allocation and the amount that can be built of each type of use to ensure balanced delivery.



Illustrative view looking west along North Quay Way

### Development Specification and the total amount in the Masterplan

This OPA seeks approval for a total maximum amount of floor area which can be constructed when the Masterplan is completed.

The Development Specification has defined this maximum area as the total that is generated by measuring the Indicative Scheme. The total floorspace applied for is 355,000 sqm GIA.

Whilst it gives an overall area, the Development Specification does not set an exact figure for any one type of use class. Instead, it defines a minimum and maximum range for each use within the Masterplan, for example, the minimum amount of business (B1) floor space is 150,000 sqm whilst the maximum is 240,000 sqm.

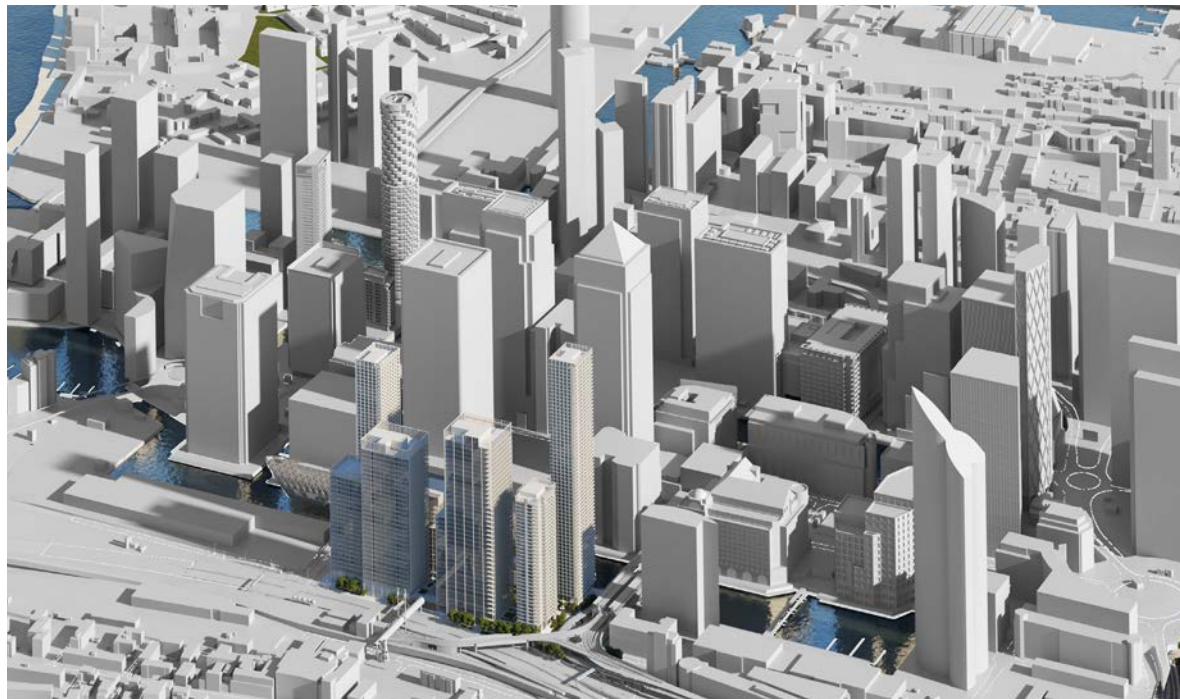
If the maximum allowable amount of each use are added together, they equal more than the total amount of floor area applied for. As such the maximum amount of every use applied for will never be fully built out.

We have already discussed the notion of the 'predominant use' of a Development Zone, where 51% of the floor area must be dedicated to the predominant use. This rule will allow 49% of the floor area to be dedicated to any permitted use or mix of uses.

Permitted uses can only be allocated to the remaining 49% of floorspace to the extent that, when the Masterplan is complete, the minimum amount of each use as listed in the Development Specification must be included on the Site and the maximum amount of any one use must not be exceeded.

The benefit of using this method is that it gives confidence that an acceptable amount

of each use will be provided whilst allowing for flexibility, ensuring that the floorspace allocation can be rebalanced as the Masterplan proceeds, when inevitable, yet unforeseen, opportunities arise. An 'Equalisation Statement' will be required with each Reserved Matters application to show how the uses will be rebalanced in the remaining Masterplan.



Visualisation of Indicative scheme

### Amount in the Indicative Scheme

The Indicative Scheme demonstrates one way of building out the OPP.

As the maximum total floorspace applied for in the Development Specification is 355,000 sqm GIA, the Indicative Scheme is a Masterplan drawn with that amount of floorspace to demonstrate clearly how that might look on the Site if fully built out to that maximum GIA.

It also represents one way in which the flexible mix of Permitted Land Uses applied for in the Development Specification could be developed on the Site and in compliance with the Parameter Plans. The Masterplan proposes 7 buildings across the proposed Development Plots, with shared basement levels that above and below ground total the maximum of 355,000 sqm GIA. This total area is split, broadly speaking, into 56% office, 25% residential, 14% serviced apartments and 5% retail space. In this regard it demonstrates an employment led mixed use Masterplan appropriate to this location.



## 7.6 APPEARANCE

The Masterplan for North Quay has been conceived as a group of buildings, delivered as 'the work of many hands'; an inclusive process that promotes the use of a number of architects to create a degree of diversity and variety across the scheme.

The Masterplan is consciously European in its philosophy, building on what is best about London, its streets, architecture and public squares and spaces. The material treatment of buildings across the Site should correspond to the hierarchy of proposed streets, spaces and character areas. Therefore, facades along key public routes will be treated in a different manner, scale and detail to frontages to more intimate garden squares. The architectural proposals should be contemporary in character, using high quality materials and be clear in their resolution of base, middle and top.

Particular consideration should be given to the perceived depth of façade, the casting of shadows and the investment of detail to ground level facades. The lower sections of all buildings define new places and streets in the Masterplan and should take reference from the dockside setting and its historic origins.

Buildings need to respect their particular context and setting with materials and detailing carefully considered to ensure an appropriate response. For example if the adjacent public realm would benefit from reflected sunlight then this could be factored into the light reflectance values of façade

**What would it all look like? The bulk and massing have been defined but, architecturally, we don't yet know. The Masterplan does not (and should not) attempt to define or overly constrain future detailed designs.**

materials – thereby providing an enhancement to the local environment. At the same time the potential for problematic glare to the space or a facing building would need to be taken into account.

Parameter Plan NQMP-PP-010 sets out the location of active frontage at lower levels, reducing the amount of service entrances on certain frontages where an active, busy street is the goal.

In terms of the form and expression of the buildings, the general philosophy is that a degree of individuality will be welcome as long as it fits within the defined parameters and adheres to the Design Guidelines. Consciously self-reverent, attention seeking excess would undermine the collective effort and should be resisted.

Design must be coherent and all encompassing, such that services pipes, cleaning equipment and other building paraphernalia should be concealed from view, unless an important part of the expression of the design concept.

The variety of building typologies, heights and uses across the Site will affect each location differently and different architectural responses will be required. Tall buildings will look over lower buildings – the rooftop view will require an organised arrangement of plant, services overruns, terraces and the like. Green roofs should be considered in the more visible locations, brown roofs may be better in the less visible (i.e. taller) buildings.

Sustainable measures should be encouraged and made visible where appropriate and in keeping with other concerns outlined above. Ensuring 'green' issues are prominent can shape public consciousness and help create a more sustainable culture. It is generally accepted that the use of responsibly sourced materials, incorporation of energy and water efficient measures, etc, are popular features for prospective tenants, visitors and permanent inhabitants. This can therefore be beneficial to the viability of the development as buildings come forward for rent, short-term use or private sale.

### Materiality

To integrate with the surrounding urban context, the masterplan and Design Guidelines propose that future buildings draw upon the local character and history of the area to suggest a range of materials appropriate to the Proposed Development. As demonstrated in the image, there is a common theme between historic and proposed developments.

The masterplan proposes a continuation of this theme, using high quality materials within this range of colours to contribute to the sense of coherence, both within the Proposed Development and the wider area.







## 7.7 THE ROUTE FROM POPLAR

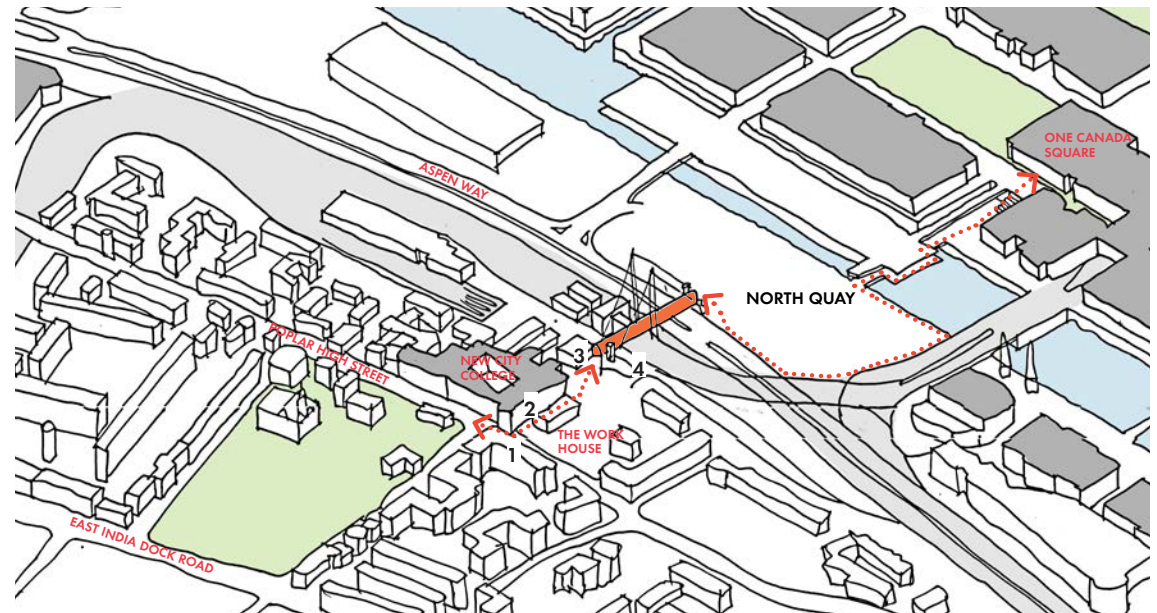
It is a joint aspiration of LBTH, the GLA, TFL and DLR to improve the connectivity between Poplar High Street and Canary Wharf. The local Plan seeks to 'Overcome barriers to movement' across Aspen Way.

### 7.7.1 ASPEN WAY FOOTBRIDGE - EXISTING CONDITION

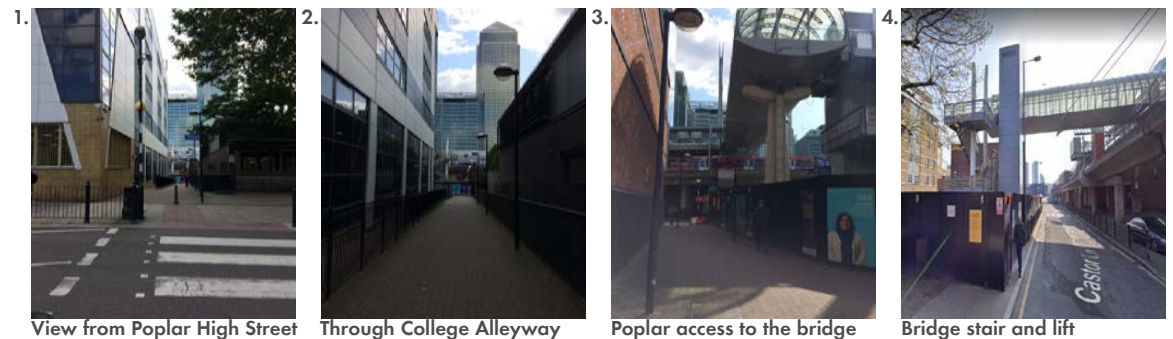
The Aspen Way Footbridge was constructed to provide access to the Poplar DLR station. The bridge spans above Aspen Way and connects Canary Wharf and Poplar. The bridge can be accessed by a set of stairs and lift from either end of the bridge and from the DLR platforms.

Poplar DLR station is an interchange station for three of the six lines on the DLR (Stratford-Canary Wharf, Bank-Woolwich Arsenal and Tower Gateway-Beckton) making it one of the busiest stations on the network in terms of services.

Based on an analysis of pedestrian flow and through comparison to other locations in London (including the Millennium Bridge, Jubilee Bridge and Westminster Bridge) the width and alignment of the current bridge structure has been assessed to be fit for



Route from Poplar High Street to One Canada Square



View from Poplar High Street

Through College Alleyway

Poplar access to the bridge

Bridge stair and lift



Existing Interior of Aspen Way Footbridge

purpose to satisfy the anticipated pedestrian footfall which could be generated by the Proposed Development and other future developments in the vicinity. This is described further in the accompanying Transport Statement (NQ.PA.10).

Whilst the bridge itself has ample capacity for future pedestrians, the legibility of the route from Poplar High Street to Canary Wharf and the ease of making the level change up and down from the bridge needs improvement. There is also a general lack of visibility to the bridge from Poplar High Street and an unclear pedestrian path connection to Canary Wharf due to its landing arrangement on the south side of Aspen Way and the inaccessibility of North Quay. The space around both sets of stairs and lifts generally lacks purpose and identity.

### 7.7.2 ASPEN WAY FOOTBRIDGE - OPPORTUNITIES

The Masterplan for North Quay provides the opportunity to significantly improve the pedestrian connection between South Poplar and Canary Wharf through reworking and improving the southern landing of the existing Aspen Way Footbridge.

A generous 20-metre-wide arrival space into North Quay will allow pedestrians to make the



transition from the Aspen Way Footbridge to the North Quay Square and onwards to Canary Wharf. The level change will be managed through the use of new stairs, lifts and dwell spaces, creating a tiered 'Poplar Plaza' that will make this connection a positive experience rather than a simple function. Poplar Plaza is described in more detail in Chapter 8 of this document.

Outside the red line of the OPA, there is a further opportunity for LBTH to coordinate, along with adjoining landowners to significantly enhance the whole route from Poplar High Street to the northern end of the Aspen Way Footbridge and make some cosmetic improvements to the bridge itself.

Local Plan Site Allocation 4.1 includes New City College, the Workhouse site and the DLR depot and requires improvements to be made to the Aspen Way Footbridge and routes to it. There is a wider opportunity to make the connection with Poplar High Street a more meaningful and legible pedestrian experience with particular emphasis on improving vertical circulation for those using wheelchairs, bikes or buggies. The Applicant has been in liaison with those adjacent landowners and their Architects to ensure that there is joined up thinking on potential improvements prior to their proposals coming forward.

In conjunction with TfL and DLR, enhancements could also be made to the existing footbridge itself which might include thorough cleaning, redecoration, improved lighting and paving materials.

Over the following pages we describe a number of possible scenarios on how to improve the northern footbridge connection, including options with and without redevelopment of the New City College western building. Though these are outside of the scope and land ownership of this application, they demonstrate

the opportunities of joined up thinking that can be achieved by LBTH, working with adjoining landowners.



Lighting precedents



Wall / Floor patterns precedents



Improve and extend  
end of tunnel so  
connects directly onto  
the Poplar Plaza  
terrace

Clean and Repair  
glazing

Interesting and  
improved lighting

Improved and  
colourful paintwork to  
existing beams

New or upgraded  
floor finish with fun  
and playful pattern



Aspen Way Footbridge Opportunities



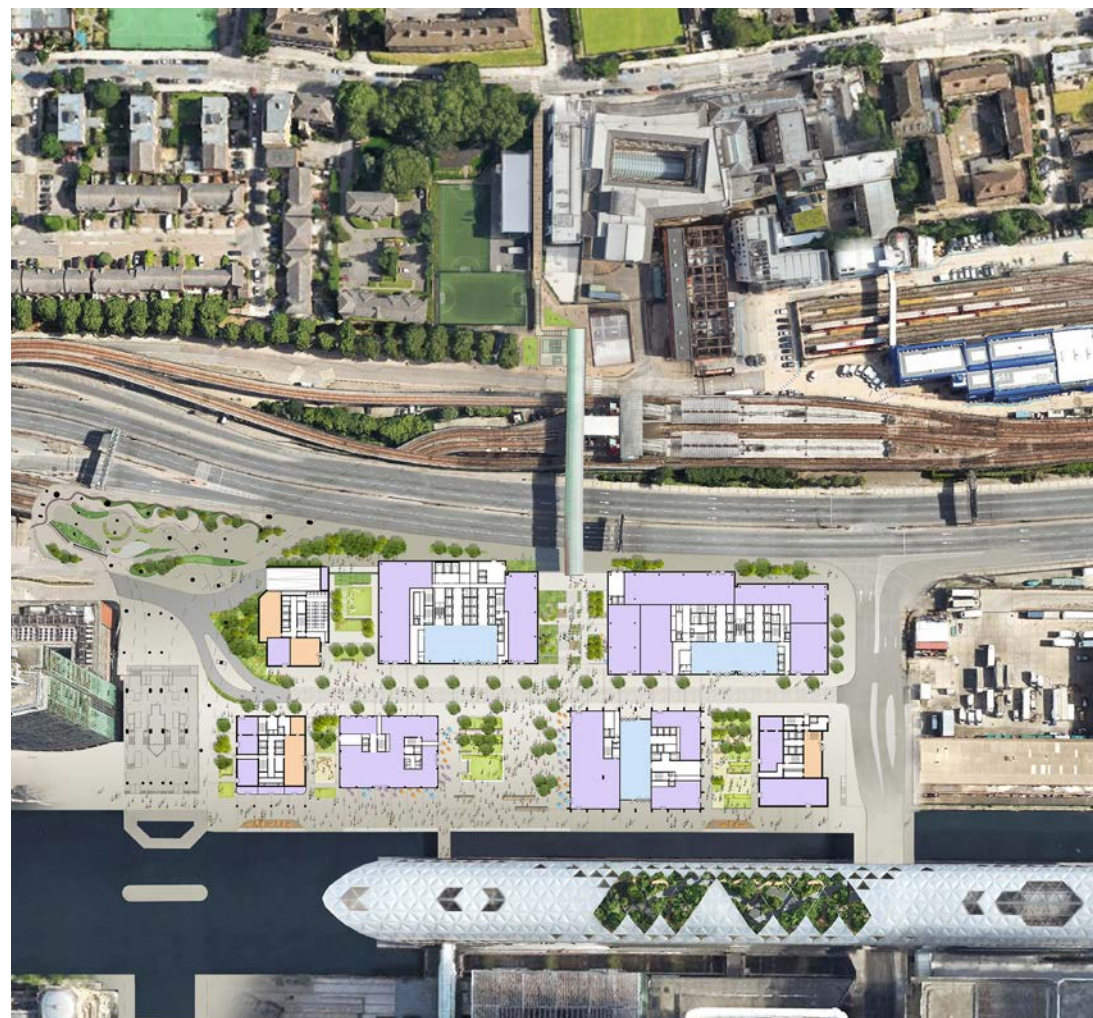
### 7.7.3 ASPEN WAY BRIDGE NORTH CONNECTION - OPTION 1

Though we understand that proposals for the redevelopment of New City College campus and Workhouse site are being developed, at the time of submitting this OPA, no application has been submitted and this option is based on the current arrangement of these sites and looks at the opportunity of improving the connection from Poplar High Street and a new and improved arrangement of level transfer up to the footbridge.

This option identifies the importance of visual connection from Poplar High Street by placing a new escalator in the view from Poplar High Street. A new bridge house which encloses the existing lift and stairs could also provide a visual connection to the bridge access point. An enhanced route could create opportunity for pockets of landscaped areas which will improve the pedestrian experience.

Key points:

- Existing stair /lift remain with new cladding/ bridge house
- New escalator connection with improved visual connection



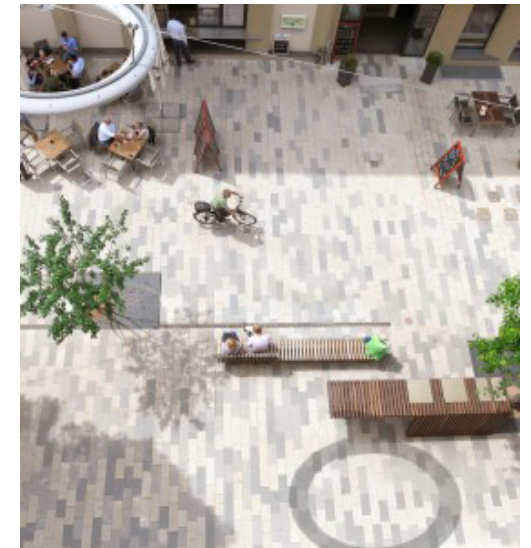
Aspen Way Bridge Link - option 1



Aspen Way Northern Connection - option 1



Precedent Image: Bridge House



Precedent Image: Public Realm

- 1 Green space
- 2 Existing lift & stair with new cladding (bridge house)
- 3 Extension to existing Aspen Way Bridge
- 4 New Escalator - direct visual connection to and from Poplar High Street
- 5 Circulation and hard landscaping
- 6 Improvement to be made to access route



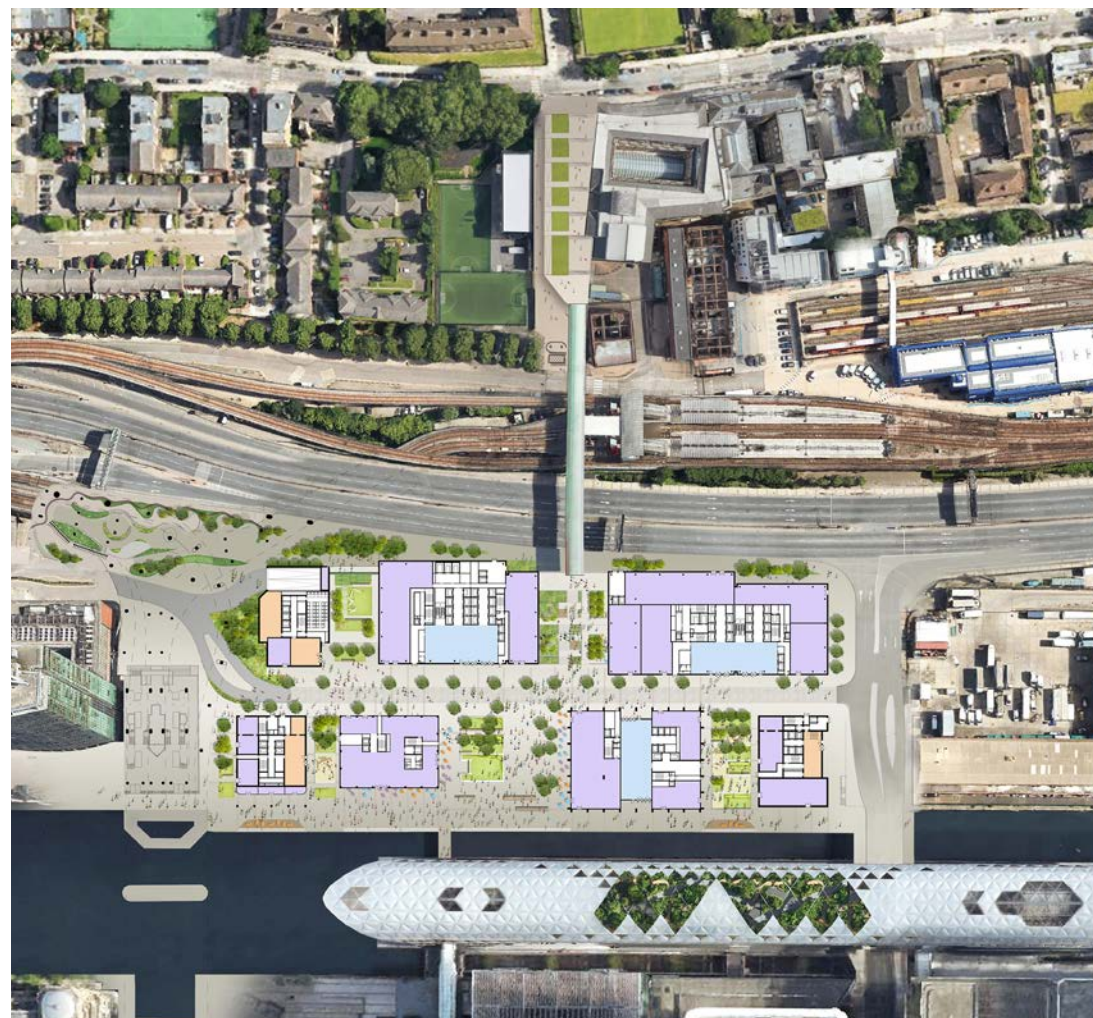
#### 7.7.4 ASPEN WAY FOOTBRIDGE NORTH CONNECTION - OPTION 2

Option 2 offers the opportunity to significantly improve the visual connection between Poplar High Street and the Poplar footbridge. This is based on the western wing of New City College being taken down and a piece of land being made available for a wider approach to be constructed providing an axial route up to the Aspen Way Footbridge.

This option could provide a series of steps and terraces that seamlessly connect Poplar High Street with the Aspen Way Footbridge. A ramp adjacent to the terraces connects Poplar High Street and Castor Lane, providing access to the DLR station.

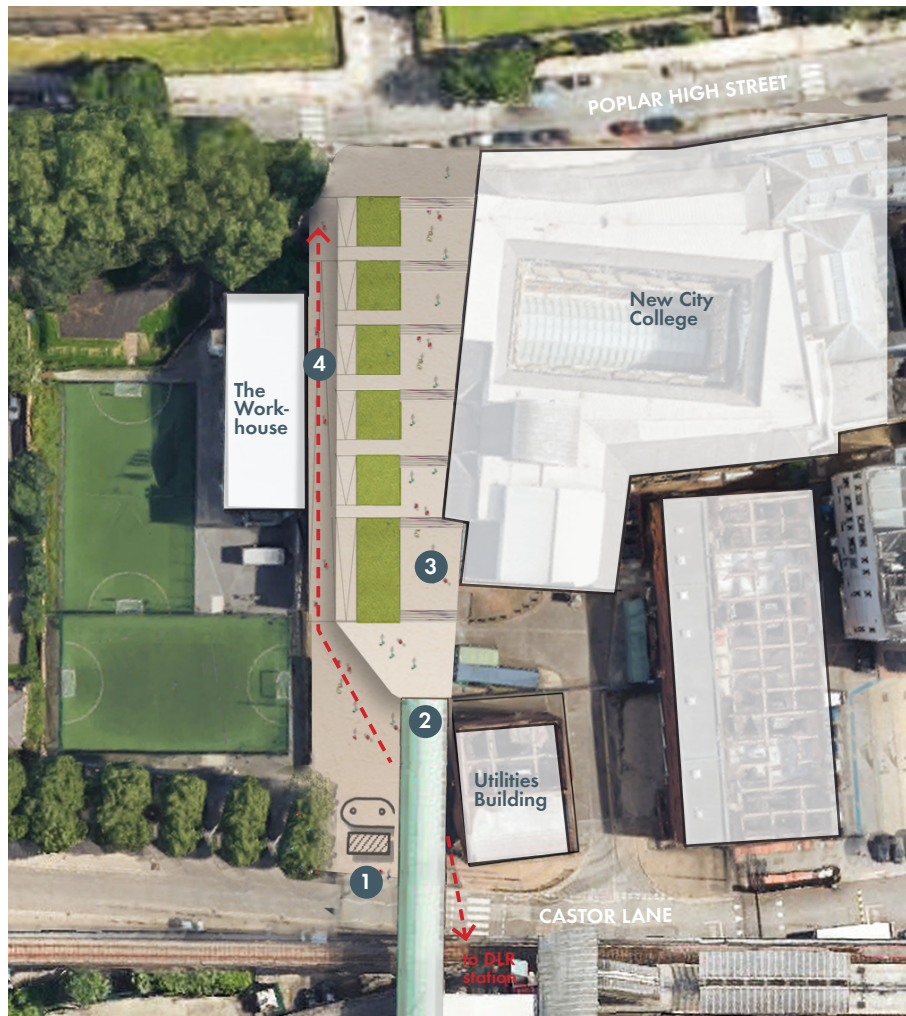
Key points:

- Existing stair /lift can remain
- New landscaped ramp/stairs to Poplar High Street
- Bridge level deck extension
- Generous and legible route



Aspen Way Footbridge - option 2





Aspen Way Footbridge - option 2



Green Wall in front of Utilities Building

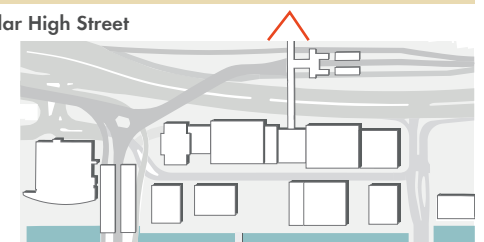


Seating in the Public Realm



View 1 - From Poplar High Street

- 1 Existing lift & stair remain
- 2 Extension to existing Aspen Way Footbridge
- 3 New ramp with terracing connecting to the bridge
- 4 Ramp from Poplar High Street to Castor Lane and DLR



View key plan







### 7.7.5 SUMMARY

Whilst outside the remit of this OPA, LBTH have an opportunity to coordinate adjoining landowners and developments coming forwards to improve this vital north south route. This would enable creation of a series of positive and active joined up spaces along the Poplar High Street to Canary Wharf and to make this a much improved and more legible connection on both sides of Aspen Way. North Quay will play a key role in making this connection successful.

A seamless transition could be created by extending the proposed material palette of the Proposed Development to Poplar High Street. A new raised table across Poplar High Street could provide a demarcated crossing to slow down vehicle movement and encourage pedestrians. New paving and lighting could also help to further enhance the pedestrian experience and with the introduction of DLR display boards and roundels and a way-finding strategy, this could help guide pedestrians to the transport and through to local connections. There is also the opportunity to create a series of active spaces along the route with the introduction of soft landscaping, raised planters and benches, which will provide meeting points to dwell and socialise.

The proposals also look at improving the experience for the users on the Aspen Way Footbridge. Redecoration, paving and lighting improvements will help to improve legibility and the aesthetics.



Illustration of The Delta